

ATTACHMENTS DISTRIBUTED UNDER SEPARATE COVER

8.3 ENDORSEMENT OF THE 2022/23 ANNUAL REPORT AND ADOPTION OF THE 2022/23 FINANCIAL STATEMENTS

Attachment A: 2022/23 Annual Report
Attachment B: 2022/23 Financial Statements

(to be distributed under separate cover - not included in this document)

8.4 SEPTEMBER QUARTERLY PERFORMANCE REPORT

Attachment A: September Quarterly Performance Report on 2023/24 Delivering Newcastle 2040 (the 2022-2026 Delivery Program)

8.5 NEW ANNUAL COUNCIL COMMITMENT

Attachment A: New Annual 2022 Wrap Report

8.6 ADOPTION OF THE WICKHAM PUBLIC DOMAIN PLAN

Attachment A: Wickham Public Domain Plan
Attachment B: Wickham PDP Summary Engagement Report
Attachment C: Wickham PDP Engagement Report Response Comments

8.8 BROADMEADOW TO HAMILTON EAST CYCLEWAY

Attachment A: Broadmeadow to Hamilton East Cycleway - Consultation Flyer
Attachment B: Broadmeadow to Hamilton East Cycleway - Summary of Submissions
Attachment C: Broadmeadow to Hamilton East Cycleway - Amended design concepts

8.9 JESMOND CYCLEWAY IMPROVEMENTS

Attachment A: Jesmond Cycleway Improvements - Consultation Flyer
Attachment B: Jesmond Cycleway Improvements - Summary of submissions
Attachment C: Jesmond Cycleway Improvements - Amended concept design

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**8.10 LAMAN STREET, COOKS HILL - PEDESTRIAN IMPROVEMENTS AND
TRAFFIC CALMING**

Attachment A: Laman Street, Cooks Hill - Consultation Flyer

Attachment B: Laman Street, Cooks Hill - Summary of submissions

8.12 EXECUTIVE MONTHLY PERFORMANCE REPORT

Attachment A: Executive Monthly Performance Report – October 2023

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**CCL 28/11/2023 - SEPTEMBER QUARTERLY PERFORMANCE
REPORT ON THE 2022-2026 DELIVERY PROGRAM**

- 8.4 Attachment A:** September Quarterly Performance
Report on 2023/24 Delivering Newcastle
2040 (the 2022-2026 Delivery Program)

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September 2023

Performance Report



Delivery Program 2022–2026
Operational Plan 2023–2024



**City of
Newcastle**

Acknowledgement of Country

City of Newcastle acknowledges with the deepest respect the Traditional Custodians of this land, a people who belong to the oldest continuing culture in the world.

We recognise their continuing connection to the land and waters, and unique cultural and spiritual relationships to the land, waters, and seas.

We are grateful for the rich, diverse, living cultures of Aboriginal people. We recognise the history of truth that acknowledges the impact of invasion and colonisation on Aboriginal people and how this still resonates today.

We pay our respect to Elders, past, present, and emerging, for they hold the memories, the traditions, the cultures, and the aspirations of Aboriginal people.

Enquiries

For information about this document contact:

City of Newcastle
PO Box 489, Newcastle NSW 2300
newcastle.nsw.gov.au

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Why we do the performance report

It is a report to our community on our performance against our Delivery Program.

CN is required under the Local Government Act s405 to provide progress reports on the Delivery Program and Operational Plan at least every six months. Our quarterly performance report details CN's progress on the principal activities detailed in the Delivery Program, *Delivering Newcastle 2040*.

Reporting to our community

The quarterly performance reports, along with the six-monthly performance report and annual report are the key points of accountability between CN and our community.

It is not a report to the Office of Local Government or the NSW Government, it is a report to our community on our performance against our Delivery Program.

Every three months, CN reports on the key activities it has undertaken which contribute to achieving our Delivery Program, *Delivering Newcastle 2040* and in the long term our Community Strategic Plan (CSP), *Newcastle 2040*.



Newcastle 2040 Community Strategic Plan

Newcastle 2040 is a shared community vision, developed as a guide to inform policies and actions throughout the city for the next 10+ years.

To guide us forward, we will focus on four themes that emerged from our shared planning process:

- Liveable
- Sustainable
- Creative
- Achieving Together

These themes work together in harmony to deliver our vision for Newcastle as a place for everyone.



Delivering Newcastle 2040

The Delivery Program is a statement of commitment to the community from our newly elected Council. It translates the community's visions and priorities into clear actions, and is the primary reference point for activities undertaken by Council during its term of office. It allows Council to determine what is achievable over the next four years, what the priorities are, and how programs will be scheduled.

The Operational Plan (including annual budget) is CN's action plan for achieving the community's priorities outlined in *Newcastle 2040* and the Delivery Program. An Operational Plan is prepared each year and adopted by Council. It identifies the projects, programs and actions that CN will deliver over a 12-month period commencing from 1 July.

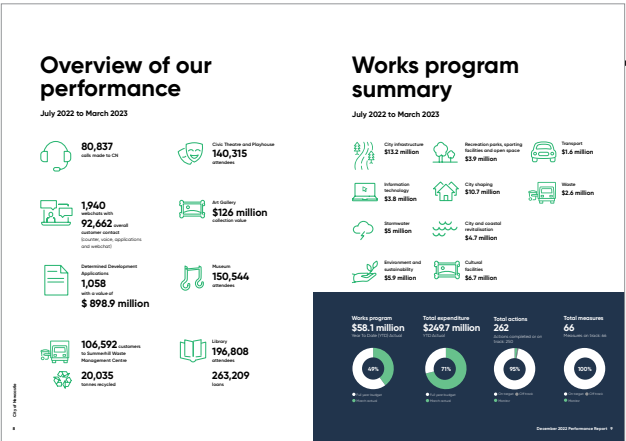
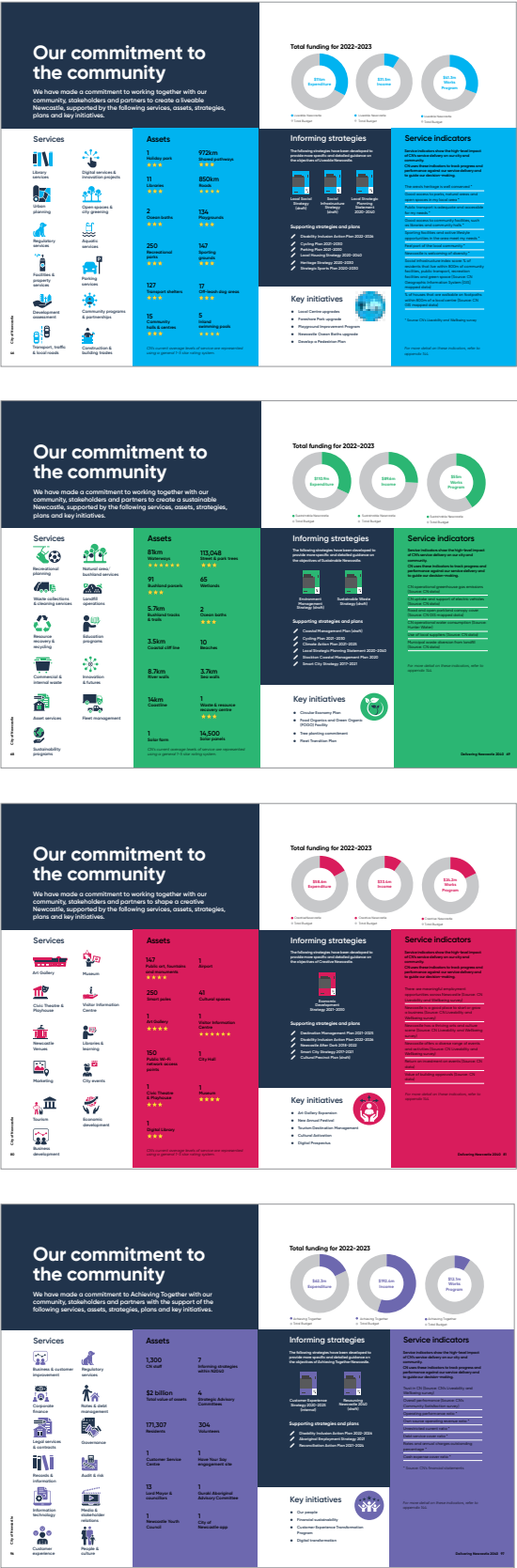


How to read our performance report

Our commitment to the community

Provides an overview for each N2040 Theme. These pages highlight the supporting initiatives identified in the Resourcing Strategy to implement *Newcastle 2040*, including:

- Funding
- Services
- Assets
- Informing strategies
- Service indicators
- Key initiatives.



Overview

A high-level overview of our performance including action progress, works program and total expenditure.

Our performance

Our actions and measures of performance by Theme.



Action progress

Action progress by Priorities and Objectives. The Strategy column indicates actions that deliver against our strategies while the BAU column indicates actions that are business as usual. Action progress is shown in the status column with the below indicators:

- On Track ✓
- Monitor ⚠
- Off Track ✗



Measures

Measures for each priority help us understand how well we are performing and allow for evidence-based decision-making to inform other stages in our planning cycle. They are reported six-monthly in the performance report and annually in the annual report. See page 60 for baseline data and more detail.

Overview of our performance

July 2023 to September 2023



27,667
calls made to CN



Civic Theatre and Playhouse
56,322
attendees



870
webchats with
92,662 overall
customer contact
(counter, voice, applications
and webchat)



Art Gallery
5,927
attendees through
offsite programs



Determined Development
Applications
102
with a value of
\$ 75.6 million



Museum
42,763
attendees



138,999 customers to
Summerhill Waste Management
Centre throughout 2022/23



5,380
tonnes recycled



Library
115,265
attendees
174,724
loans

Works program summary

July 2023 to September 2023



City infrastructure
\$3.5 million



Recreation parks, sporting
facilities and open space
\$1 million



Transport
\$1 million



Corporate services
\$0.9 million



City shaping
\$5.2 million



Waste
\$3.6 million



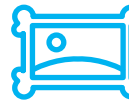
Stormwater
\$1 million



City and coastal
revitalisation
\$1.8 million

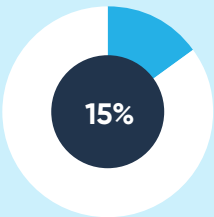


Environment and
sustainability
\$1.1 million



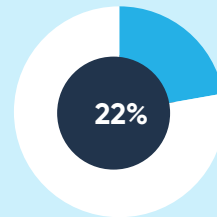
Cultural
facilities
\$1.7 million

Works program
\$20.8 million
Year To Date (YTD) Actual



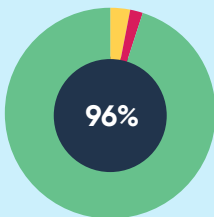
● Full year budget
● March actual

Total expenditure
\$84.7 million
YTD Actual



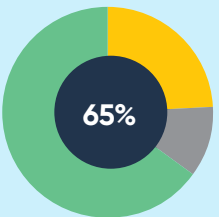
● Full year budget
● March actual

Total actions
202
Actions completed or on
track: 198



● On Track
● Monitor
● Off track

Total measures
72
Measures on track: 47



● On Track
● Monitor
● Off track
● N/A



Liveable Newcastle

We have made a commitment to working together with our community, stakeholders and partners to create a liveable Newcastle, supported by the following services, assets, strategies, plans and key initiatives.

Services



Library services



Digital services & innovation projects



Strategic planning



Open spaces & city greening



Regulatory services



Aquatic services



Facilities management & city presentation



Parking services



Development assessment



Community facilities, programs & partnerships



Transport, traffic & local roads



Construction & building trades

Assets

1 holiday park

972km shared pathways

11 libraries

850km roads

250 smart poles

134 playgrounds

250 recreational parks

147 sporting grounds

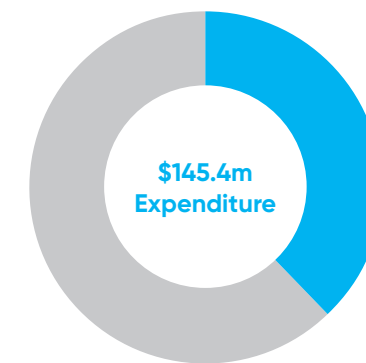
127 transport shelters

17 off-leash dog areas

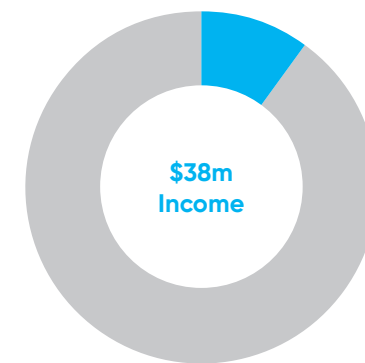
15 community halls & centres

2 ocean baths
5 inland swimming pools

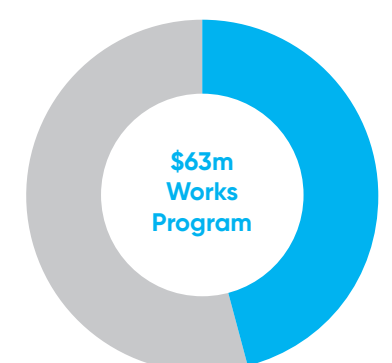
Total funding for 2023-2024



● Liveable Newcastle
● Total Budget



● Liveable Newcastle
● Total Budget



● Liveable Newcastle
● Total Budget

Informing strategies

The following strategies have been developed to provide more specific and detailed guidance on the objectives of Liveable Newcastle.



Social Strategy



Social Infrastructure Strategy (future draft)



Local Strategic Planning Statement 2020-2040

Supporting strategies and plans:

- Disability Inclusion Action Plan 2022-2026
- Cycling Plan 2021-2030
- Parking Plan 2021-2030
- Local Housing Strategy 2020-2040
- Heritage Strategy 2020-2030
- Strategic Sports Plan 2020-2030

Key initiatives

- Local Centre upgrades
- Foreshore Park upgrade
- Playground Improvement Program
- Newcastle Ocean Baths upgrade
- Walking and Mobility Plan
- Boscawen Street Bridge replacement at Wallsend

Service indicators

Service indicators show the high-level impact of CN's service delivery on our city and community.

CN uses these indicators to track progress and performance against our service delivery and to guide our decision-making.

The area's heritage is well conserved *

Good access to parks, natural areas and open spaces in my local area *

Public transport is adequate and accessible for my needs *

Good access to community facilities, such as libraries and community halls *

Sporting facilities and active lifestyle opportunities in the area meet my needs *

Feel part of the local community *

Newcastle is welcoming of diversity *

Social infrastructure index score: % of residents that live within 800m of community facilities, public transport, recreation facilities and green space (Source: CN Geographic Information System [GIS] mapped data)

% of houses that are walkable on footpaths within 800m of a local centre (Source: CN GIS mapped data)

* Source: CN's Liveability and Wellbeing survey



How we performed

Total actions
72

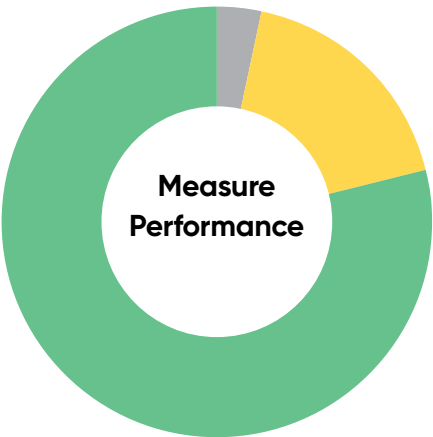
Actions completed
or on track: 72



On Track 72 | 100%
Monitor 0 | 0%
Off Track 0 | 0%

Total measures
25

Measures on track: 18



On Track 18 | 72%
Monitor 4 | 16%
Off Track 0 | 0%
N/A 3 | 12%



Recent road upgrades have transformed Hamilton's Beaumont Street. Councillor Carol Duncan, Deputy Lord Mayor Declan Clausen, CN Civil Works Contracts Supervisor's Bruce Pemberton and Geoff Blomfield, CN Operations Manager Bianca Field-Vo and Hamilton BIA chair Reece Hignall.

Priority 1.1 Enriched neighbourhoods and places

1.1.1 Great spaces

ACTION	STRATEGY/ BAU	SERVICE UNIT	STATUS
Assist Transport for NSW in the maintenance and asset renewal of regional and state roads to create and enhance welcoming entrances to the city	BAU	Civil Construction & Maintenance	✓
Work towards the launch of a world-class building expansion in late 2024 and renewed Newcastle Art Gallery identity in the lead-up	Strategy	Art Gallery	✓
Provide aquatic facilities to meet community needs and industry requirements	BAU	Community & Recreation Services	✓
Support safe use of beaches and baths through professional lifeguard services	BAU	Community & Recreation Services	✓
Provide and maintain active and passive open spaces to promote the wellbeing of the community	BAU	Community & Recreation Services	✓
Implement Social Infrastructure Strategy and plan for healthy and vibrant community centres	BAU	Community & Recreation Services	✓
Facilitate the operation of community centres and halls for use by the community	BAU	Community & Recreation Services	✓
Plan parks and recreation facilities that support inclusivity, health and wellbeing, safety and liveability	BAU	Community & Recreation Services	✓
Provide and maintain sportsgrounds and supporting services to appropriate standards that meet community needs	BAU	Community & Recreation Services	✓
Provide venues and spaces across the service unit's facilities for hire by the community	BAU	Museum Archive Libraries & Learning	✓
Deliver recreational and educational opportunities and expand community learning at Blackbutt Reserve	BAU	Environment & Sustainability	✓
Build on relationships with Local Area Land Councils	BAU	Planning, Transport & Regulation	✓

BAU: business as usual
Strategy: actions that deliver against CN's strategies

1.1.2 Well-designed places

ACTION	STRATEGY/ BAU	SERVICE UNIT	STATUS
Provide a responsive, high-quality facility management service across the organisation to meet service level standards	BAU	Assets & Facilities	✓
Undertake building asset condition inspection and reporting to identify and implement maintenance action plans, asset standards, gap analysis and long-term capital upgrade program	BAU	Assets & Facilities	✓
Provide responsive building trade services to ensure high-quality facilities management for CN's facilities and assets to meet service level standards	BAU	Assets & Facilities	✓
Develop and implement asset management processes to guide delivery of services for CN's built and civil infrastructure	BAU	Assets & Facilities	✓
Deliver retaining wall program, including inspections, design and renewal implementation to meet service level standards	BAU	Assets & Facilities	✓
Plan, design and implement remaining sections of Bathers Way	BAU	Project Management Office	✓
Apply appropriate design principle considerations and standards to public domain infrastructure project design	BAU	Project Management Office	✓
Continue to deliver Local Centres Public Domain Program to foster new growth in urban centres	BAU	Project Management Office	✓
Implement Property Portfolio Strategy to sustainably manage property assets	BAU	Finance, Property & Performance	✓
Deliver parks and recreation facilities that support inclusivity, health and wellbeing, safety and liveability with construction of new assets and renewal of existing assets	BAU	Community & Recreation Services	✓
Deliver improvements that are focused on inclusion	BAU	Museum Archive Libraries & Learning	✓
Carry out renewal and maintenance of assets and facilities to ensure they are fit for purpose	BAU	Museum Archive Libraries & Learning	✓
Deliver a high standard of development outcomes for the built environment, including efficient determination of applications	BAU	Planning, Transport & Regulation	✓
Prepare updated Newcastle Development Control Plan	BAU	Planning, Transport & Regulation	✓
Participate in government planning reform and implement required changes to internal processes	BAU	Planning, Transport & Regulation	✓
Implement Housing Strategy and continue to develop Affordable Housing Contribution Scheme	Strategy	Planning, Transport & Regulation	✓
Implement initiatives through Local Strategic Planning Statement and facilitate delivery of actions	Strategy	Planning, Transport & Regulation	✓
Prepare and facilitate delivery of Broadmeadow Place Strategy	Strategy	Planning, Transport & Regulation	✓

1.1.3 Protected heritage

ACTION	STRATEGY/ BAU	SERVICE UNIT	STATUS
Ensure operation of heritage assets (City Hall and Civic Theatre) meets conservation goals and is legislatively compliant	BAU	Civic Services	✓
Plan and manage community land as per Local Government Act 1993 and Crown Land Management Act 2016	BAU	Community & Recreation Services	✓
Increase local community understanding and participation to conserve, enhance and celebrate Newcastle's heritage places	BAU	Planning, Transport & Regulation	✓

Measures



* Measure changed from sportsgrounds to sporting facilities in 2021/2022.

Priority 1.2 Connected and fair communities

1.2.1 Connected communities

ACTION	STRATEGY/ BAU	SERVICE UNIT	STATUS
Maintain and deliver community information and data sources to support community development within the city	BAU	Community & Recreation Services	✓
Drive campaigns, education and awareness-raising initiatives that support community inclusion, liveability and belonging and speak to Local Social issues	BAU	Community & Recreation Services	✓

1.2.2 Inclusive communities

ACTION	STRATEGY/ BAU	SERVICE UNIT	STATUS
Implement Inclusion, Diversity and Equity Strategy (2023–2027)	Strategy	People & Culture	✓
Implement identified actions within the Disability Inclusion Action Plan (2022–2026)	BAU	Community & Recreation Services	✓

1.2.3 Equitable communities

ACTION	STRATEGY/ BAU	SERVICE UNIT	STATUS
Embed Sustainable Development Goals across the LGA through support, advocacy and strategic alignment of CN activities	BAU	Finance, Property & Performance	✓
Govern and support Grants and Sponsorship Program and support grant governance across the organisation	BAU	Finance, Property & Performance	✓
Build capacity within the community to improve the quality of grant applications and funded activities within the LGA	BAU	Finance, Property & Performance	✓
Implement Reconciliation Action Plan (2022–2025)	Strategy	People & Culture	✓
Undertake social research, analysis and advocacy that supports the community	BAU	Community & Recreation Services	✓
Facilitate projects and programs that support and build capacity of the community sector	BAU	Community & Recreation Services	✓
Facilitate targeted partnerships that contribute to socio-economic inclusion outcomes for the people of Newcastle	BAU	Community & Recreation Services	✓
Coordinate fair and equitable licensing of public spaces with positive customer experience	Strategy	Media, Engagement, Economy & Corporate Affairs	✓



Cr Elizabeth Adamczyk, Uncle Amos Simon and Cr Deahanna Richardson with some of the participants of the first monthly Sharing Culture Storytime session held at Wallsend Library today in celebration of NAIDOC Week.



CN champions disability inclusion with launch of Count Us In festival. Councillor Margaret Wood with Songbird drummers and Councillor Katrina Wark.

1.2.4 Healthy communities

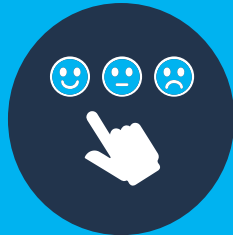
ACTION	STRATEGY/ BAU	SERVICE UNIT	STATUS
Deliver programs for maintenance, renewal and upgrade of existing stormwater infrastructure	BAU	Assets & Facilities	✓
Maintain city and coastline assets to a high standard of cleanliness for community and visitors	BAU	Assets & Facilities	✓
Promote water safety awareness that supports community wellbeing and continue to develop and deliver initiatives to increase awareness	BAU	Community & Recreation Services	✓
Undertake Social Impact Assessment on identified development as part of Development Assessment process	BAU	Community & Recreation Services	✓
Facilitate community programs to meet objectives of Local Social Strategy	BAU	Community & Recreation Services	✓
Facilitate cemetery operations and management in Minmi, Stockton and Beresfield	BAU	Community & Recreation Services	✓
Promote and encourage use of recreation parks and facilities for community health and wellbeing through a variety of channels	BAU	Community & Recreation Services	✓
Deliver high-quality childcare and early childhood education services to families in Beresfield and surrounds that is responsive, fit for purpose and meets legislative and statutory requirements	BAU	Museum Archive Libraries & Learning	✓
Provide animal management services, including education of the community and enforcement of regulations	BAU	Planning, Transport & Regulation	✓
Carry out a broad range of compliance activities, including patrols, inspections, investigations and education, to protect public safety, the environment and public amenity	BAU	Planning, Transport & Regulation	✓



Councillor Carol Duncan, Wickham Park Hotel owner Marcus Wright and musician One Man Dan announce the new round of Locally Made and Played funding from City of Newcastle.



Measures



Qualitative measures based on community perceptions



Level of community satisfaction with libraries



Number of library loans



Number of Home Library Service items and members



Quantitative measures based on data



Number of awareness-raising initiatives specific to inclusion



Program attendance (libraries)



Visits to physical service points (libraries)

Priority 1.3 Safe, active and linked movement across the city

1.3.1 Connected cycleways and pedestrian networks

ACTION	STRATEGY/BAU	SERVICE UNIT	STATUS
Ensure projects incorporate objectives in the Disability Inclusion Action Plan (2022–2026) to enable safe and active movement across the city	BAU	Assets & Facilities	✓
Deliver improvements to existing infrastructure to accommodate better pedestrian connectivity and accessibility across the LGA's network of footpaths, shared paths and cycleways	BAU	Civil Construction & Maintenance	✓
Undertake forward planning based off data-driven projections and manage effective delivery of investment in transport infrastructure in alignment with strategic goals under the Transport Program (Parking, Pedestrian, Cycling, Traffic Management)	Strategy	Planning, Transport & Regulation	✓
Upgrade, expand and connect cycling facilities (in accordance with the Safe System approach), including shared paths, dedicated cycleways, bike parking and on-road provision	BAU	Planning, Transport & Regulation	✓

1.3.2 Road networks

ACTION	STRATEGY/BAU	SERVICE UNIT	STATUS
Develop and implement road rehabilitation and resurfacing programs to meet service level standards	BAU	Assets & Facilities	✓
Deliver bridge program, including inspections, design and renewal to meet service level standards	BAU	Assets & Facilities	✓
Schedule and deliver routine inspection program for building and civil infrastructure asset condition	BAU	Assets & Facilities	✓
Deliver Capital Works Program for civil infrastructure renewal and replacement in line with community needs and nominated targets for roads and drainage assets, and proactively manage maintenance of existing road infrastructure	BAU	Civil Construction & Maintenance	✓

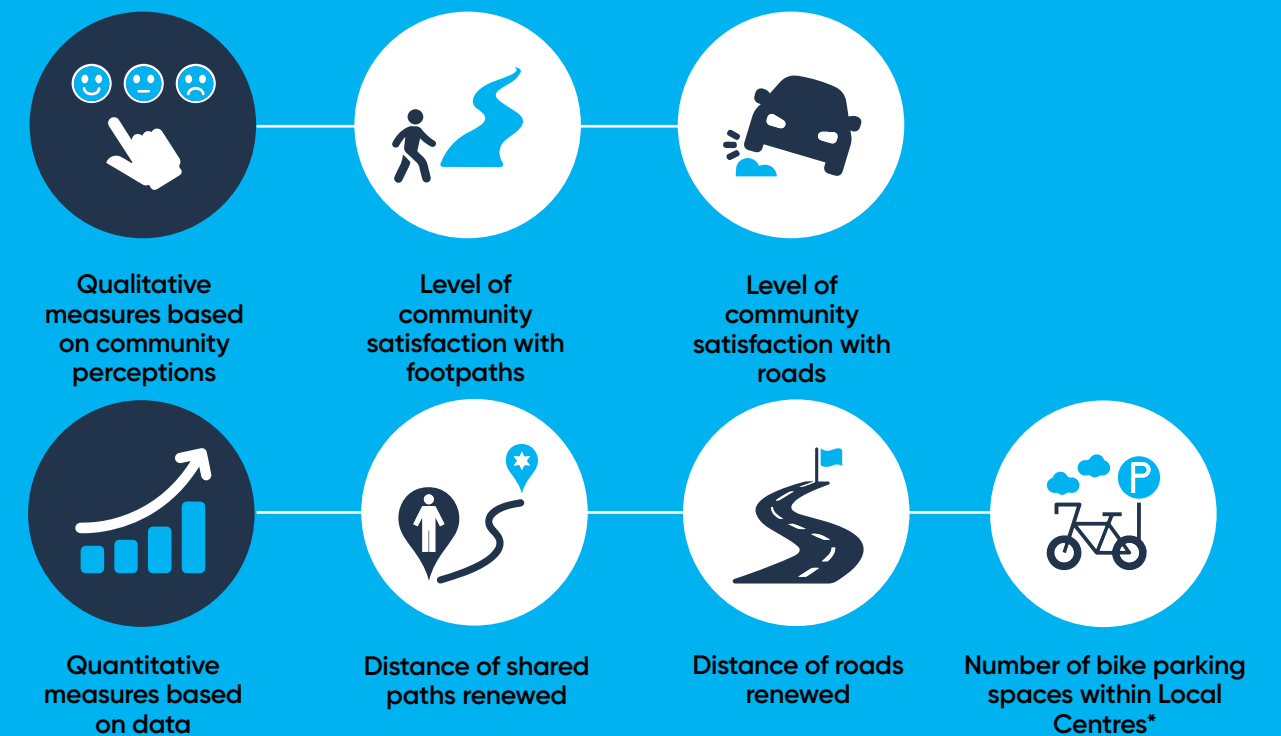
1.3.3 Managed parking

ACTION	STRATEGY/BAU	SERVICE UNIT	STATUS
Enforce legislation related to parked vehicles, as delegated to CN under the Roads Act 1993 and Local Government Act 1993	BAU	Planning, Transport & Regulation	✓
Manage all areas with on-street parking restrictions, including paid parking areas, assets, services and signage, and undertake strategic approach to paid parking elements, rates and innovation in smart parking	BAU	Planning, Transport & Regulation	✓

1.3.4 Effective public transport

ACTION	STRATEGY/BAU	SERVICE UNIT	STATUS
Develop Transport Stop Renewal and Upgrade Program	BAU	Assets & Facilities	✓
Develop and promote effective traffic and transport management, information and strategy, including coordination with transport stakeholders on strategic transport outcomes and promotion of modal shift and active transport	Strategy	Planning, Transport & Regulation	✓

Measures



* New measure, no data available.

Priority 1.4 Innovative and connected city

1.4.1 Emerging technologies

ACTION	STRATEGY/ BAU	SERVICE UNIT	STATUS
Continue to mature and enhance Spatial Digital Twin to include more of the city's natural, built and social environments and facilitate better planning, service delivery and outcomes for the city	BAU	Information Technology	✓
Evolve smart city data and platforms, including sensors, IoT networks, data platforms and apps to inform decision-making	BAU	Information Technology	✓

1.4.2 Digital inclusion and social innovation

ACTION	STRATEGY/ BAU	SERVICE UNIT	STATUS
Establish a fully resourced virtual library and seamless online membership experience	Strategy	Museum Archive Libraries & Learning	✓
Create and improve digital experiences and focus on Science, Technology, Engineering and Maths (STEM) and Robotics to provide inclusive access and exposure to current and future technologies	BAU	Museum Archive Libraries & Learning	✓
Provide access to technology and Wi-Fi for research and recreation to increase participation in a digital society and reduce social isolation	BAU	Museum Archive Libraries & Learning	✓



Newcastle Lord Mayor Nuatali Nelmes and Deputy Lord Mayor Declan Clausen with City of Newcastle Project Managers Grant Mansfield and Alex Dunn utilising the new traffic signals at the intersection of Clyde and Chinchin streets in Islington, which became operational last night.



Science show presenters Sharni Pike and Jacquelyn Flynn from the University of Newcastle's SMART Program kept audiences engaged during the free science shows held at Wallsend Library to kick off National Science Week on 13 August.

Measures



* Webchat satisfaction measure removed due to changes in software. This measure has been added to replace webchat satisfaction.



Sustainable Newcastle

We have made a commitment to working together with our community, stakeholders and partners to create a sustainable Newcastle, supported by the following services, assets, strategies, plans and key initiatives.

Services



Sustainability programs



Natural area/ bushland services



Waste collections & cleaning services



Landfill operations



Resource recovery & recycling



Education programs



Commercial & internal waste



Innovation & futures



Asset services

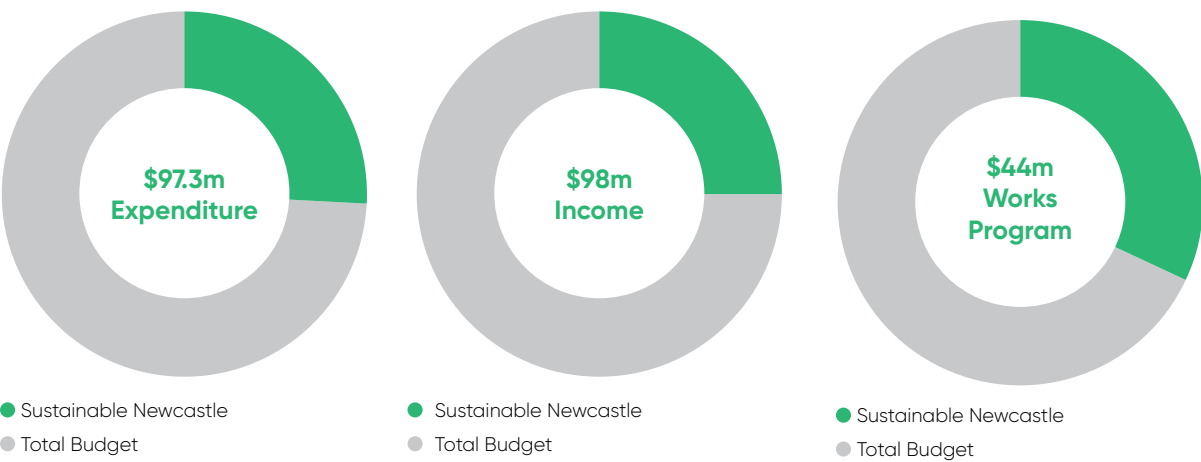


Fleet management

Assets

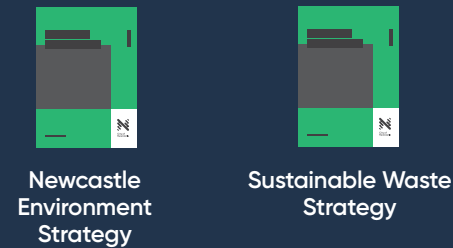
81km Waterways	113,048 Street & park trees
91 Bushland parcels	65 Wetlands
5.7km Bushland tracks & trails	2 Ocean baths
3.5km Coastal cliff line	10 Beaches
8.7km River walls	3.7km Sea walls
14km Coastline	1 Waste & resource recovery centre
1 Solar farm	14,500 Solar panels

Total funding for 2023-2024

Sustainable NewcastleTotal Budget

Informing strategies

The following strategies have been developed to provide more specific and detailed guidance on the objectives of Sustainable Newcastle.



Supporting strategies and plans

- Coastal Management Plan
- Cycling Plan 2021-2030
- Climate Action Plan 2021-2025
- Local Strategic Planning Statement 2020-2040
- Stockton Coastal Management Plan 2020
- Smart City Strategy 2017-2021

Key initiatives

- Food Organics and Green Organic (FOGO) Facility
- Tree planting commitment
- Fleet Transition Plan



Service indicators

Service indicators show the high-level impact of CN's service delivery on our city and community.

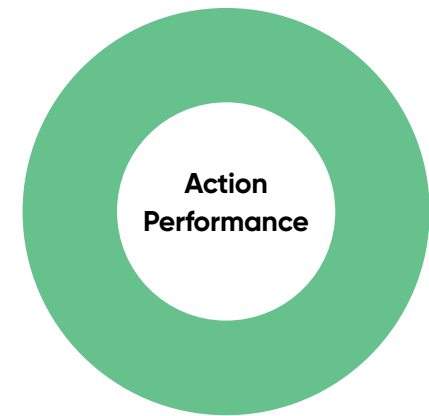
CN uses these indicators to track progress and performance against our service delivery and to guide our decision-making.

- CN operational greenhouse gas emissions (Source: CN data)
- CN uptake and support of electric vehicles (Source: CN data)
- Road and open parkland canopy cover (Source: CN GIS mapped data)
- CN operational water consumption (Source: Hunter Water)
- Use of local suppliers (Source: CN data)
- Municipal waste diversion from landfill (Source: CN data)
- Proximity to natural areas (Source: CN GIS mapped data)



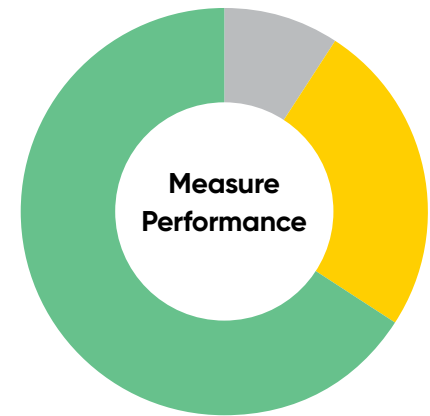
How we performed

Total actions
17
Actions completed
or on track: 17



On Track 17 | 100%
Monitor 0 | 0%
Off Track 0 | 0%

Total measures
12
Measures on track: 8



On Track 8 | 66%
Monitor 3 | 25%
Off Track 0 | 0%
N/A 1 | 9%



Electric Vehicle (EV) charging pole, Dixon Park Beach. To find a public charging station near you, visit our [interactive map](#).

Priority 2.1 Action on climate change

2.1.1 Towards net zero emissions

ACTION	STRATEGY/BAU	SERVICE UNIT	STATUS
Develop transition plan to zero-emissions vehicles for entire fleet and continue replacement of fleet with Electric Vehicles (EV)	Strategy	Environment & Sustainability	✓
Deliver priority actions from Climate Action Plan (2021–2025)	Strategy	Environment & Sustainability	✓

2.1.2 Know and share our climate risk

ACTION	STRATEGY/BAU	SERVICE UNIT	STATUS
Improve our knowledge of the risks of climate change on our urban and natural environments and our community	BAU	Environment & Sustainability	✓

2.1.3 Resilient urban and natural areas

ACTION	STRATEGY/BAU	SERVICE UNIT	STATUS
Deliver coastal management program for Newcastle LGA	Strategy	Environment & Sustainability	✓
Deliver flood risk management program for Newcastle LGA	Strategy	Environment & Sustainability	✓
Integrate climate-resilient species and successional planting into urban forest improvement program	BAU	Environment & Sustainability	✓
Develop Climate Risk and Resilience Action Plan, including a climate risk analysis, to support CN and community adaptation	Strategy	Environment & Sustainability	✓

BAU: business as usual
Strategy: actions that deliver against CN's strategies

Measures



Qualitative measures based on community perceptions



Level of community satisfaction with climate action



Quantitative measures based on data



All installed lighting to be LED



Reduction in CN electricity use



Number of EV chargers available to the community

Priority 2.2 Nature-based solutions

2.2.1 Regenerate natural systems

ACTION	STRATEGY/BAU	SERVICE UNIT	STATUS
Review and update natural asset registers to support improved strategic and operational planning	BAU	Environment & Sustainability	✓
Deliver projects that improve the health and condition of our natural assets and protect and enhance the natural environment	BAU	Environment & Sustainability	✓
Deliver environmental education and volunteering programs to enhance community stewardship of our natural environment	BAU	Environment & Sustainability	✓

2.2.2 Expand the urban forest

ACTION	STRATEGY/BAU	SERVICE UNIT	STATUS
Deliver street and park tree replacement program to expand the city's urban forest	BAU	Environment & Sustainability	✓
Commence development of blue and green grid mapping for Newcastle LGA	BAU	Environment & Sustainability	✓

2.2.3 Achieve a water-sensitive city

ACTION	STRATEGY/BAU	SERVICE UNIT	STATUS
Partner with external stakeholders to implement stormwater management and water quality improvements across the city	BAU	Environment & Sustainability	✓

Measures



Qualitative measures based on community perceptions



Level of community satisfaction with wetlands and estuary



Level of community satisfaction with bushland and waterways



Level of community satisfaction with greening and tree preservation



Quantitative measures based on data



Tree vacancies identified in Tree Asset Management System*



Number of plants used in urban forest planting

* Data is currently on hold due to changes in CN systems.

Priority 2.3 Circular economy

2.3.1 Design out waste

ACTION	STRATEGY/ BAU	SERVICE UNIT	STATUS
Positioning for delivery of Sustainable Waste Strategy	Strategy	Waste Services	✓

Note: Actions relating to the Sustainable Waste Strategy have been updated with the above Action as we focus on early-stage positioning to better enable delivery upon the strategy.

2.3.2 Localised supply chain and sustainable procurement

ACTION	STRATEGY/ BAU	SERVICE UNIT	STATUS
Ensure works program will incorporate local suppliers and sustainable procurement where financially responsible	BAU	Finance, Property & Performance	✓
Showcase local suppliers and support circular economy with 85% of menu items from within the catchment of the Hunter Joint Organisation	BAU	Civic Services	✓
Provide a unique retail space that showcases local artists and producers	BAU	Media, Engagement, Economy & Corporate Affairs	✓

Measures



Qualitative measures based on community perceptions



Level of community satisfaction with green waste collection



Quantitative measures based on data



Tonnes of waste material recovered



Tonnes of municipal waste material landfilled



Community digs deep to create habitat on National Tree Day. Councillor Margaret Wood, Councillor Dr Elizabeth Adamczyk, Councillor Peta Winney-Baartz, Lord Mayor Nuatali Nelmes, Councillor Carol Duncan and Councillor Deahnna Richardson with City of Newcastle team members (back row) and volunteer tree planters (front).




Electric Vehicle (EV) charging station, No.2 Sportsground.





Creative Newcastle


We have made a commitment to working together with our community, stakeholders and partners to shape a creative Newcastle, supported by the following services, assets, strategies, plans and key initiatives.


Services


- 


Art Gallery
- 


Museum
- 


Civic Theatre & Playhouse
- 


Visitor Information Centre
- 


Newcastle Venues
- 

Libraries & learning
- 

Marketing
- 

City events
- 

Tourism
- 

Economic development
- 

Business development

Assets

- 176

public art, fountains and monuments
- 1

airport
- 1

Art Gallery
- 41

cultural spaces
- 150

public Wi-Fi network access points
- 1

Visitor Information Centre
- 1

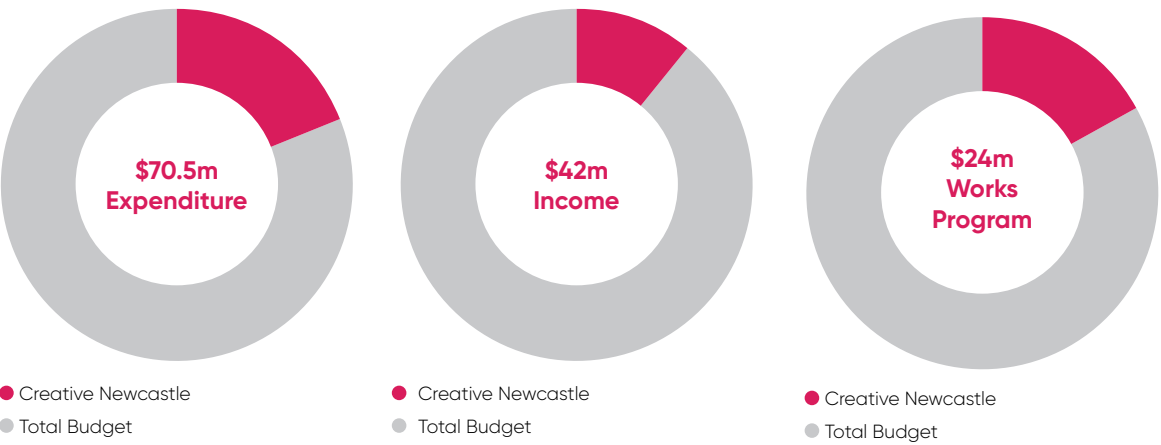
Civic Theatre & Playhouse
- 1

City Hall
- 1

Digital Library
- 1

Museum

Total funding for 2023-2024



Informing strategies

The following strategies have been developed to provide more specific and detailed guidance on the objectives of Creative Newcastle.



Economic Development Strategy 2021-2030

Supporting strategies and plans:

- Destination Management Plan 2021-2025
- Disability Inclusion Action Plan 2022-2026
- Cultural Precinct Masterplan 2022

Key initiatives

- Art Gallery expansion
- New Annual Festival
- Tourism destination management
- Cultural activation
- Digital prospectus

Service indicators

Service indicators show the high-level impact of CN's service delivery on our city and community.

CN uses these indicators to track progress and performance against our service delivery and to guide our decision-making.

There are meaningful employment opportunities across Newcastle (Source: CN Liveability and Wellbeing survey)

Newcastle is a good place to start or grow a business (Source: CN Liveability and Wellbeing survey)

Newcastle has a thriving arts and culture scene (Source: CN Liveability and Wellbeing survey)

Newcastle offers a diverse range of events and activities (Source: CN Liveability and Wellbeing survey)

Return on investment on events (Source: CN data)

Value of building approvals (Source: CN data)

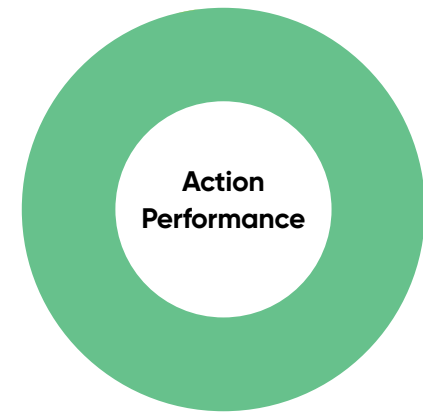


How we performed

Total actions

36

Actions completed or on track: 36

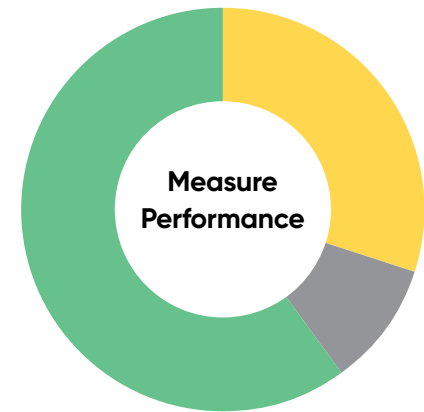


On Track 36 | 100%
Monitor 0 | 0%
Off Track 0 | 0%

Total measures

20

Measures on track: 12



On Track 12 | 60%
Monitor 6 | 30%
Off Track 0 | 0%
N/A 2 | 10%



New Annual opens. New Annual Senior Producer & Curator Adrian Burnett, Christ Church Cathedral Dean Katherine Bowyer, Lord Mayor Nuatali Nelmes, Circa Contemporary Circus Associate Director Ben Knapton and performers celebrate the start of New Annual.

Priority 3.1 Vibrant and creative city

3.1.1 Vibrant events

ACTION	STRATEGY/BAU	SERVICE UNIT	STATUS
Plan and develop launch of a new artistic program of temporary exhibitions	BAU	Art Gallery	✓
Present offsite programming that foreshadows the reopening program	BAU	Art Gallery	✓
Attract business events to City Hall from outside the LGA	BAU	Civic Services	✓
Deliver Event Sponsorship Program and Strategic Events Partnership Program to create vibrant spaces for community and visitors and support Newcastle's visitor economy	Strategy	Media, Engagement, Economy & Corporate Affairs	✓
Deliver New Annual, CN's flagship arts and cultural event	BAU	Media, Engagement, Economy & Corporate Affairs	✓
Develop and deliver updated Events Plan aligned with Destination Management Plan (2021–2025)	Strategy	Media, Engagement, Economy & Corporate Affairs	✓

3.1.2 Bold and challenging programs

ACTION	STRATEGY/BAU	SERVICE UNIT	STATUS
Plan and prepare an opening and ongoing artistic program that is inclusive, representative and reflects the strength and diversity of global artistic expression	BAU	Art Gallery	✓
Present the best of international, national and local live performances across a broad arts spectrum	BAU	Civic Services	✓
Deliver Major Events Program including New Year's Eve and Anzac Day	BAU	Media, Engagement, Economy & Corporate Affairs	✓
Develop and deliver a program of permanent, travelling, temporary and community exhibitions for and about Newcastle	BAU	Museum Archive Libraries & Learning	✓
Deliver engaging, diverse and inclusive programs that support, connect and reflect our community	BAU	Museum Archive Libraries & Learning	✓

BAU: business as usual
Strategy: actions that deliver against CN's strategies

3.1.3 Tourism and visitor economy

ACTION	STRATEGY/ BAU	SERVICE UNIT	STATUS
Develop a Masterplan for Stockton Beach Holiday Park that promotes Newcastle's tourism and visitor economy and ensures financial sustainability	BAU	Finance, Property & Performance	✓
Lead sustainable growth of Newcastle's visitor economy as identified in the Newcastle Destination Management Plan (2021–2025)	Strategy	Media, Engagement, Economy & Corporate Affairs	✓
Manage Newcastle's destination brand Seek Off Beat	Strategy		✓
Maintain destination digital consumer assets, including Visit Newcastle website and Business Events website, as well as print promotions such as City Guide, self-guided itineraries and maps	Strategy		✓
Increase awareness of Newcastle as a premier regional business events destination and secure high-yielding business events that support Newcastle's visitor economy	Strategy		✓
Enhance digital engagement for CN's corporate and social sites	BAU		✓
Provide complimentary support to visitor economy businesses and large-scale events	BAU		✓
Support tourism industry to enhance visitor experience in Newcastle	BAU		✓

3.1.4 Vibrant night-time economy

ACTION	STRATEGY/ BAU	SERVICE UNIT	STATUS
Support projects to understand, increase and enhance venue diversity at night	Strategy	Media, Engagement, Economy & Corporate Affairs	✓
Deliver place activation and continue to develop and advocate for strategic policy and planning to enable, enhance and support night-time economy and live music	Strategy	Media, Engagement, Economy & Corporate Affairs	✓



Home-grown talent celebrated with additional live performances during New Annual. Cr Carol Duncan with local band Butterknife at the Hamilton Station Hotel.

Measures



Priority 3.2 Opportunities in jobs, learning and innovation

3.2.1 Inclusive opportunities

ACTION	STRATEGY/BAU	SERVICE UNIT	STATUS
Design and deliver member-responsive, diverse, entertaining, innovative and educational library collections	BAU	Museum Archive Libraries & Learning	✓

3.2.2 Skilled people and businesses

ACTION	STRATEGY/BAU	SERVICE UNIT	STATUS
Implement New Move community program and leverage to drive engagement, talent attraction and advocacy	BAU	Media, Engagement, Economy & Corporate Affairs	✓
Deliver economic and workforce development programs and resources at our libraries to improve employment and productivity outcomes	BAU	Museum Archive Libraries & Learning	✓

3.2.3 Innovative people and businesses

ACTION	STRATEGY/BAU	SERVICE UNIT	STATUS
Deliver programming that supports attraction and development of startup/scaleup businesses	Strategy	Media, Engagement, Economy & Corporate Affairs	✓



CN delivers million-dollar investment to support innovative local business projects. Newcastle Lord Mayor Nuatali Nemes and Newcastle Councillors with SBR funding recipients and business community representatives.

Measures



Qualitative measures based on community perceptions



Level of community satisfaction with economic development



Quantitative measures based on data



Number of Newskills training projects and number of participants*



Number of users of Landing Pad startups/scaleups considering relocation to Newcastle*

* These programs are complete, we are no longer capturing this data.

Priority 3.3 Celebrating culture

3.3.1 Nurture cultural and creative practitioners

ACTION	STRATEGY/ BAU	SERVICE UNIT	STATUS
Present accessible and inclusive range of free or low-cost activities to build new audiences	BAU	Civic Services	✓
Manage, conserve and digitise cultural collections, ensuring adherence to relevant policies and procedures	BAU	Museum Archive Libraries & Learning	✓
Provide sector development support for Newcastle and the Hunter Region's network of volunteer and community-initiated museums, historical societies and Keeping Places	Strategy	Museum Archive Libraries & Learning	✓

3.3.2 Promote Newcastle as a major arts and cultural destination

ACTION	STRATEGY/ BAU	SERVICE UNIT	STATUS
Attract new and existing audiences from across the state, nation and globe and deepen engagement with art and artists through public and educational programming	BAU	Art Gallery	✓
Prepare the Gallery's permanent collection for long-term public displays within the expanded Newcastle Art Gallery	BAU	Art Gallery	✓
Deliver NewSkills program to provide support for training initiatives that address skills gaps and areas of economic transformation	Strategy	Media, Engagement, Economy & Corporate Affairs	✓

3.3.3 Culture in everyday life

ACTION	STRATEGY/ BAU	SERVICE UNIT	STATUS
Collaborate with internal CN partners to deliver cultural activities of community benefit	BAU	Civic Services	✓
Maintain community access to physical and digital cultural collections for the purposes of research, entertainment and education	BAU	Museum Archive Libraries & Learning	✓
Enhance and expand cultural collections through the acceptance of relevant heritage material, ensuring adherence to relevant policies and procedures	BAU	Museum Archive Libraries & Learning	✓



The community was invited to celebration of First Nations creativity and culture. Hamilton's James Street Plaza.

Measures



Qualitative measures based on community perceptions



Level of community satisfaction with Art Gallery and programs



Level of community satisfaction with Civic venues



Level of community satisfaction with Museum



Quantitative measures based on data



Students attending Art Gallery programming*



Number of Museum ticketed attendees



Number of artists celebrated in Art Gallery programming

* New measure due to closure of Art Gallery for expansion works.

Priority 3.4 City-shaping partnerships

3.4.1 Optimise city opportunities

ACTION	STRATEGY/ BAU	SERVICE UNIT	STATUS
Develop and maintain a digital platform aimed at raising the profile of Newcastle's economic development opportunities	BAU	Media, Engagement, Economy & Corporate Affairs	✓

3.4.2 Advocacy and partnerships

ACTION	STRATEGY/ BAU	SERVICE UNIT	STATUS
Develop and implement rolling 12-month Government Relations Roadmap that articulates forthcoming advocacy actions	Strategy	Media, Engagement, Economy & Corporate Affairs	✓
Support the operation of Newcastle's 4 Business Improvement Associations	Strategy	Media, Engagement, Economy & Corporate Affairs	✓
Establish a monthly newsletter to local businesses	Strategy	Media, Engagement, Economy & Corporate Affairs	✓



CN pitching in to help local sporting clubs. Cr Peta Winney-Baartz presenting to sports club representatives at City of Newcastle's Sports Summit

Measures



Port Stephens Council, Deputy Mayor, Councillor Leah Anderson, RAAF Head of Air Shows Air Commodore Micka Gray, Newcastle Lord Mayor Nuatali Nelmes and RAAF Williamtown Senior Australian Defence Force Officer, Group Captain Anthony Stainton at the Williamtown RAAF base today.



Achieving Together

We have made a commitment to Achieving Together with our community, stakeholders and partners with the support of the following services, assets, strategies, plans and key initiatives.

Services



Procurement & contracts



Corporate planning & performance



Corporate finance



Rates & debt management



Legal services



Governance



Records & information



Audit & risk



Information technology



Media & stakeholder relations



Customer experience



People & culture

Assets

1,300
CN staff

7
Informing strategies within N2040

\$2 billion
Total value of assets

4
Strategic Advisory Committees

171,307
Residents

304
Volunteers

1
Customer Service Centre

1
Have Your Say engagement site

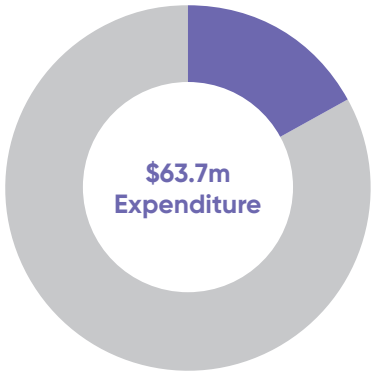
13
Lord Mayor & councillors

1
Guraki Aboriginal Advisory Committee

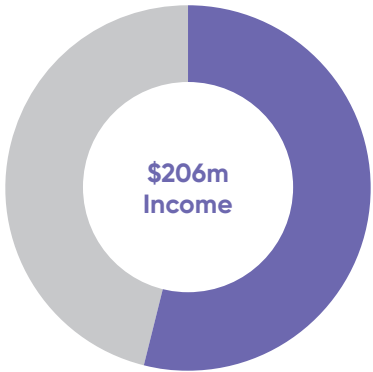
1
Newcastle Youth Council

1
Audit & Risk Committee

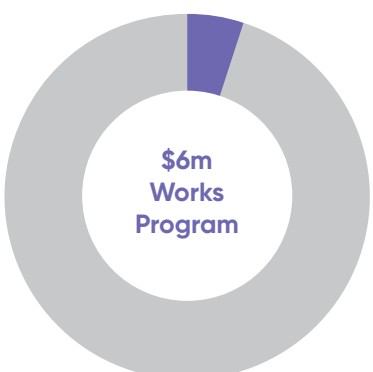
Total funding for 2022-2023



Achieving Together
Total Budget



Achieving Together
Total Budget



Achieving Together
Total Budget

Informing strategies

The following strategies have been developed to provide more specific and detailed guidance on the objectives of Achieving Together Newcastle.



Customer Experience Strategy 2020-2025 (internal)



Resourcing Newcastle 2040



Workforce Development Strategic Plan 2022-2026

Supporting strategies and plans:

- Inclusion, Diversity & Equity Strategy 2023-2027
- Disability Inclusion Action Plan 2022-2026
- Aboriginal Employment Strategy 2021
- Reconciliation Action Plan 2021-2024

Key initiatives

- Our people
- Financial sustainability
- Customer Experience Transformation Program
- Digital transformation

Service indicators

Service indicators show the high-level impact of CN's service delivery on our city and community.
CN uses these indicators to track progress and performance against our service delivery and to guide our decision-making.

Trust in CN (Source: CN's Liveability and Wellbeing survey)

Overall performance (Source: CN's Community Satisfaction survey)

Operating performance ratio *

Own source operating revenue ratio *

Unrestricted current ratio *

Debt service cover ratio *

Rates and annual charges outstanding percentage *

Cash expense cover ratio *

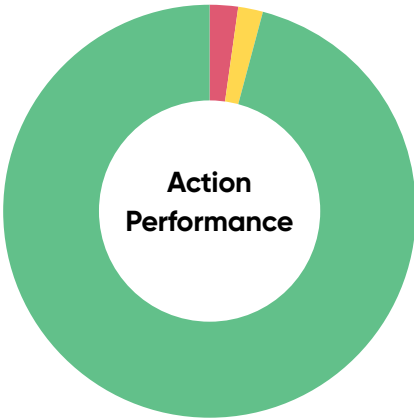
* Source: CN's financial statements



How we performed

Total actions
77

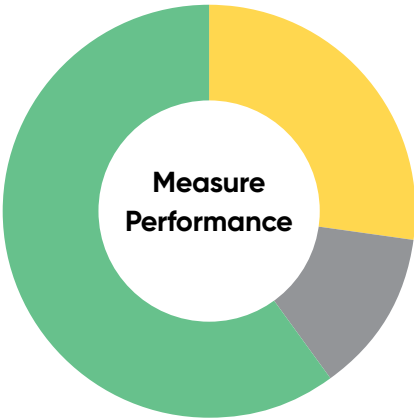
Actions completed
or on track: 73



On Track 73 | 96%
Monitor 2 | 2%
Off Track 2 | 2%

Total measures
15

Measures completed
or on track: 9



On Track 9 | 60%
Monitor 4 | 27%
Off Track 0 | 0%
N/A 2 | 13%

Newcastle
ENVIRONMENT
Strategy 2023

Roadmap to a sustainable Newcastle



Community members were given the chance to comment on the Newcastle Environment Strategy (NES) via the Have Your Say page on CN's website. The NES was adopted by Council on 31 November 2023.

Priority 4.1 Inclusive and integrated planning

4.1.1 Financial sustainability

ACTION	STRATEGY/ BAU	SERVICE UNIT	STATUS
Lead prudent and proactive financial management across the organisation that ensures a positive financial legacy	BAU	Finance, Property & Performance	✓
Ensure timely and accurate management of accounts payable, stores and logistics, purchasing procedures and financial authorisations to provide both internal and external customers with a high level of service	BAU	Finance, Property & Performance	✓
Ensure rates and charges for the financial year are levied and collected in accordance with relevant legislation, while also incorporating rates assistance provisions	BAU	Finance, Property & Performance	✓
Provide effective management of investment portfolio to maximise return within our policy and risk framework	BAU	Finance, Property & Performance	✓
Increase ticket sales and optimise financial returns, including through use of a range of commercial models for venue hires and partnerships	BAU	Civic Services	✓
Operate commercial function and event venues to full capacity and maximise profit	BAU	Civic Services	✓

4.1.2 Integrated planning and reporting

ACTION	STRATEGY/ BAU	SERVICE UNIT	STATUS
Coordinate and report on the Capital Works Program	BAU	Project Management Office	✓
Develop Delivering Newcastle 2040 and quarterly reports through inclusive, Integrated Planning and Reporting and collaboration across the organisation	BAU	Finance, Property & Performance	✓
Build awareness across councillors and the community around Newcastle 2040 and its impact on the work we do in response to community needs	BAU	Finance, Property & Performance	✓
Integrate Newcastle 2040's vision and priorities into all that we do, through structured and supported planning and monitoring and reporting activities across CN	BAU	Finance, Property & Performance	✓
Build awareness and capabilities around Integrated Planning and Reporting and strategic planning with a corporate online hub	BAU	Finance, Property & Performance	✓
Manage CN's privacy management obligations	BAU	Legal & Governance	✓
Deliver ongoing best practice improvements and embed Corporate Governance Framework	BAU	Legal & Governance	✓

BAU: business as usual
Strategy: includes actions that deliver against CN's strategies

4.1.3 Aligned and engaged workforce

ACTION	STRATEGY/ BAU	SERVICE UNIT	STATUS
Develop and implement a Psychological Claims and Injury Management Pathway	BAU	People & Culture	✓
Develop and implement an end-to-end process for managing return to work	BAU	People & Culture	✓
Develop and deliver Safety Education Program	Strategy	People & Culture	✓
Review and assess Work Health and Safety Management System to ensure it remains fit for purpose	Strategy	People & Culture	✓
Embed a resource-to-risk approach to SWP service delivery	BAU	People & Culture	✓
Continue to develop and deliver Safety Culture Program	Strategy	People & Culture	✓
Build CN's employer brand	BAU	People & Culture	✓
Build resource planning capability and ensure resourcing is aligned with Newcastle 2040 objectives	BAU	People & Culture	✓
Implement Remuneration Governance Framework	BAU	People & Culture	✓
Develop and implement Cultural Strategy 2016-2019	Strategy	People & Culture	○



CN has boosted its annual apprentice and trainee intake for 2024 with 18 roles on offer to help people take their first career steps. CN apprentice Shanden Mills, Lord Mayor Nuatali Nelmes, CN City Greening Coordinator Tahn Woolmer and CN Tree Planting Officer Michael Linsley.

Measures



Priority 4.2 Trust and transparency

4.2.1 Genuine engagement

ACTION	STRATEGY/ BAU	SERVICE UNIT	STATUS
Deliver information to the community to enable active participation in CN's decision-making process	Strategy	Media, Engagement, Economy & Corporate Affairs	✓
Deliver best practice engagement that is inclusive and accessible	Strategy	Media, Engagement, Economy & Corporate Affairs	✓
Deliver best practice community engagement services that build trust in the process	Strategy	Media, Engagement, Economy & Corporate Affairs	✓
Ensure delivery of engaging communications and promotional campaigns to promote services and offerings	Strategy	Museum Archive Libraries & Learning	✓
Regularly engage with, listen to and encourage participation of stakeholders	Strategy	Waste Services	✓
Provide important and relevant updates to stakeholders regarding development, planning and regulations	BAU	Planning, Transport & Regulation	✓



Inspirational Matildas midfielder receives Key to the City of Newcastle Lord Mayor Nuatali Nelmes with Matildas midfielder Emily Van Egmond and her father Gary following the presentation of the Key.

4.2.2 Shared information and celebration of success

ACTION	STRATEGY/ BAU	SERVICE UNIT	STATUS
Develop and implement communication campaigns using a range of channels and media to support achievement of strategic priorities	BAU	Media, Engagement, Economy & Corporate Affairs	✓
Identify high-risk projects and ensure strategic communication and stakeholder management plans are in place to manage risks to reputation	BAU	Media, Engagement, Economy & Corporate Affairs	✓
Deliver impactful centralised marketing programs to improve commercial and community outcomes for major events, key projects, CN cultural institutions and corporate marketing through integrated planning and strategic partnership	BAU	Media, Engagement, Economy & Corporate Affairs	✓
Foster a positive reputation and community goodwill by effective management of the CN brand	BAU	Media, Engagement, Economy & Corporate Affairs	✓
Celebrate our achievements through our Annual Report	BAU	Finance, Property & Performance	✓
Review the performance of CN using comparison analysis of local government and collaborate to improve processes	BAU	Finance, Property & Performance	✓
Maintain and deliver best practice information management including access, storage and release of information	BAU	Legal & Governance	✓
Deliver ongoing best practice improvements and embed Enterprise Risk Management Framework	BAU	Legal & Governance	✓
Implement Leadership Capability Framework and facilitate Leadership Development Program	Strategy	People & Culture	✓

4.2.3 Trusted customer experience

ACTION	STRATEGY/ BAU	SERVICE UNIT	STATUS
Oversee corporate website content, including homepage curation and coordination of the editor/champion network	BAU	Media, Engagement, Economy & Corporate Affairs	✓
Develop and deliver a Digital Marketing Strategy to increase online profile and presence	Strategy	Media, Engagement, Economy & Corporate Affairs	✓
Provide regular and meaningful communications to both internal and external customers around customer experience improvement initiatives and customer satisfaction/success indicators	BAU	Customer Experience	✓
Manage and expand Voice of the Customer Program to ensure effective operation of closed-loop feedback	BAU	Customer Experience	✓
Deliver complaints-handling management and reporting	BAU	Customer Experience	✓
Continue to provide high-quality, responsive customer service delivery to the community via phone, digital and counter channels	BAU	Customer Experience	✓
Design customer-centred experiences, digitised services and ways of working to empower customers and employees	Strategy	Customer Experience	○*
Embed a trusted customer experience and a collaborative approach with both internal and external stakeholders through best practice property management	BAU	Finance, Property & Performance	✓
Deliver business partnering excellence by building on a foundation of trust and recommending solutions that sustainably enable CN's strategic priorities	BAU	Information Technology	✓
Implement business partnering and consistent project management to facilitate delivery of CN's strategic priorities	BAU	Information Technology	✓
Develop and implement information security operations to manage and audit IT governance and meet legislation and regulatory compliance requirements	BAU	Information Technology	✓
Provide timely advice and representation in high-risk legal matters supporting the delivery of strategic objectives	BAU	Legal & Governance	✓
Provide an exceptional visitor experience for all customers and stakeholders	BAU	Civic Services	✓
Develop and maintain high-quality customer experiences and satisfaction	BAU	Museum Archive Libraries & Learning	✓

*Five forms have been delivered. An external communications campaign has been developed, following insights from customer research, to raise awareness of CN's self-service capability. The 'your city, your way' campaign launched in October.



Safety first as CN lifeguards prepare students for summer. CN Aquatic Operations & Education Specialist Chris Griffin (left) and lifeguard Sam Biddle (right) with students from Waratah Public School following today's Water Safety Program.

Measures



Priority 4.3 Collaborative and innovative approach

4.3.1 Collaborative organisation

ACTION	STRATEGY/BAU	SERVICE UNIT	STATUS
Explore and deliver partnerships, delivery models and funding opportunities based on greatest benefit for community and customers	Strategy	Waste Services	✓
Support delivery of Capital Works Program through the provision of survey, design, planning, project and program management	BAU	Project Management Office	✓
Strengthen CN's crisis and emergency management capabilities	BAU	Legal & Governance	✓
Maintain a best practice internal audit function in compliance with legislative requirements and Office of Local Government guidelines	BAU	Legal & Governance	✓
Support delivery of CN functions through provision of event services for meetings and civic events	BAU	Civic Services	✓

4.3.2 Innovation and continuous improvement

ACTION	STRATEGY/BAU	SERVICE UNIT	STATUS
Establish data analytics service under City Intelligence Program, aimed at providing evidence-based insights to the business community	Strategy	Media, Engagement, Economy & Corporate Affairs	✓
Maintain operational fleet and plant to provide cost-effective, safe, fit-for-purpose, legislatively compliant assets that support the needs of internal customers in delivering services to the community	BAU	Civil Construction & Maintenance	✓
Develop and implement Business Excellence Framework and continuous improvement program through a holistic Service Review program of work	BAU	Finance, Property & Performance	✓
Drive cost savings and improved customer service levels through growth in use of electronic rates emailing platform	BAU	Finance, Property & Performance	✓
Undertake Service Review of land transactions and other dealings to implement innovation and continuous improvement	BAU	Finance, Property & Performance	✓
Continue optimisation of Human Resource Information System (TechOne)	BAU	People & Culture	✓
Expand Employee Listening Strategy and engagement with staff	Strategy	People & Culture	✓
Pursue best practice service delivery through a process of continuous improvement and investments in technology	BAU	Civic Services	✓
Identify process improvements to optimise processing timeframes and continue to improve customer experience	BAU	Planning, Transport & Regulation	✓

4.3.3. Data-driven decision-making and insights

ACTION	STRATEGY/BAU	SERVICE UNIT	STATUS
Manage, improve and refine fleet assets through strategic planning, data-driven decision-making, alignment with CN's sustainability goals and legislation to meet service requirements	BAU	Assets & Facilities	✓
Develop a Property Investment Strategy that is underpinned by data-driven decision-making and financial sustainability	BAU	Finance, Property & Performance	✓
Deliver digital transformation of CN services by leading development of platforms and processes to maximise benefit of digital investments	BAU	Information Technology	✓
Create a data-led organisation where business intelligence actively informs decision-making and future strategy development	BAU	Information Technology	✓
Deliver needs-based solution architecture that directly links to CN's priorities, objectives and governance requirements	BAU	Information Technology	✓
Establish safety and wellbeing KPIs	Strategy	People & Culture	✓
Automate Performance and Development process	BAU	People & Culture	○
Develop an organisation position matrix and critical skills inventory	BAU	People & Culture	○*
Build digital literacy for digital enablement	BAU	People & Culture	✓

*This Action has not started yet.

Measures





Budget review statement

Income and expenses budget review statement

Result for the financial quarter ending 30 September, 2023.

FULL YEAR ADOPTED BUDGET	INCOME STATEMENT	SEPTEMBER 2023 RECOMMENDED CHANGES	PROJECTED YEAR END RESULT 2023/24	YTD ACTUAL RESULT (SEPTEMBER)
\$ '000		\$ '000	\$ '000	\$ '000
Income from continuing operations				
219,236	Rates & annual charges	-	219,236	54,809
117,640	User charges & fees	137	117,777	28,959
9,341	Other revenues	269	9,610	2,646
21,220	Grants & contributions - operating	1,055	22,275	637
38,626	Grants & contributions - capital	389	39,015	2,442
9,315	Interest & investment revenue	558	9,873	3,501
7,089	Other income	24	7,113	879
422,467	Total income from continuing operations	2,432	424,899	93,872
Expenses from continuing operations				
131,648	Employee benefits & on-costs	(844)	130,804	31,289
112,483	Materials & services	1,659	114,142	20,759
4,039	Borrowing costs	-	4,039	1,020
69,601	Depreciation & amortisation	-	69,601	17,746
52,147	Other expenses	815	52,962	12,766
7,002	Net loss from the disposal of assets	-	7,002	1,094
376,920	Total expenses from continuing operations	1,630	378,550	84,675
45,547	Operating result from continuing operations	802	46,349	9,197
6,921	Net operating result for the year before grants and contributions provided for capital purposes	413	7,334	6,756

Income statement variations

Result for the financial quarter ending 30 September, 2023.

	RECOMMENDED CHANGES (\$'000)	EXPLANATION
INCOME		
Rates & annual charges	-	
User charges & fees	137	\$0.2m increase in revenue related to State Road Maintenance \$0.1m increase in expected revenue from RAAF Airshow \$0.1m reduction in Private Tree Planting works delivered in prior year \$0.1m reduction in revenue from Beresfield Swim Centre
Other revenues	269	\$0.4m increase for insurance claims received \$0.1m reduction in revenue from Summerhill solar farm
Grants & contributions - operating	1,055	\$0.8m for Round for of Federal Government Local Roads and Community Infrastructure Program
Grants & contributions - capital	389	\$0.3m increase in Transport grants \$0.2m in Recreation and Culture grants
Interest & investment revenue	558	Increase in interest income based on forecast market conditions
Other income	24	
Income from continuing operations	2,432	
EXPENSES		
Employee benefits & on-costs	(844)	Reductions related to current vacancies
Materials & services	1,659	\$0.6m increase related to Major Events including RAAF Airshow & Matilda Live sites \$0.5m increase in external labour \$0.2m increase for consultants related to Regional Housing planning \$0.1m increase in Street lighting costs
Borrowing costs	-	
Depreciation & amortisation	-	
Other expenses	815	\$0.8m increase in Fire & Emergency Services levy
Net loss from the disposal of assets	-	
Expenses from continuing operations	1,630	

Capital statement

Result for the financial quarter ending 30 September, 2023.

		CHANGES		
FULL YEAR ADOPTED BUDGET		RECOMMENDED SEPTEMBER 2023	PROJECTED YEAR END RESULT 2023/24	ACTUAL YTD
\$'000		\$'000	\$'000	\$'000
CAPITAL FUNDING				
70,107	General fund contribution to capital	412	70,519	21,895
2,410	Stormwater Management Service Charge	-	2,410	603
32,127	Capital Grants & Contributions	389	32,516	2,442
719	Proceeds from the sale of assets	-	719	10
(3,339)	Net Loans Borrowings / (Repayments)	-	(3,339)	(835)
102,024	Funding available for capital expenditure	801	102,825	24,115
CAPITAL EXPENDITURE				
42,956	Asset Renewal	-	42,956	6,146
41,764	New / Upgrade	-	41,764	7,256
84,720	Total capital expenditure	-	84,720	13,402
17,304	Transfer to or (Draw down on) reserves	801	18,105	10,713

Cash and investments budget review statement

Result for the financial quarter ending 30 September, 2023.

	OPENING BALANCE IN NOTE C1-3	TRANSFERS TO	TRANSFERS FROM	RECOMMENDED CHANGES FOR COUNCIL RESOLUTION	CLOSING BALANCE IN NOTE C1-3
	01/07/2023	2023/24	2023/24	(Sep QBR)	30/06/2024
	(000's)	(000's)	(000's)	(000's)	(000's)
Unrestricted (Available Cash)	26,512	-	(912)	135	25,735
EXTERNAL RESTRICTIONS					
Included in Liabilities					
Security bonds, deposits and retentions	1,412	-	-	-	1,412
Special purpose unexpended grants - general fund	10,097	-	(4,073)	-	6,024
Other					
Specific purpose unexpended grants (recognised as revenue) - general fund	7,697	-	(7,697)	-	-
Developer Contributions	22,506	-	(3,478)	-	19,028
Contributions to Specific works	344	-	(344)	-	-
Domestic Waste Management	11,611	-	(2,410)	-	9,201
Bequests and Donations	1,542	-	-	-	1,542
Special Benefit rates	1,254	-	(49)	(103)	1,102
Rawson Crown Land Reserve	3,376	-	(810)	(148)	2,418
Building Better Cities	324	-	(80)	-	244
Community Facilities fund	243	-	-	-	243
Childcare sinking fund	806	212	-	-	1,018
Deferred Salary Scheme	418	119	(159)	-	378
Total Externally restricted	61,630	331	(19,100)	(251)	42,611
Internal Restrictions					
Works program: New and upgrade	14,722	-	70,273	(1,438)	83,557
Works Program: Infrastructure Agreed level of service	173,478	-	-	-	173,478
Works program: Specific projects	19,614	-	(122)	1,355	20,847
Works carried forward	10,834	-	(10,834)	-	-
Waste Management: Remediation Provision	61,972	-	(22,332)	-	39,640
Employee Leave Entitlements	13,451	-	-	-	13,451
Superannuation: Defined Benefits	655	-	-	-	655
Workers Compensation: Self insurance	10,001	-	-	-	10,001
Newcastle Airport Partnership	9,717	-	-	-	9,717
Local Committees and childcare	628	-	-	-	628
Inland Pools Reserve Fund	-	-	-	1,000	1,000
Total Internally restricted	315,072	-	36,985	917	352,974
Total Restricted	376,703	331	17,885	666	395,584
Total cash and investments	403,215				421,319

Works program summary

FULL YEAR ADOPTED BUDGET \$,000	PORTFOLIO/PROGRAM	RECOMMENDED CHANGES SEPTEMBER 2023 (\$,000)	SEPTEMBER ACTUAL YTD (\$,000)
35,313	City Infrastructure - Assets & Facilities	-	4,528
260	Buildings - Council Support Services	-	126
150	Public Toilets	-	-
140	Retaining walls	-	135
7,780	Bridges	-	109
1,160	Footpaths	-	361
1,983	Roadside Furniture	-	151
7,240	Road Rehabilitation	-	458
7,500	Road Resurfacing	-	2,184
300	Parking Infrastructure	-	-
4,975	Stormwater System	-	920
3,825	Fleet Replacement	-	83
5,000	Planning & Environment - Transport	-	1,021
3,610	Cycleways *	-	227
250	Pedestrian Access and Mobility Plan (PAMP)	-	462
1,140	Local Area Traffic Management (LATM)	-	332
13,458	Planning & Environment - Environment & Sustainability	-	1,126
1,050	Blackbutt Reserve	-	45
254	Flood Planning	-	19
8,009	Coast, Estuary and Wetlands	-	391
1,810	Bushland and Watercourses	-	117
1,660	Street and Park Trees	-	338
675	Sustainability & Climate	-	215
6,400	Corporate Services	-	734
800	Digital Enablement	-	42
5,600	Core Systems Development & Maintenance	-	692
41,160	City Shaping	-	5,251
36,500	Citywide	-	4,772
4,660	Summerhill	-	480

FULL YEAR ADOPTED BUDGET \$,000	PORTFOLIO/PROGRAM	RECOMMENDED CHANGES SEPTEMBER 2023 (\$,000)	SEPTEMBER ACTUAL YTD (\$,000)
22,334	Creative & Community Services	-	2,719
7,450	Aquatics	-	1,194
349	Community Buildings	-	86
920	Civic Venues / Civic Services	-	195
12,245	Recreation & Sport	-	1,053
475	Economic Development	-	10
150	Art Gallery	-	1
745	Museum / Libraries / Historic Fort Scratchley	-	180
4,375	Waste Services	-	3,628
4,375	Waste Management	-	3,628
9,216	City Infrastructure - Revitalisation	-	1,837
5,620	City Centre	-	593
2,300	Coastal	-	541
1,296	Urban Centres	-	703
137,255	Total Works Program	-	20,845

* The total spend on Cycleways across all Programs YTD totals \$0.6m against the projected year end result of \$4.9m.

Contracts

Result for the financial quarter ending 30 September, 2023.

CONTRACTOR	CONTRACT DETAIL	CONTRACT VALUE	COMMENCEMENT DATE	ESTIMATED COMPLETION	BUDGETED (Y/N)
Central Waste Station Pty Ltd	Waste Disposal at SWMC – Central Waste Station	\$96,307,200	7/1/2023	6/30/2033	Y
Benedict Recycling Pty Ltd	Waste Disposal at SWMC – Benedict Recycling	\$87,486,300	7/1/2023	6/30/2033	Y
Port Stephens Council	Waste Disposal at SWMC – Port Stephens Council	\$25,341,272	7/1/2023	6/30/2027	Y
Technology One Pty Ltd	Technology One SaaS Platform	\$10,676,893	9/28/2023	6/30/2030	Y
Brefni Pty Ltd	Stockton Buried Protection Structure #1	\$4,028,358	8/5/2023	8/5/2024	Y
DATA#3 Limited	Microsoft Software Licens-ing	\$3,960,000	7/1/2023	8/31/2026	Y
Ironbuilt Infrastructure Pty Ltd	Replace 2 X Leachate Pits	\$1,351,703	7/23/2023	10/31/2023	Y
Greenstar Water Solutions Pty Ltd	Foreshore Park Irrigation Early Works	\$435,051	8/8/2023	10/20/2023	Y
Department of Primary Industries – Lands	Throsby Creek Emergency Riverwall Repair	\$266,795	7/31/2023	6/30/2024	Y
Bluefit Pty Ltd	Management Services for Beresfield Swimming Centre	\$200,200	9/1/2023	6/30/2024	N
Reino International Pty Ltd	Licence Plate Recognition System	\$196,597	7/1/2023	6/30/2025	N
Newcastle Weighing Services Pty Ltd	Waste Management System for Summerhill	\$173,649	7/1/2023	7/1/2026	Y
The Missing Link Security Pty Ltd	CrowdStrike Endpoint Protection	\$141,692	7/2/2023	7/2/2024	N

- Notes:
- 1. Minimum reporting contract value is \$50,000.
 - 2. Contracts to be listed are those entered into during the quarter and have yet to be fully performed, excluding contractors that are on Council's preferred suppliers list.
 - 3. Contracts for employment are not included.

Consultancy and legal expenses

Result for the financial quarter ending 30 September, 2023.

EXPENSES	EXPENDITURE YTD \$	BUDGETED
Consultancies	\$3,202,692	Y
Legal	\$214,124	Y

- Notes:
- 1. A consultant is a person or organisation engaged under contract on a temporary basis to provide recommendations or high level specialist or professional advice to assist decision-making by management. Generally it is the advisory nature of the work that differentiates a consultant from other contractors.
 - 2. Where any expenses for consultancy or legal fees (including Code of Conduct expenses) have not been budgeted for, an explanation is to be given. Report on external expenses only (not internal expenses).

City of Newcastle

Report by Responsible Accounting Officer for the quarter ending 30 September 2023.

The following statement is made in accordance with Clause 203(2) of the Local Government (General) Regulations 2005.

It is my opinion that the Quarterly Budget Review Statement for Newcastle City Council for the quarter ended 30 September 2023 indicated that Council's projected financial position at 30 June 2024 will be satisfactory at year end, having regard to the projected estimates of income and expenditure and the original budgeted income and expenditure.

Signed:  Date: 13/11/2023

Name: David Clarke

Responsible Accounting Officer, City of Newcastle



Appendix

Works program

City Infrastructure - Assets & Facilities
Bridges
Boscawen Street Bridge Renewal
Bridge (Pedestrian) Waratah and Jesmond Parks
Bridge and large Culvert repairs
Bridge inspection and load rating
Chinchen St Bridge Renewal
Cottage Creek Bridge replacement
Nelson Street Wallsend Bridge renewal
Pedestrian Bridge Handrail replacement
Pedestrian Bridges Renewal
Buildings - Council Support Services
Engineering Advice General
Rooftop Solar Upgrade program
Structures- Survey
Fleet Replacement
Fleet Replacement Program 2024
Footpaths
Citywide - Minor Footpath renewal
Fernleigh trck - Stairs
Honeysuckle Drv Newcastle - footpath reinstatement
Throsby Creek Shared Pathway Renewal Wickham to Maryville
Union Street Wickham - Footpath Upgrade Wickham Master
Various cycleways - linemarking and signage program
Various Footpath Connectivity
Warabrook Wetland Reserve - Shared Path Renewal
William St Tighes Hill - footpath rehabilitation
Parking Infrastructure
Off Street Car Parks Furniture Renewal
Off Street Car Parks Minor Renewal
Off Street Car Parks Resurfacing
Parking Meter Replacement
Public Toilets
Waratah Park Tennis club Toilets - Demolition and Repla
Retaining walls
Retaining Wall - Perkins Street Newcastle
Retaining wall Renewal - Various
Spruce Street - Batter Protection repair

Works program (continued)

Road Rehabilitation
Citywide - Laneway Renewal
Citywide - Road and Laneway Dedication
Corona St Hamilton East reconstruction
Duncan Cl Elernore Vale - turning facility
Fern St Islington road reconstruction
Harriet St Waratah reconstruction
Hope Street Wallsend - road renewal
Howell St Kotara- road reconstruction
Howell Street Kotara - Road rehabilitation extension betw
Lambton Road New Lambton - Alma Rd to Avondale Rd - Road
Lexington Pde AdamHts - road embankment
Longworth Ave Wallsend - road renewal design
Madison Drive Adamstown Heights - Road Renewal
Margaret St Merewether watermain renewal
Mathieson St Carrington - road renewal design
Memorial Drive The Hill - Road Embankment
Menkens Lane The Hill
Minmi Rd Bunnings Roundabout - intersection upgrade
Mlnmi Rd Wallsend - road upgrade
Old Maitland Road Hexham - road renewal
Pride Ave Lambton - road renewal design
Samdon St Hamilton - road renewal design
Tyrone Road New Lambton reconstruction
Unnamed Laneway off Morehead St Lambton - renewal & upg
Vera St Waratah West
Wall Lane North Lambton
Woodward St Merewether road and embankment
Workshop Way Newcastle Road Renewal and Upgrade
Road Resurfacing
Road Resurfacing - pavement and road roughness testing
Road Resurfacing - site preparation
Road Resurfacing Citywide

Works program (continued)

Roadside Furniture
Albert Street Wickham - Traffic Calming Devices
Banner Holder Removal or Replacement in Newcastle CBD
Bathers Way - furniture renewal and maintenance
Bathers Way - Lighting Renewal
Bulkara St Wallsend - guardrail and footpath
Charlestown Rd Kotara - fence renewal and footpath
City Wide - CN Smart Pole - Renewal
City Wide - CN Smart Pole Inspection and Maintenance
City Wide - Lighting Renewal
City Wide - street lighting assessment
CityWide - Structural Inspection - Art & Monuments in R
Fernleigh Track - fencing, furniture renewal and mainte
Honeysuckle Promenade - Lighting Renewal
Local Centre - furniture renewal and maintenance
Northcott Drive Kotara at Rail Over Bridge - Fencing
Road Furniture - renewal
Roadside Furniture - renewal
Transport Stop Upgrade
Young Street Georgetown - Turton to Parkview St -
Stormwater System
Albert Street Stormwater Design
Chilcott St, Lambton Stormwater Rehabilitation
City Centre Drainage Master Plan
Citywide - stormwater quantity and quality modeling
Citywide - trenchless technology drainage rehab implementati
Coorumbung Road Broadmeadow Drainage Rehabilitation
Creeks and Waterways - inspect erosion and sediment control
Fairfield Avenue New Lambton - Stormwater Design & Construct
Glebe Road Adamstown - Stormwater drainage Upgrade (Sta
Grandview Pde Elmore Vale- Sediment basin and culvert
Low Lying Suburbs- Tide gate rehabilitation
Mayfield East Drainage Design (George St construct & Selwyn
Minmi Rd Detention Basin Fletcher Dam Safety Works and
Mitchell Street Merewether - Stormwater Drainage upgrad
Roe Street Mayfield - Drainage connection laneway
Sandgate Road Birmingham Gardens - Drainage Constructio
Stockton Laneways - Infiltration and unrelieved sags
Stormwater Drainage - Construct Access
Stormwater Drainage - Replace Grates
Tooke St Cooks Hill - Stage 2 Drainage Rehabilitation Design
Union Street Cooks Hill BetweenTooke St and Parkway Ave
Various Headwall & Outlet Rehabilitation
Young Road Lambton Stormwater Project

Works program (continued)

City Infrastructure - Revitalisation
City Centre
2021 Christmas Tree Installation and Removal
HSR - Civic PDP
HSR - East End PDP - (Hunter St Mall)
HSR - Place Activation Initiatives (Signage)
HSR - West End PDP - Stage 2 (Cycleway)
Wickham PDP Implementation
Coastal
Bathers Way - King Edward Park
Bathers Way - South Newcastle
Bathers Way Signage Strategy
Coastal Building Revitalisation Plan
Coastal Revitalisation - Planning
Urban Centres
Darby Street - SASS grant trial
Georgetown Local Centre Renewal
Local Centres - Establishment Maintenance
Local Centres - Facade Improvement Scheme
Local Centres - Feasibility
Local Centres - Orchardtown Rd New Lambton
City Shaping
Citywide
Art Gallery - Expansion
Astra St Remediation
Summerhill
Bulk Earthworks for the Organics Processing Facility
Construction of operational area at SWMC and relocation
Organics Facility
OSD_MRF - Construction - Phase 1 - Shed
Corporate Services
Core Systems Development & Maintenance
Computing Hardware Refresh
Directorate Technology Initiatives
Enterprise Risk
Geographic information system (GIS)
Information Security and Privacy
Kentico Upgrade
Technology Foundations
TechOne CiA migration
Digital Enablement
Customer Experience
Employee Experience
Storing, analysising, governing and understanding data

Works program (continued)

Creative & Community Services
Aquatics
Inland Pools - Minor Infrastructure Renewal Program
Lambton Pool Grandstand Upgrade
Lambton Swimming Centre - Staged facility upgrade and
Newcastle Ocean Baths Upgrade - Stage 2
Newcastle Ocean Baths Upgrade Project
Art Gallery
Art Gallery - cultural asset preservation
Art Gallery Works of Art
Civic Venues / Civic Services
City Hall - Refurbish operational areas
City Hall - Storage of Lord Mayoral gifts
City Hall Fire Dampers Rectification Works and Upgrades
Civic Theatre - Replace Playhouse seating
Civic Theatre - replace technical equipment
Civic Theatre - replace vinyl flooring
Civic Theatre - Upgrade Air Conditioning Units
Civic Theatre- reupholster 50 seats in Stalls & Dress
Fort Scratchley Function Centre -flooring replacement
Community Buildings
Community Buildings refurbishment/renewal
Economic Development
City Analytics Program
Digital Prospectus
Economic Development Strategy Implementation
Newcastle After Dark
Museum / Libraries / Historic Fort Scratchley
Historic Fort Scratchley - Salt and Moisture Levels
Library Resources
Museum - Paving Replacement
Recreation & Sport
Darling St Oval - Grandstand
Fenced Off Leash Dog Areas
Fencing - Sports Grounds
Floodlight Renewal Program
Foreshore Park - All abilities playground & water park
Gregson Park Playground
Park Accessibility Improvement Program
Passmore Oval Grandstand Upgrade
Plans of Management Review
Playground replacement programme
Playground Shade Program
Smith Park - Field Renovation

Works program (continued)

Sportsgrounds - Renew sub surface drainage/irrigation system
Sportsgrounds - Renewal of lighting poles
Western Corridor Active Hub - Wallsend
Planning & Environment - Environment & Sustainability
Blackbutt Reserve
Blackbutt Planning and Design
SRV - Blackbutt CARA Kiosk - Detailed Design
Bushland and Watercourses
Aries Way Reserve - creek rehabilitation
Blackbutt Reserve - bushland regeneration
Bush Fire Prone Land Mapping (BFPLM)
Community Education at environment rehabilitation worksites
Condition and Investigation - Natural Assets
Environmental Management System (EMS) - Develop and Imp
Environmental Project Delivery Support
Green Newcastle Strategy
Inland Clifline Rehabilitation - Waratah West
Ironbark Ck Rehabilitation - St 5 - 7
Jesmond Bushland Complex Rehabilitation
Maryland Creek - Rehabilitation
Natural Asset Management Systems - development
Natural Connection - Newcastle's Healthy Catchments Program
North Lambton Catchment- Drainage and creek design
Various Creeks - rehabilitation
Various Reserves - bushland regeneration
Waterdragon Ck Kotara Park- Riparian rehabilitation
Wentworth Creek Rehabilitation - Stage 1 of 2
Coast, Estuary and Wetlands
Astra St EEC Action Plan
Buried Protection Structures - Barrie Crescent Stockton
Buried Protection Structures - Zone 2 (2 sites - Sth Mi
Coastal Clifffline Rehabilitation Monitoring
Coastlal Cliff line Rehab Stabilisation- Kilgour and Nobbys
Coastline - dune preservation and restoration
Hunter Estuary Coastal Management Program Preparation
Hunter River Foreshore Stockton - revegetation
Jersey Road Sandgate - Wetland Rehabilitation
Lloyd St Res Merewether-Littoral Rainforest Restoration
Market Swamp Wetland- rehabilitation design and construct
Mitchell St Sea Wall repair Stockton
Newcastle Coastal Management Program Investigation and
Newcastle South Seawall Rehabilitation
Newcastle Sth Clifffline (North of skate park)

Works program (continued)


Southern Beaches Coastal Management Program Preparation
Stockton Beach Nourishment
Stockton Coastal Works
Stockton King St Breakwater Protection Structure
Stockton Riverwall- Stage 6 -9 design & construct - Stage 7
Stockton SLSC Seawall Maintenance - Zone 1
Various Seawalls Monitoring and Works
Various Sites - coastal revegetation
Wetlands- Rehabilitation design and construct
Flood Planning
Amplification of Hunter Water Drainage Network
Flash Flood Alert Service - Operation & Maintenance
Flood Education Campaign
Sea and Groundwater Level Monitoring
Update existing flood studies to 2019 AR&R methodology
Upgrade of major flood evacuation routes
Street and Park Trees
Citywide - Community Urban Forest Program
Citywide - tree audit for all attributes
Citywide Tree - inspection and monitoring
Gateway Tree Planting - Hannel Street Wickham & Maryvil
Living Streets Campaign
Park and Street Tree - Life extension program
SAPTRP - Street Tree Planting
Street Verge Gardens
Sustainability & Climate
2020 CWMAP review and update

Works program (continued)

Electric Vehicle Trial - Pool Car replacement
Energy Savings Projects
Smart Moves Newcastle
Planning & Environment - Transport
Cycleways
CP - Chatham Road and Clyde Street, Hamilton North
CP - Chinchon St Islington - Scholey St to Maitland Rd (Isli
CP - Cycleways Education and Promotion
CP - Cycleways Investigation & Development
CP - Glebe Rd Adamstown to Newcastle West
CP - Hunter Street Trial Cycleway - National Park St, N
CP - Lambton Park to Croudace St
CP - Maud Street - University to City Centre Cycleway
CP - Mayfield Precinct - Feasibility Study & Concept Design
CP - Parkway Avenue to Honeysuckle Foreshore, Newcastle
CP - Parkway Avenue, Hamilton South - Roundabout Bypass
CP - Richmond Vale Rail Trail
CP - Shortland to Tarro Cycleway
Cycleways Program Management
H23 Overpass to Mordue Parade
Local Area Traffic Management (LATM)
LATM - Croudace Road at Garsdale Avenue - Traffic Contr
LATM - Design and Construction of Traffic Calming Devic
LATM - Glebe Rd & Park Ave, Adamstown - Traffic Control Sign
LATM - Traffic Modelling, Local Area Traffic Management
Pedestrian Access and Mobility Plan (PAMP)
PAMP - Design and Construction - PPN Projects
PAMP/LATM Minor Works
Waste Services
Waste Management
Cell 10 Landfill Design and Construction
Domestic Bins (Repair, Replacement, New Deliveries and
Public Place Bins (Replacement and Upgrades)
Stormwater and Leachate Management - Design & Review of
Summerhill site Upgrades
Summerhill Waste Management Centre - road rehabilitation
SWMC Landfill Rehabilitation Program

Measures

Measures help us understand how well CN is performing. They also allow for evidence-based decision-making to inform other stages in our planning cycle.



Service and program measures

The impact of our individual services and programs

KEY FOR BASELINE DATA	
All baseline data is 2020-2021 unless otherwise stated	
*New measure	New measure means targets will be set after 12 months of data collection.
**Satisfaction reasoning	A mean score above 3.0 indicates more satisfaction than dissatisfaction within the community. CN aims for satisfaction with these assets and services.
	A mean score above 3.5 indicates high satisfaction. CN aims for higher satisfaction for these higher-performing assets and services.

N2040 THEME: LIVEABLE NEWCASTLE			N2040 THEME: LIVEABLE NEWCASTLE		
PRIORITIES	SERVICE AND PROGRAM MEASURES	TARGET	BASELINE	WHY THIS IS IMPORTANT	SOURCE
1.1 Enriched neighbourhoods and places	<ul style="list-style-type: none">Level of community satisfaction with sporting facilitiesLevel of community satisfaction with beaches and beach facilitiesLevel of community satisfaction with parks and recreational areasNumber of community seasonal sport bookingsBeach usage attendancePool attendance	<ul style="list-style-type: none">Greater than 3.5**Greater than 3.5**Greater than 3.5**New measure*MaintainMaintain	<ul style="list-style-type: none">3.84.03.71141,411,258336,703	<p>The community satisfaction survey's intention is to provide insights into the factors that drive satisfaction within the community and show CN the effectiveness of its service delivery in meeting community expectations. Satisfaction is measured out of 5 with a mean score above 3.0 indicating more satisfaction than dissatisfaction within the community in relation to a particular CN attribute.</p> <p>CN is responsible for the development, maintenance and management of many community assets including parks, gardens, playgrounds and ovals, beaches and pools, to name just a few. Within these spaces there may be a number of facilities including BBQs, fitness and play equipment, toilets, walking tracks, irrigation systems and water features.</p> <p>This data provides insight into how people use our city over time and informs decision-making that responds to current and future needs and behaviours of our community.</p>	<ul style="list-style-type: none">CN - Satisfaction SurveyCN - Satisfaction SurveyCN - Satisfaction SurveyCN - Booking SystemBlueFit Report
1.2 Connected and fair communities	<ul style="list-style-type: none">Number of awareness-raising initiatives relating to inclusionLibrary program, event and exhibition attendanceVisits to Library physical service pointsLevel of community satisfaction with LibrariesNumber of Home Library Service items/membersNumber of Library loans	<ul style="list-style-type: none">New measure*Increase by 5%Increase by 5%Greater than 3.5**Increase by 5%Increase by 5%	<ul style="list-style-type: none">New measure*54,964263,4954.025,000/277769,329	<p>These insights help us understand the needs of our audiences to support more effective planning, engagement and design outcomes for our program, events and exhibitions.</p> <p>CN is responsible for the management and maintenance of many community assets across the LGA, including libraries. Within these spaces there may be a number of facilities, services and programs. This data provides insight into how people use our libraries over time and informs decision-making that responds to current and future needs and behaviours of our community.</p> <p>The community satisfaction survey's intention is to provide insights into the factors that drive satisfaction within the community and show CN the effectiveness of its service delivery in meeting community expectations. Satisfaction is measured out of 5 with a mean score above 3.0 indicating more satisfaction than dissatisfaction within the community in relation to a particular CN attribute.</p> <p>The Home Library Service is for anyone who isn't physically able to make it to a branch because of a disability, illness or limited mobility. This insight helps us understand the needs of our community and plan our programs for the future, but also highlights the important of connection outside of our assets.</p>	<ul style="list-style-type: none">CN dataCN - Library dataCN - Library dataCN - Satisfaction SurveyCN - Library dataCN - Library data

N2040 THEME: LIVEABLE NEWCASTLE			N2040 THEME: LIVEABLE NEWCASTLE		
PRIORITIES	SERVICE AND PROGRAM MEASURES	TARGET	BASELINE	WHY THIS IS IMPORTANT	SOURCE
1.3 Safe, active and linked movement across the city	<ul style="list-style-type: none">• Level of community satisfaction with footpaths• Level of community satisfaction with roads• Distance of shared paths renewed• Distance of roads renewed• Number of bike parking spaces within local centres	<ul style="list-style-type: none">• Greater than 3**• Greater than 3**• 800m• 4.2km• N/A* <p>*Data not available</p>	<ul style="list-style-type: none">• 3.1• 3.2• 800m• 4.2km• N/A* <p>*Data not available</p>	<p>The community satisfaction survey's intention is to provide insights into the factors that drive satisfaction within the community and show CN the effectiveness of its service delivery in meeting community expectations. Satisfaction is measured out of 5 with a mean score above 3.0 indicating more satisfaction than dissatisfaction within the community in relation to a particular CN attribute.</p> <p>Walking and cycling are basic, affordable and clean forms of travel available to almost all ages and groups in society. In Newcastle, though private cars are the dominant mode for commuting and, indeed, all trip purposes, the large majority of trips involve distances that could reasonably be undertaken by walking or cycling. Coupled with large areas of relatively flat topography and Newcastle's favourable climate, potential for mode substitution is high, and this indicator assesses the perceived walkability and cycle-friendly nature of our city.</p>	<ul style="list-style-type: none">• CN – Satisfaction Survey• CN – Satisfaction Survey• CN data• CN data• CN data
1.4 Innovative and connected city	<ul style="list-style-type: none">• Number of heritage collection items digitised• Number of Pay by Phone parking transactions• Number of webchat conversations• Number of e-Library loans• Level of community satisfaction with the city's innovation	<ul style="list-style-type: none">• 10,000 per annum• Increase by 10%• Increase by 10%• Maintain• Greater than 3.5**	<ul style="list-style-type: none">• 67,027 items• 851,827• 1,546• 105,773• 3.5	<p>For more than 60 years the Library has been committed to collecting and documenting the story of Newcastle and the Hunter. Over that time the Library has acquired an extensive and important collection of books, documents, archives, maps, pictures and photographs that document the story of Newcastle. The Library has a number of collections that feature rare, unique and notable items. Many of the items in these collections have come to the Library through the generous donations of members of the community. Key to accessing this information is the use of new digital technologies to make old information more widely accessible and able to be reused.</p> <p>Increased digital uptake will support improvements in living standards, ensuring we remain globally competitive and are well positioned to protect our interests. Greater adoption of digital technology in a secure and trusted environment is one of the drivers of liveability.</p> <p>The community satisfaction survey's intention is to provide insights into the factors that drive satisfaction within the community and show CN the effectiveness of its service delivery in meeting community expectations. Satisfaction is measured out of 5 with a mean score above 3.0 indicating more satisfaction than dissatisfaction within the community in relation to a particular CN attribute.</p>	<ul style="list-style-type: none">• CN – Library data• CN data• CN data• CN data• CN data• CN – Satisfaction Survey

N2040 THEME: SUSTAINABLE NEWCASTLE			N2040 THEME: SUSTAINABLE NEWCASTLE		
PRIORITIES	SERVICE AND PROGRAM MEASURES	TARGET	BASELINE	WHY THIS IS IMPORTANT	SOURCE
2.1 Action on climate change	<ul style="list-style-type: none">• Installed lighting to be LED• CN reduction in electricity use• Number of EV chargers available to the community• Level of community satisfaction with climate action	<ul style="list-style-type: none">• 100% of all installed lighting to be LED by 2025• 30% reduction by 2025• All key sites throughout the city• Greater than 3**	<ul style="list-style-type: none">• 5,000• 20.8% progress to date• 4 public EV charging sites (7 chargers, 11 charging bays)• 3.2	<p>LED lighting produces less waste light and more useful lumens than other lighting technologies. By replacing all the lighting in our LGA with LEDs, we will see as much as a 60% to 70% improvement in our overall energy efficiency.</p> <p>Saving electricity reduces energy costs, as well as how much carbon dioxide is released into the atmosphere.</p> <p>Transport emissions play a significant role in our city's carbon emissions, as well as air and noise pollution, which will be reduced through the uptake of electric vehicles.</p> <p>The community satisfaction survey's intention is to provide insights into the factors that drive satisfaction within the community and show CN the effectiveness of its service delivery in meeting community expectations. Satisfaction is measured out of 5 with a mean score above 3.0 indicating more satisfaction than dissatisfaction within the community in relation to a particular CN attribute.</p>	<ul style="list-style-type: none">• TBA• Ironbark Sustainability snapshotclimate.com.au• CN data• CN – Satisfaction Survey
2.2 Nature-based solutions	<ul style="list-style-type: none">• Tree vacancies identified in Tree Asset Management System (TAMS)• Number of plants used in urban forest planting (CN open space) annually• Level of community satisfaction with the city's wetlands and estuary• Level of community satisfaction with greening and tree preservation• Level of community satisfaction with the city's bushland and waterways	<ul style="list-style-type: none">• 85% of vacancies to be planted by 2045. 100% of vacancies to be planted by 2060 (TAMS)• 4 areas per annum• Greater than 3.5**• Greater than 3.5**• Greater than 3.5**	<ul style="list-style-type: none">• 63,000 vacant tree spots at 31/01/2022 (TAMS) reducing by effective 500 p.a.• 4 areas per annum• 3.6• 3.4• 3.5	<p>The overall performance of tree planting needs to be a comparison between the trees planted vs the trees being removed within the same timeframe to be a true record. Vacancies are measured because when vacancies reduce, there is a net gain in trees.</p> <p>Native plants play a very important role in our ecosystems. They are more favourable for supporting local wildlife and have evolved for survival. Consequently, they tend to be more naturally adapted to local growing conditions and often require fewer inputs (for example, fertiliser or water) for successful establishment, and this can mean reduced maintenance.</p> <p>CN plays a role in the provision of green, blue and wild spaces for the health and enjoyment of the community. Community satisfaction with these spaces is examined in the annual satisfaction survey with the aim to increase satisfaction scores year on year. Our community should be satisfied with the environmental assets and services that CN provides.</p>	<ul style="list-style-type: none">• TAMS• CN data• CN – Satisfaction Survey• CN – Satisfaction Survey• CN – Satisfaction Survey

N2040 THEME: SUSTAINABLE NEWCASTLE				N2040 THEME: SUSTAINABLE NEWCASTLE	
PRIORITIES	SERVICE AND PROGRAM MEASURES	TARGET	BASELINE	WHY THIS IS IMPORTANT	SOURCE
2.3 Circular economy	<ul style="list-style-type: none">• Tonnes of recyclables recovered• Level of community satisfaction with green waste collection• Tonnes of municipal waste material landfilled	<ul style="list-style-type: none">• Maintain• Greater than 3.5**• Maintain	<ul style="list-style-type: none">• 31,928.77 tonnes• 3.7• 52,344 tonnes	<p>Reducing waste conserves space in our landfills and reduces the need to build more landfills, which take up valuable space and are a source of air and water pollution.</p> <p>By reducing our waste, we are also conserving our resources. Resources like aluminium, petroleum and trees are all used to make new materials such as cans, plastic bags and paper packaging.</p> <p>Less energy is used to recycle materials as opposed to creating new materials. The manufacturing of consumer goods is a process that consumes a lot of energy, so by limiting the amount of new resources required, a large amount of energy can be saved.</p> <p>By recycling, reusing and reducing the amount of waste we have, we are helping to build a more sustainable future for all. We only have a limited amount of natural resources on this planet and a limited capacity to process waste, so it's important to do our part each day towards a better future.</p> <p>The community satisfaction survey's intention is to provide insights into the factors that drive satisfaction within the community and show CN the effectiveness of its service delivery in meeting community expectations. Satisfaction is measured out of 5 with a mean score above 3.0 indicating more satisfaction than dissatisfaction within the community in relation to a particular CN attribute.</p> <p>Local businesses are the lifeblood of our community. Supporting local businesses creates jobs, reduces carbon footprints, injects money into the local economy and promotes a sense of community.</p> <p>CN commits to the preference of local businesses when considering the acquisition of goods and services.</p>	<ul style="list-style-type: none">• CN data*• CN - Satisfaction Survey• CN data <p><i>* Includes kerbside collection, bulk waste and drop-off</i></p>

N2040 THEME: CREATIVE NEWCASTLE			N2040 THEME: CREATIVE NEWCASTLE		
PRIORITIES	SERVICE AND PROGRAM MEASURES	TARGET	BASELINE	WHY THIS IS IMPORTANT	SOURCE
3.1 Vibrant and creative city	<ul style="list-style-type: none">• Number of events delivered• Number of event licences processed• Number of ticketed attendance at Civic Theatre and City Hall• Growth in business tourism• Level of community satisfaction with promotion of tourism• Level of community satisfaction with entertainment and events• Number of social media followers across all CN platforms• Social media reach on the CN corporate channel	<ul style="list-style-type: none">• Increase by 10%• Increase by 10%• Increase by 10%• New measure*• Greater than 3.5**• Greater than 3.5**• New measure*• Increase by 5%	<ul style="list-style-type: none">• 252 (2021/22)• 462/240• 101,259• \$219m (2019)• 3.4• 3.7• 285,846 (Q1 2022)• 225,259 (Q1 2022)	Success begins with getting to know your audience members – what motivates them, what makes them engage or disengage. Tailoring your event design to create meaningful, personal connections with attendees will ultimately help you drive behaviour change and create value for your business. An event is recognised as having a low, medium, high or major impact based on numbers of attendees. Minor: 1 to 250, Low: 250 to 2,500, Medium: 3,000 to 6,000, High: 6,500 to 10,000, Major: 10,000 to 25,000 + Multiple Venues. Events and performances are a key component of developing a sense of community and pride, generating economic growth, accentuating natural and man-made assets, and giving the city its identity, both regionally and nationally. In addition to being one of the key drivers of the tourism industry's development, the business events sector is an important generator of income, employment, innovation and investment. Individual LGA profiles are no longer available; consequently Newcastle data is incorporated into Hunter Region data. The community satisfaction survey's intention is to provide insights into the factors that drive satisfaction within the community and show CN the effectiveness of its service delivery in meeting community expectations. Satisfaction is measured out of 5 with a mean score above 3.0 indicating more satisfaction than dissatisfaction within the community in relation to a particular CN attribute. We use social media to engage and communicate with our community. We can measure how people are interacting with us and if our message resonates with them. We can also use social media to learn from our customers and community about how we can improve their experience. 'Reach' provides a meaningful measure, as it looks at how many individual people actually engage with our social media.	<ul style="list-style-type: none">• Ungerboeck• Ungerboeck• CN data• CN data• CN – Satisfaction Survey• CN – Satisfaction Survey• CN data• CN data* <i>*Includes CN Corporate Facebook (CN FB), LinkedIn (LI)</i>
3.2. Opportunities in jobs, learning and innovation	<ul style="list-style-type: none">• Number of Newskills training projects and number of participants• Number of users of the Landing Pad. Startups/scaleups considering relocating to Newcastle• Level of community satisfaction with economic development	<ul style="list-style-type: none">• Maintain• Maintain• Greater than 3.5**	<ul style="list-style-type: none">• 10 projects• 26 users• 3.5	The growth of our local skills base, an increase in skilled migration to Newcastle and the embedding of inclusive practices in all actions of local business. Newcastle will effectively establish a skills-based labour market. We are a city that embraces and cultivates innovation. Business and industry are confident to experiment and collaborate to create new growth. Entrepreneurship is encouraged and the resources needed to create change are plentiful. Newcastle is a city where anyone can nurture an idea into a globally scaleable business. The community satisfaction survey's intention is to provide insights into the factors that drive satisfaction within the community and show CN the effectiveness of its service delivery in meeting community expectations. Satisfaction is measured out of 5 with a mean score above 3.0 indicating more satisfaction than dissatisfaction within the community in relation to a particular CN attribute.	<ul style="list-style-type: none">• Program now completed• Program now completed• CN – Satisfaction Survey

N2040 THEME: CREATIVE NEWCASTLE			N2040 THEME: CREATIVE NEWCASTLE		
PRIORITIES	SERVICE AND PROGRAM MEASURES	TARGET	BASELINE	WHY THIS IS IMPORTANT	SOURCE
3.3 Celebrating culture	<ul style="list-style-type: none">• Level of community satisfaction with Art Gallery and programs• Level of community satisfaction with Civic venues• Level of community satisfaction with Museum• Number of Art Gallery outreach program attendees• Number of artists celebrated in Art Gallery programming• Number of Museum ticketed attendees	<ul style="list-style-type: none">• Greater than 3.5**• Greater than 3.5**• Greater than 3.5**• New measure*• New measure*• Increase by 5%	<ul style="list-style-type: none">• 3.7• 3.7• 3.7• New measure*• 176• 105,745	<p>The community satisfaction survey's intention is to provide insights into the factors that drive satisfaction within the community and show CN the effectiveness of its service delivery in meeting community expectations. Satisfaction is measured out of 5 with a mean score above 3.0 indicating more satisfaction than dissatisfaction within the community in relation to a particular CN attribute.</p> <p>Over the next two years the Art Gallery will modify some of the ways it connects with the community, while its physical space is being upgraded. This change will lead to increased outreach programs. To capture the Art Gallery's impact on how we celebrate culture, outreach programs will be measured by number of people attending.</p> <p>Reinforce the links between artistic creations and society, encourage greater awareness of the diversity of artistic expressions and highlight the contribution of artists to sustainable development.</p> <p>Events and performances are a key component of developing a sense of community and pride, generating economic growth, accentuating natural and man-made assets, and giving the city its identity, both regionally and nationally.</p>	<ul style="list-style-type: none">• CN – Satisfaction survey• CN – Satisfaction survey• CN – Satisfaction survey• CN data• CN data• Camms
3.4 City-shaping partnerships	<ul style="list-style-type: none">• Number of DAs determined• Level of community satisfaction with management of residential development• Reduction in backlog of undetermined DAs	<ul style="list-style-type: none">• Maintain• Greater than 3**• Maintain	<ul style="list-style-type: none">• 1,645• 3.2• 410	<p>Development Applications (DAs) are a merit-based assessment conducted directly through CN.</p>	<ul style="list-style-type: none">• CN data• CN data• CN – Satisfaction survey• CN data

N2040 THEME: ACHIEVING TOGETHER			N2040 THEME: ACHIEVING TOGETHER		
PRIORITIES	SERVICE AND PROGRAM MEASURES	TARGET	BASELINE	WHY THIS IS IMPORTANT	SOURCE
4.1. Inclusive and integrated planning	<ul style="list-style-type: none">• Level of community satisfaction with CN's long-term planning and vision for the city• Level of community satisfaction with CN's overall performance• Decrease first year employee turnover rate• Increase Indigenous workforce representation• Increase our workplace engagement score	<ul style="list-style-type: none">• Greater than 3**• Greater than 3.5**• Less than 12.50%• 4%• 7.2%	<ul style="list-style-type: none">• 3.3• 3.7• 12.50%• 3%• 7%	<p>During engagement for Newcastle 2040, the community told us that considered and long-term planning should be a key focus area to ensure our vision of a liveable, sustainable and thriving global city is obtained. Community views on the success of CN to plan long-term are gained through the annual satisfaction survey with the aim to increase satisfaction scores year on year.</p> <p>The community satisfaction survey's intention is to provide insights into the factors that drive satisfaction within the community and show CN the effectiveness of its service delivery in meeting community expectations. Satisfaction is measured out of 5 with a mean score above 3.0 indicating more satisfaction than dissatisfaction within the community in relation to a particular CN attribute.</p> <p>This data provides insights into our people. A strong culture attracts talent that fits into our organisation better and helps keep them on board longer. People who feel like they are where they belong are more likely to stay, which means higher retention rates and lower turnover and increases overall wellbeing for both individuals and communities.</p>	<ul style="list-style-type: none">• CN – Satisfaction survey• CN – Satisfaction survey• HRIS• HRIS• Engagement Survey
4.2. Trust and transparency	<ul style="list-style-type: none">• Number of resolutions, completed and resolved• Level of community satisfaction with CN's response to community needs• Number of compliments received at CN• Number of complaints received at CN• CN website visitors per month	<ul style="list-style-type: none">• Greater than 80%• Greater than 3.5**• Maintain higher compliments than complaints• More than 334,496	<ul style="list-style-type: none">• 227 total/191 completed• 3.4• 288 compliments/122 complaints• 334,496	<p>Council resolutions are decisions made at Council to take action; this measure provides insights into how many resolutions happen at Council and how many are completed within the year.</p> <p>The community satisfaction survey's intention is to provide insights into the factors that drive satisfaction within the community and show CN the effectiveness of its service delivery in meeting community expectations. Satisfaction is measured out of 5 with a mean score above 3.0 indicating more satisfaction than dissatisfaction within the community in relation to a particular CN attribute.</p> <p>A key responsibility of CN is to respond to community needs. The community perception of this response is gained through the annual satisfaction survey and indicates the success of CN in this space.</p>	<ul style="list-style-type: none">• CN data• CN – Satisfaction survey• CN data• CN data
4.3. Collaborative and innovative approach	<ul style="list-style-type: none">• Number and value of community grants• Number of process completed within Promapp• Number of staff trained in process mapping• Level of community satisfaction with involvement in council decision-making	<ul style="list-style-type: none">• Maintain• N/A• N/A• Greater than 3**	<ul style="list-style-type: none">• \$585,110• N/A• N/A• 3.2	<p>CN collaborates with numerous government organisations, businesses, community groups and individuals to deliver positive outcomes for the Newcastle community. Our Community Grants provide a number of funding opportunities to support initiatives that contribute to the social, cultural, environmental and economic life of the city.</p> <p>Measuring process improvements is important because it allows us to see whether the actions we take are actually helping CN thrive and become more efficient over time.</p> <p>The community satisfaction survey's intention is to provide insights into the factors that drive satisfaction within the community and show CN the effectiveness of its service delivery in meeting community expectations. Satisfaction is measured out of 5 with a mean score above 3.0 indicating more satisfaction than dissatisfaction within the community in relation to a particular CN attribute.</p>	<ul style="list-style-type: none">• CN data• CN data – Promapp*• CN data – Promapp*• CN – Satisfaction survey <p><i>*This data is no longer available due to changes in software.</i></p>

Glossary

ABS Australian Bureau of Statistics.

ADVOCACY The act of speaking or arguing in favour of something, such as a cause, idea or policy. In the context of the Strategic Priorities it refers to another sphere of government or organisation delivering a service or outcome for the city.

BAU Business as usual.

COMMUNITY LAND Land classified as community land must be kept for use by the general community. All community land must be regulated by a Plan of Management, which may apply to one or more areas of land.

CN City of Newcastle.

CROWN LAND Land is land that is owned by the NSW Government but managed on its behalf by Council.

CX STRATEGY Our plan for a better customer experience. Our CX strategy aims to flip the power from the institution to the customer, building life-long trust.

DA Development Application.

DCP Development Control Plan.

DIAP The Disability Inclusion Action plan will at as a roadmap to guide CN's actions and establish strategies to ensure we create an inclusive community for all people who live, visit and work in Newcastle.

DELIVERY PROGRAM A strategic document with a minimum four-year outlook, which outlines the key strategies the organisation will undertake to achieve its desired outcomes. (Note: this is a legislative requirement).

EEO Equal Employment Opportunity.

ELT CN's Executive Leadership Team is led by the CEO and comprises five Directorates: Governance, Strategy and Engagement, People and Culture, Infrastructure and Property, and City Wide Services.

FBT Fringe Benefits Tax.

FINANCIAL YEAR The financial year we are reporting on in this document is the period from 1 July 2020 to 30 June 2021.

FTE Full-time equivalent. In relation to staff numbers this refers to a figure that is based on the wages for full-time staff.

GIPA *The Government Information (Public Access) Act 2009* (NSW), or GIPA Act, replaced freedom of information legislation.

GIS Geographic Information System.

IPART Independent Pricing and Regulatory Tribunal.

KPI a quantifiable measure of performance over time for a specific objective. KPIs provide targets for teams to shoot for, milestones to gauge progress, and insights that help people across the organization make better decisions.

LGA Local Government Area.

LIVEABILITY AND WELLBEING SURVEY The Liveability and Wellbeing survey is conducted to help benchmark key measures and track themes within the Newcastle 2040 Community Strategic Plan, measure quality of life and liveability within Newcastle and understand trust towards CN.

NEWCASTLE 2040 (N2040) *Newcastle 2040* is our integrated Community Strategic Plan which provides clear strategic direction for the long term and identifies the main priorities, aspirations and future vision of the community.

DELIVERING NEWCASTLE 2040 (DN2040) *Delivering Newcastle 2040* is our combined Delivery Program and Operational Plan, and translates the community's visions and priorities into clear actions. It is the primary reference point for all activities undertaken by Council during its term of office.

OPERATIONAL PLAN A document with a one-year outlook that outlines the key activities to be undertaken to achieve the desired outcomes set out in the Community Strategic Plan. (Note: this is a legislative requirement.)

PAMP Pedestrian Accessibility and Mobility Plan.

PARTNERING A structured approach to working together with other parties to achieve a mutually beneficial outcome.

PERFORMANCE The results of activities and progress in achieving the desired outcomes over a given period of time.

RATE PEGGING The percentage limit by which a council may increase the total income it will receive from rates. The percentage is set each year by the NSW Minister for Local Government.

RISK MANAGEMENT A discipline for developing appropriate procedures to reduce the possibility of adverse effects from future events.

SRV Special Rate Variation.

SUSTAINABLE DEVELOPMENT Development that meets the needs of the present generation without compromising the capacity of future generations to meet their needs.

SDGs Sustainable Development Goals.

TARGET A goal to be reached by a specific date, which may be higher than the forecasted performance. It aims to continually improve performance.

newcastle.nsw.gov.au

ATTACHMENTS DISTRIBUTED UNDER SEPARATE COVER

CCL 28/11/2023 - NEW ANNUAL COUNCIL COMMITMENT

8.5 **Attachment A:** New Annual 2022 Wrap Report

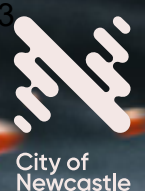
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Wrap Up Report

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23 Sept —
02 Oct 2022

NEW ANNUAL

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ACKNOWLEDGEMENT

City of Newcastle acknowledges the Traditional Custodians of the land of the Newcastle LGA, the Awabakal and Worimi peoples.



EXECUTIVE SUMMARY

Introduction

New Annual, City of Newcastle's flagship cultural event is a ten-day festival showcasing art, culture, and creativity. The festival aims to put Newcastle on the radar nationally and internationally, as a creative city where exciting, ambitious, and accessible contemporary art and culture grows and thrives. With this cultural shift comes new opportunities for residents, cultural institutions, communities, businesses, and philanthropic organisations to engage with the festival.

Our vision for New Annual is bold, future-forward, and inclusive. New Annual, provides a platform for the creative exploration of ideas, actions, and issues that inform the city. Expressed through multiple art forms and delivered across multiple venues, artists engage in dialogue and exchange with audiences and citizens. The festival supports and champions local artists while fostering creative networks that reach beyond the city. It acknowledges the key role those diverse perspectives bring and seeks to generate new takes on old issues. It will position CN as a contemporary and creative city on the rise.

This report documents the outcomes of New Annual 2022 and includes links to additional reports in appendices for further details. It provides analysis of the marketing campaign, program curation and highlights, budget summary and insights into the learnings and recommendations to build capacity and long-term success.

The New Annual festival has already had a significant impact directly on the city's arts and cultural communities — and the local economy.

New Annual 2022 has built on the success of the inaugural event in 2021, expanding its breadth, depth, and impact by showcasing the City of Newcastle's creative offerings. It included an extended five-week season of the blockbuster Van Gogh Alive experience, which attracted more than 84,000 people to Newcastle in its first visit to a regional area in Australia.

Over ten days, New Annual attracted almost 40,000 people to more than 140 performances and activities across the city. The festival engaged more than 530 artists and over 400 staff, crew, and volunteers, and was recently awarded \$400,000 over two years (2023-24) from the NSW Government as part of its Major Festival and Events grants program through Multicultural NSW.

City of Newcastle engaged Culture Counts to implement a research and impact evaluation framework that effectively details the nuance of the festival's impact and benefits, across its cultural, artistic, and economic priorities (See link to Evaluation Report at Appendix B). Culture Counts have developed an evaluation strategy to meet these requirements and establish an ongoing methodology for future events. The platform contains an extensive set of metrics that measure the impact of investments, programs, and activities across cultural, social, civic, economic, and environmental outcome areas.

It is often challenging to demonstrate the full value of activities, such as those based in arts, culture, or community because it requires a sophisticated understanding of the holistic benefits these programs deliver. Culture Counts is a proven and effective means to realise this understanding.

The evaluation was conducted by surveying members of the public and distributed to ticket holders. In partnership with Culture Counts, this survey was designed to evaluate the strategic goals and outcomes of New Annual.

Highlights from the 2022 Culture Counts evaluation report:

78%

of respondents were first time attendees

96%

of respondents agreed that the festival had a positive impact on their physical health and mental wellbeing

91%

of respondents had a positive overall experience

53*

Net Promoter Score

**An NPS that is positive (i.e. higher than zero) is felt to be good, and an NPS of 50+ is excellent. New Annual Festival's NPS of 53 indicates that audiences have a high level of loyalty towards the festival and are likely to promote it when speaking with others.*



City of Newcastle engaged J9 Consulting to develop a Private Sector Support Strategy to inform the best approach to increase investment from government and the private sector for future sustainability and growth.

As a result, New Annual developed several strategic, financial and in-kind partners for 2022 including The University of Newcastle, Newcastle Airport, and Hunter Central Coast Development Corporation, Crystalbrook Kingsley, Foghorn Brewery, Equip Media, Holiday Inn Express & Suites, and Newcastle Transport. City of Newcastle intends to build on these partnerships for future festivals.

Opportunities and Challenges

This is a critical time for New Annual to establish its place on the national calendar of events and ensure that it has a long-term and sustainable strategy. As a festival in its infancy, New Annual is building processes, cultural infrastructure, brand awareness, local engagement, and strategic partnerships from the ground up. The New Annual 2022 brand campaign was recently awarded 'distinction' at the Australian Graphic Design Awards, recognition that compelling, and bold creative ideas can gain national attention.

Notwithstanding challenges with inclement weather at outdoor events and changes in audience ticket buying behaviour since the pandemic, New Annual 2022 successfully showcased artistic virtuosity, global relevance, garnered critical acclaim and showcased an evolving city in a new light for locals and visitors.

New Annual is uniquely positioned as Newcastle's leading cultural festival to solidify its place in the national arts calendar, and leverage this momentum for future growth, local pride, and engagement. New Annual seeks to foster a prosperous, diverse, and resilient economy and build capacity in the arts and cultural sector by delivering a contemporary cultural event that showcases Newcastle as a creative hub.

32 Venues

37,633 Attendees

533 Artists

325 Support Crew

95 Venue Staff

82 Days of exhibitions
(galleries/street art)

140 Performances



New Annual 2022 was an exciting platform for civic pride and celebration. It was an ambitious and inspiring program of contemporary, boundary-pushing productions curated with the aim of positioning New Annual as one of the regions, and indeed Australia's, pre-eminent contemporary arts festivals.

New Annual showcased some of Australia's most respected and significant contemporary artists across multiple artforms in Newcastle for the first time. Importantly, the festival also celebrated extraordinary artists from Newcastle, with City of Newcastle support providing a platform for local talent to evolve and showcase their work to more diverse and engaged audiences than many could otherwise achieve.

New Annual 2022 celebrated a dynamic, modern city in transformation, with bold and exciting art from creative visionaries presented against the backdrop of some of Newcastle's most significant historical spaces including those outlined below.

VENUE	PRODUCTION
City Hall	CREATION the performance and Epoch Wars
Civic Park Stage	Mountain, This Land, Light Years from Home, National Young Writers' Festival, Future Innovators Summit, Menagerie (workshops and performance).
Civic Theatre	Colossus and Radial film presentation
Foreshore Park	Van Gogh Alive
The Lock Up	CREATION workshops and exhibition
The Playhouse	Well
The Station	Radial filming and Chalk the Walk
Newcastle Museum	Deejay x Dancer and Ex Machina
Wheeler Place (a dedicated First Nations space)	Pavilion of Sand
164 Hunter St	Titan Arum

New Annual 2022 showcased 533 Australian artists through its Curated and Community programs, stimulating the local and regional economies in substantial and long-lasting ways. This included the City of Newcastle's inbound tourism and hospitality sectors. Direct nights generated by the event by visitors from outside the region were 9,303.

'Embodied' was the underlying curatorial theme for the 2022 edition. It was experiential for the audience and expressed through the body in the art works. 'Embodied' is an expression of or to give a tangible or visible form to an idea, quality or feeling and embodies spirit and skill; all qualities that the invited artists brought to Newcastle for New Annual 2022. And this is the ambition of New Annual itself, to embody the spirit of Newcastle through authentic community experiences.

The depth and quality of the program is best represented by its 16 headline acts and many of the program highlights were collaborations with local institutions. (See appendix A for full program details).

Van Gogh Alive – City of Newcastle and Andrew Kay Management

City of Newcastle and New Annual festival brought the immersive, multi-sensory experience *Van Gogh Alive* to Newcastle's premier major events space, Foreshore Park, which attracted more than 84,000 people to Newcastle in its first visit to a regional area in Australia. It was presented in association with Andrew Kay Management in a specially designed 2,300-square-metre gallery known as The Grand Pavilion and ran in an extended season from the start of New Annual to 23 October.

TITAN ARUM - Newcastle Gallery

Titan Arum by renowned artist Justin Shoulder, was a collaboration with Newcastle Gallery. The gallery is closed for two years of capital works, and this ambitious project was conceived to signal the future direction of the gallery when it reopens. The work was offered free to the public and presented in a pop-up space at 164 Hunter St, with the aim of invigorating this section of the mall through its interface with the passing foot traffic.

Titan Arum was one of the standout projects of the festival which sold out over the ten days. The project included 11 performances by the artists and an exhibition that was open 9-5pm for 9 days of the festival. We also presented a preview of the work for an invited audience for Major Partner the University of Newcastle.

This Land - University of Newcastle

This Land was a partnership with the University of Newcastle's School of Humanities, Creative Industries and Social Sciences. Curated by local First Nations artist Jacob Ridgeway and supported by Adam Manning the Conservatorium Coordinator, *This Land* was a contemporary Aboriginal & Torres Strait Islander music event. It featured nine unique acts with a range of high profile and emerging First Nations artists. University of Newcastle's Researching Australian Music students and staff performed and worked behind the scenes as a work-integrated learning opportunity. *This Land* demonstrated University of Newcastle's and City of Newcastle's commitment to First Nations music making.

Radial - Civic Theatre

New Annual, Civic Theatre and Tantrum Youth Arts' Accelerate Ensemble collaborated with one of the world's most respected theatre organisations, Back to Back Theatre. The team filmed across the city throughout the festival in four iconic locations to produce a new film work, *Radial*, which premiered at the Civic Theatre on Sunday 2 October. *Radial* was proudly supported by Hunter and Central Coast Development Corporation and is available for viewing on the Back to Back website. <https://backtobacktheatre.com/project/radial/>

Pavilion of Sand - First Nations Communities

Pavilion of Sand was developed over three years, and it was satisfying to bring this ambitious and complex project to fruition at New Annual 2022. Wheeler Place was re-imagined as a landscape and sand was returned to the heart of the city as a structure, ballast, shelter, and seating for a pavilion that celebrated Awabakal and Worrimi Culture. The Pavilion was home to a 10-day program of dance, native food workshops, language workshops and performance and the response from workshop participants and families was unanimously positive.

CREATION - The Lock Up and Catapult Dance

CREATION was a major exhibition of artist Deborah Kelly's work at The Lock Up accompanied by a series of free public workshops in costume-making, song, and dance. Working with local artists, choirs, and dancers, the workshops lead to a spectacular procession and concert at the City Hall showcasing the songs of *CREATION*.

Colossus – Stephanie Lake Company and Sydney Dance Company

New Annual presented internationally acclaimed choreographer Stephanie Lake's work *Colossus* at the Civic Theatre on the opening weekend. The exhilarating performances featured 40+ dancers from Sydney Dance Company's Pre-Professional Year and sound design by renowned sound artist Robin Fox.

The programming model for New Annual 2022 embraced a multifaceted approach. It offered a range of artistic and cultural experiences that differed in scale, artform, genre, and delivery. Each program element envisioned a potential audience. Each utilised specific venues, spaces, and precincts of the city and matched them with appropriate performances, community partners, and interactive activities.

Together, the suite of programs celebrated the City of Newcastle's creative spirit and dramatically multiplied the impact. They helped create new visionary works and extended the life of existing productions, honoured the culture of Australia's traditional owners, showcased Newcastle's many physical assets, created an open-air family-friendly atmosphere, built the capacity of local artists, and raised their profiles nationally while stimulating employment and economic development through cultural tourism.

The Future

To achieve the ongoing vision of the festival, it is our ambition to continue to present 10-12 signature works highlighting the best quality projects from across the country. A wide range of community and local activities will provide different access points for people to discover unexpected things and follow the artists they love.

While the vision is for a highly curated festival (unlike an open-access fringe festival), it is equally important that New Annual is not perceived as 'gatekeepers' by the community. We will continue to showcase local artists, celebrate the City of Newcastle's unique landscape and stories, share our Aboriginal culture, and provide unexpected experiences for locals and visitors in an inclusive and welcoming environment.

The first two festivals included a dedicated Indigenous program in the Pavilion of Sand, a purpose-built structure located in the centre of Newcastle's cultural precinct. New Annual's ambition is to enhance this by including complimentary and dedicated programming that is reflective of Newcastle's multicultural communities. This will be achieved by working with existing community groups and will result in a festival program that further showcases the vibrancy and richness of the community.

As we prepare for new horizons and diversify Australia's economy, the City of Newcastle looks to the creative industries and innovation to grow the regional economy, stimulated through destination tourism and activation. It will position City of Newcastle as a contemporary and creative city on the rise.



New Annual partnered city-wide with local businesses, venues, and service providers to enable Newcastle communities to rebuild and reimagine their city after the disruption caused by the pandemic. This was visually communicated through our key brand message ‘Our City Made New’. (See link to Marketing Report at Appendix C).

Four of the most innovative and creative projects funded through CN’s ‘Special Business Rates Program’ were delivered during New Annual 2022. These included UP & UP which activated Museum Park for two days with street art workshops and live painting exhibitions; Mad Bitches Inc. – a feminist inspired cabaret show; Chalk the Walk a temporary 3D art trail created for key outdoor locations and Big Picture Festival – large scale murals painted live in the city centre.

These projects were free to the public and helped support local businesses through collaborative promotions, increased foot traffic and significantly contributed to increased vibrancy in the city centre during the festival.

TYPE OF PERFORMANCE	# OF EVENTS	CN INVESTMENT	CN BUDGET LINE	TICKETED/FREE
CN Acquisition (100% CN funded)	18	\$384,344	New Annual	Ticketed
Van Gogh Alive (Part CN funded)	1	\$100,000	Events Sponsorship Program	Ticketed (All revenue to promoter)
Umbrella (Part CN funded)	5	\$299,478	SBR Community Grant	Free
Partner Events (No CN financial support)	9	0	New Annual	Ticketed (All revenue to promoter)

Sponsorship

J9 Consulting was initially engaged by City of Newcastle from November 2021 to May 2022 to provide advice on income diversification and secure sponsors for the New Annual Festival. The festival had no history of sponsorship and proposed a financial target of \$100,000 of cash support.

While there was strong enthusiasm from businesses approached for sponsorship, most were unfamiliar with arts partnerships.

Businesses that elected not to sponsor in 2022 predominately pointed to Covid-19 (either with lingering fears that it may derail the festival or insufficient funds for sponsorship given the effects of the pandemic on their business) or caution owing to the “newness” of the festival. The economic climate at the time also meant business had little appetite for risk.

As this was the first time many prospective sponsors for New Annual had been offered tailored proposals, J9 was able to assess realistic cash sponsorship levels and gain valuable insights into the strategic business decisions which will inform future approaches.

As Covid-19 moves from pandemic to endemic and the New Annual Festival continues to solidify its place as Newcastle’s flagship arts festival, it’s anticipated that prospective sponsor concerns will be less of a barrier for the event in 2023. There are several ‘warm leads’ that elected not to sponsor this year’s festival but would be an obvious starting point for new sponsors for next year.

When J9 completed the initial project in June 2022, an agreement and two tailored proposals valued at \$70,000 had been issued. Negotiations were also underway with an additional 13 prospects with sponsorship valued at \$170,000. These discussions were transitioned to the Festival team.

J9 was contracted again between August and October 2022 for the purpose of finalising sponsor agreements and managing the delivery of benefits during the festival. The result was cash and in-kind sponsorship valued at \$180,000. See appendix (D) for the link to the full sponsorship report.

SPONSOR	DESIGNATION	PURPOSE	SPONSORSHIP VALUE
University of Newcastle	Presenting Partner	Presenting partner across the whole festival	\$30,000
Hunter & Central Coast Development Corporation	Presenting Partner	Chalk the Walk and Radial Film	\$30,000
Newcastle Airport	Major Partner	Exclusive Civic Park Stage Presenting Partner	\$20,000
Foghorn Brewery	Supporting Partner	Exclusive beer selling rights at Civic Park Stage	\$10,000
Equip Media	Supporting Partner	Audio Visual equipment and technical services at Civic Park Stage, the Museum and 164 Hunter St	\$80,000 (in-kind)
Crystalbrook Kingsley	Supporting Partner	Launch event and artists' accommodation	\$10,000 (in-kind)
Holiday Inn	Supporting Partner	Artists' accommodation	\$1,000 (in-kind)
Newcastle Transport/Keolis Downer	Supporting Partner	Audio trail on Newcastle public transport	\$1,000 (in-kind)
Total Sponsorship Value			\$182,000
Total sponsorship target			\$100,000

Economic and Attendee Impact

New Annual 2022 has boosted the local Arts industry with over 40 unique events and exhibitions across 10 days. There were 37,633 attendees, 140 performances, 32 venues, 533 artists, 325 support crew and 95 venue staff involved in the festival.

Total unique attendees	30,755
Primary purpose visitors from outside of the region	6,487
Percentage of visitors from outside region who stayed overnight	51%
Number of unique outside of region attendees	7,989
Direct nights generated by the event by visitors from outside the region	9,303
Direct audience expenditure - Visitors (additional to economy)	\$6,371,221
Direct audience expenditure - Locals (stimulated)	\$1,530,517
Total Impact	\$7,901,738

AUDIENCE DEMOGRAPHICS⁶⁴

Age

74% of feedback survey respondents were aged 30-59, and 72% were female. which is consistent with the ages most dominant in the ticket purchase data and marketing audience engagement, where 35.30% of ticket bookers were female and 9.85% were marked as male.

Repeat or existing customers

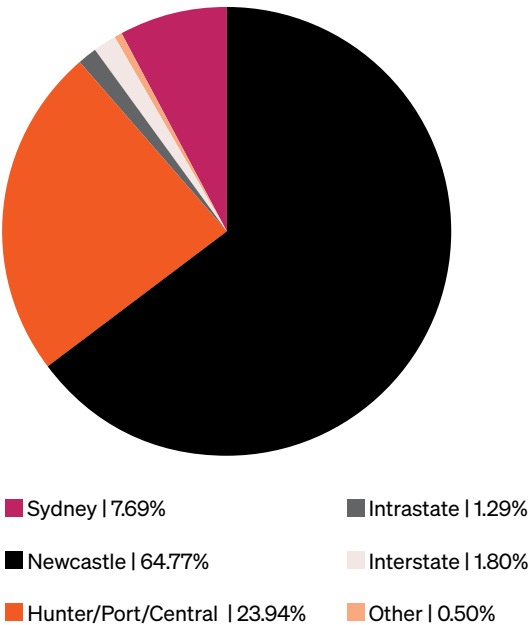
According to Civic Theatre booking data, 49.76% of customers were first time attendees of New Annual.

Geographic data of customers

The largest proportion of customers were from Newcastle and Lake Macquarie representing 78.64% of bookings. 7.69% of customers were Sydney, predominantly from the Western suburbs. 1.79% of customers were from interstate. The booking data very closely aligns with the Culture Counts survey data.

LOCATION	# OF CUSTOMERS	%
Newcastle	901	64.77
Lake Mac	193	13.87
Sydney	107	7.69
Hunter region	91	6.54
Central Coast	25	1.80
Nelson Bay/Port Stephens	24	1.73
VIC	14	1.01
Regional NSW North	11	0.79
Other	7	0.50
QLD	6	0.43
Regional West	5	0.36
ACT	1	0.07
NT	1	0.07
SA	1	0.07
Southern Highlands	1	0.07
Tamworth	1	0.07
TAS	1	0.07
WA	1	0.07
TOTAL	1391 customers	100%

Customer location by %



This presents 35-54yo females residing in Newcastle/ Lake Mac and the outer suburbs of Sydney as the biggest opportunity for targeting in 2023. With the majority being first time attendees of New Annual and indicating that they would return to New Annual again, this leaves a huge potential audience for 2023's festival encompassing additional new attendees alongside those returning from 2022 if the data can be effectively utilised.



TICKETING DATA

Civic Theatre Ticketed Events

EVENT	NO. PERFORMANCES	TOTAL CAPACITY	TOTAL ATTENDEES	% SOLD (PAID ONLY)
Light Years from home	2	NA	NA	NA
Creation - Performance	1	356	338	86%
Music & Robotics Workshop	1	30	30	100%
Robot Dance Party	3	36	36	100%
Wild Side	2	240	135	100%
Menagerie - Performance	1	150	143	74%
Deejay x Dancer	3	240	226	70%
Well	11	171	142	80%
Ex Machina	2	160	126	66%
Menagerie - Workshop	5	80	57	71%
I liked it but...	2	100	70	69%
This Land	1	476	244	29%
Epoch Wars	1	200	75	18%
Colossus	3	1,000	716	21%
Aftermath	1	500	119	19%
Mountain	3	1,540	368	21%
TOTAL		5,279	2,825	54%
AVG				62%

There was a range of free events in the festival with recorded attendance as follows:

EVENT	NO. PERFORMANCES	TOTAL CAPACITY	TOTAL ATTENDEES	% SOLD (PAID ONLY)
Trolleys	2	NA	1,780	0%
Titan Arum - Exhibition (days)		NA	3,490	NA
Titan Arum - Performances	14	800	800	0%
Nihilartikel Launch	1	150	90	0%
Night Gallery Tour	16	180	109	0%
Pavilion of Sand	16	800	687	0%
Radial - Film Presentation	1	348	227	0%
TOTAL		2,278	7,183	

Third Party Events & Exhibitions

EVENT	NO. PERFORMANCES	TOTAL CAPACITY	TOTAL ATTENDEES	% SOLD (PAID ONLY)
Van Gogh Alive		NA	21,874	NA
Chalk the Walk		NA	3,150	NA
Creation Exhibition	10	NA	983	NA
Future Innovators Summit		NA	637	NA
Studio Art Trail		NA	237	NA
Clouds Exhibition	1	NA	170	NA
Handpicked		NA	160	NA
Ripple Effect Band	1	400	123	31%
Mad Bitches inc	4	220	149	65%
Underground Epicurians	2	29	27	93%
City Stories x Makers Traders		NA	65	NA
Starry Night Masterclass		50	50	100%
TOTAL		649	27,625	

In total, the New Annual commissioned program achieved an attendance of **9,510, which incorporates attendees to free exhibitions and 1,391 bookings.**

The total attendance at events under the New Annual program, including Chalk the Walk and Big Picture Fest, has been estimated at more than **37,633** people combined over the 10 days.

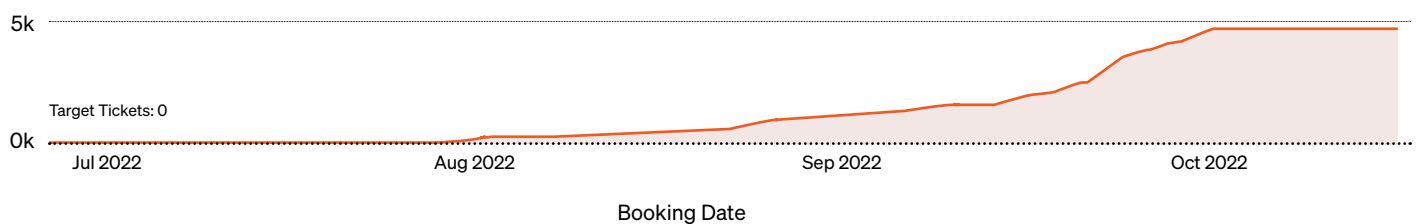
Bookings & group size

45% of bookings were made for two people, followed by 26% of bookings made for single tickets. Most bookings were made for a single performance 86%, followed by 8.37% and 3.35 % booking for two and three performances respectively. This indicates an opportunity to incentivise multi-performance bookings from the start of the campaign.

Booking timing

Consumer trends demonstrate that customers are purchasing tickets much later than in the past, expect greater flexibility and are less willing to make commitments too far in advance. This is not dissimilar to purchasing behaviour in 2021 and reflects current industry-wide trends.

Cumulative Tickets by Booking Date



New Annual 2022 has boosted the local Arts industry with over 40 unique events and exhibitions across 10 days. There were 37,633 attendees, 140 performances, 32 venues, 533 artists, 325 support crew and 95 venue staff involved in the festival.

ACCOMMODATION STATISTICS

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Newcastle's visitor economy utilises events as a key driver to attract day and overnight visitation and create economic benefit and dispersal. The 2022 New Annual festival demonstrated a significant and positive economic impact across the city's arts and cultural communities, hospitality, and tourism sectors, and the broader visitor economy.

Data from Australian Accommodation Monitor Reports confirm the average occupancy rate during the 10-days of New Annual was 76.9%, the average daily rate was \$228.61 and the total revenue was \$2.35 million. Last year's accommodation rates are note comparable as they were heavily impacted by COVID-19.

Hotel Occupancy Rate	76.9%
Average Daily Hotel Rate	\$228.61
Total Revenue	\$2.35 Million

"The 2022 New Annual Festivities drove life & visitation into the East End of Newcastle CBD. QT Newcastle traded strongly during this period with occupancy above 90% most weekdays with a full hotel across the weekends during the event. Rates across this period also increased by roughly 15% to what we normally see.

A fantastic outcome considering the weather was not on our side!"

Michael Stamboulidis
General Manager

Newcastle exploding with the Arts! Last night I had the privilege of officially opening New Annual 2022...Yesterday's opening featured the incredibly riveting and hypnotic dance work by internationally renowned Australian choreographer Stephanie Lake – 'Colossus'. Huge congrats to the dancers from Sydney Dance Company's Pre-Professional Year. They were utterly outstanding.

And this morning I saw 'Van Gogh Alive'. I had unfortunately missed it earlier in the year, but now I see what all the fuss was about. It is a visually stunning immersive experience.

The Hon. Ben Franklin, NSW Minister for Tourism, Aboriginal Affairs, the Arts and Regional Youth

So lucky to see the show at The Civic! Stunning, inspiring, mesmerising choreography and production and such energy and talent from the crew of young dancers. The future of modern dance in Australia looks to be in very safe hands. Another thank you for bringing it to Newcastle (please come back!)

Audience feedback

The festival program was sophisticated and of a national standard! It was very exciting for this kind of offering to be delivered by the city. This will absolutely put Newcastle on the map - more people just need to know about it!

Audience feedback – Culture Counts Survey

The Civic Park stage was incredible. Tying in the shipping container aesthetic without Newcastle as a port city was genius, and something I've not seen done in other major festivals. The vision of the festival's director was really clear in the incredible calibre of programming. As a new resident of Newcastle, this festival it made me feel thrilled to be here and signalled how much the city values art and culture. The festival program gave me an opportunity to bring in visitors from interstate and to show them around the city with purpose and pride. Congratulations. More of this cultural investment and brilliance please. - !

Audience feedback – Culture Counts Survey

Creation gala was mind-blowing. Their references to the sacrifice of Newcastle's bees was also something I've never seen in an artistic context and it was truly touching.

Audience Feedback – Culture Counts Survey

We were thrilled to have our inaugural Little Festival which celebrates miniature and small-scale street art involved with the New Annual program. Inclusion in their umbrella of events provides reach across festival audiences, it helps give context to our event and provides an important support system for a start-up Festival...To have a flagship event such as New Annual in a regional area is critical, it rises the tide for all associated events and extends the audience reach across diverse communities in a way that our event would not be able to do if we were flying solo.

Jacinta Fintan, Little Festival Director

Proud to curate and perform at a significant cultural event for First Nations artists. An event of this size has not been seen in Newcastle for years for First Nations artists.

Jacob Ridgeway, This Land Curator and Manager National Aboriginal & Torres Strait Islander Music Office APRA AMCOS

Now more than ever community is looking to creative experiences as a beacon of light to guide the way out of what has been a dark time...'Well' lit the way...But 'Well' was more than sheer entertainment. The narrative of "Well" invited children to think about feelings, to work together cooperatively towards greater goals and to discuss ways to overcome difficult emotions, all in fun and memorable ways.

Jade Perry, Artist and Educator, Art Mania Studio

The New Annual commissioned myself and Conor Fox to create a new work of theatre for children. This support enabled us to test a new idea, take creative risks and create a bespoke immersive theatre experience. The support to develop the work and the platform of the festival enabled us to create something unique and beautiful for young audiences in Newcastle.

Janie Gibson, Whale Chorus

New Annual was fantastic for the city and for the people. It brought together many of Newcastle's talented performers, artists and creatives as well as setting the bar of programming to a metropolitan level.

Katarina Skoumbas, Big Picture Festival Director

Thank you for programming RADIAL Newcastle in New Annual, adding diversity to your program, giving voice and presence to those that are traditionally underrepresented. Being involved in New Annual is more than the outcome of a beautiful film, it's about relationships, friendships, collaboration, brave and bold art making, and mostly it's about a portrait of a community and the environment in which they live, work and make great art.

Nikki Watson, Community and Education Producer, Back to Back Theatre

We were so thrilled to be part of New Annual and have the opportunity to present at Newcastle City Hall. The program was chock-a-block of leading Australian artists, we were excited to be in such good company. We felt very supported by the Festival and would love to work together again.

Samantha Butterworth, Senior Producer, Performing Lines

It has been great to work with you on the City Stories project. We are proud to be promoting local businesses and hopefully can continue working together in the future.

Tegan Hughes, Customer Experience & Engagement Manager, Keolis Downer / Newcastle Transport



Overview

CN contracted Persona Communications to develop a communications plan and lead the delivery of the marketing campaign. The campaign targeted locals predominantly as well as regional markets within a four-hour drive distance, including Sydney.

The total marketing spend was \$258,736 and was split as per below:

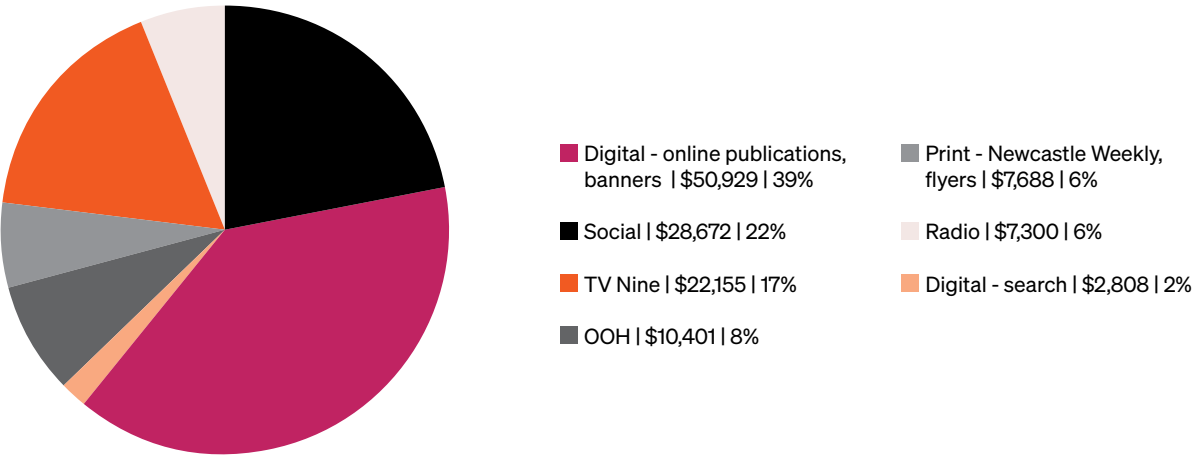
Brand Development & Website Refresh	\$21964
Campaign Asset Development & Content Creation	\$51577
Media Buy	\$129,953
Communication Plan, Media Strategy & Implementation	\$46114
2022 Event Content Capture & Photography	\$9127
Total	\$258,736

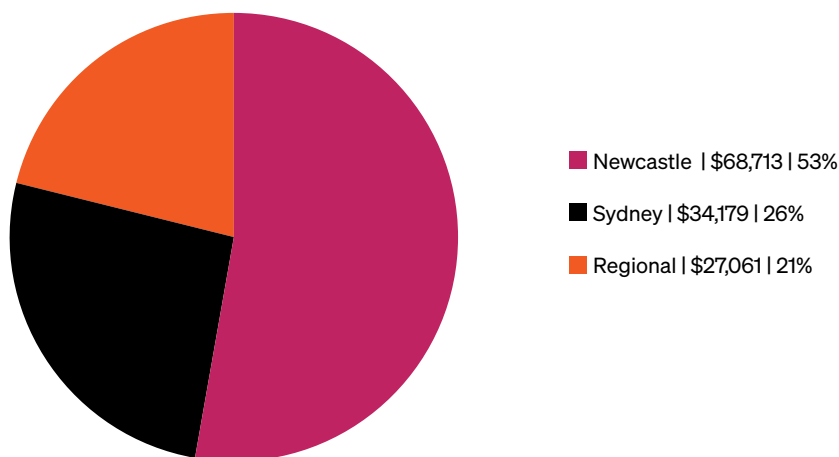
New Annual’s marketing campaign, commenced on August 01 with the launch of the program and website. The media campaign commenced from 08 August – 02 October with a combined media budget of \$129,953. The campaign reached over 2.7million consumers.

The overarching objective of the campaign was to raise awareness of New Annual as a progressive and contemporary arts festival delivering an exceptional caliber of local and interstate events and exhibitions with “ten days of art, culture and creativity in the centre of Newcastle.”

Media

Media spend by channel





Key Stats

The New Annual paid campaign reached over 2.7 million people.

Southern Cross Austereo radio ads reached a total of 160,048 people with an average frequency of 8.89

Local social media ads attracted 4205 link clicks, regional social media ads attracted 1452 link clicks 6965 total

Visit Newcastle amplified New Annual marketing assets reaching 261,569 through organic social media

There were over 50,000 video plays through to completion, with video posts having the highest engagement

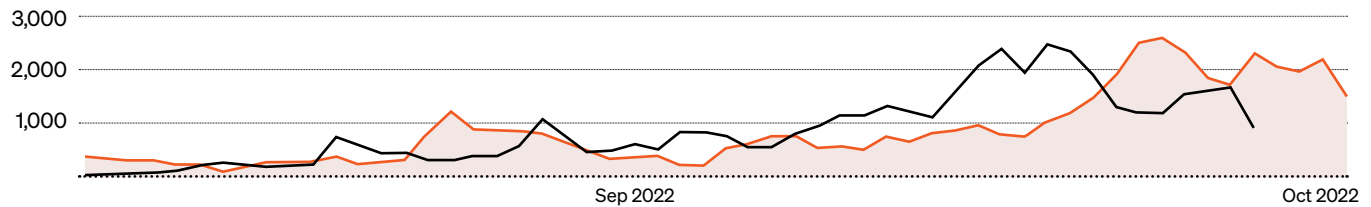
42108 unique users visited newannual.com during the campaign period spending an average of 02:02mins on site, representing an increase in users of 25.11% from '21.

Newannual.com had 185,539 page views (up 44.25% on '21) with an average of 2.98 pages viewed per session (up 23.33%)

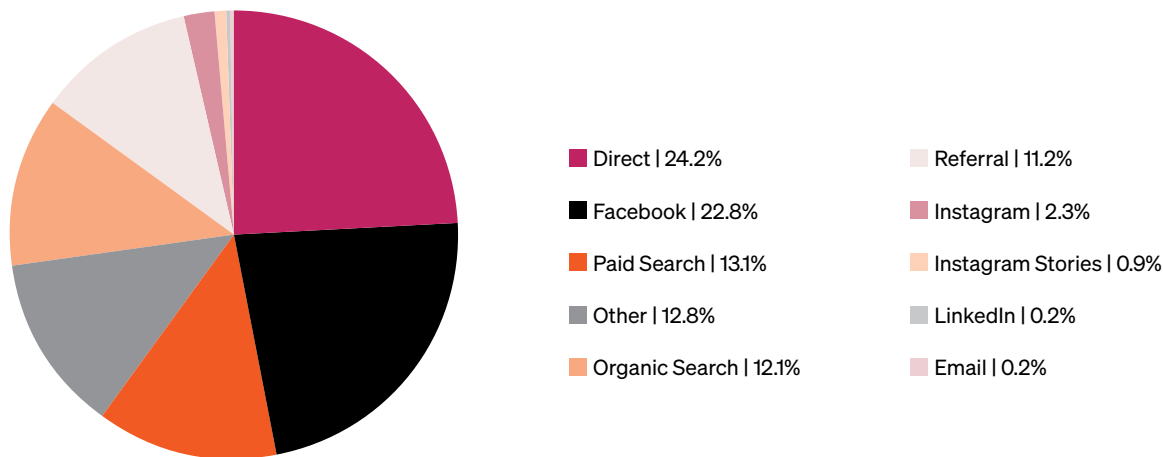
Website

Newannual.com for the period of 01 Aug – 02 October 2022

8 Aug 2022 - 2 Oct 2022: — Users
1 Jan 2021 - 21 Feb 2021: — Users



Users



Media Reporting

An analysis of coverage published between 1 Aug 2022 and 19 Oct 2022 found 246 mentions of New Annual across media with five dedicated releases being issued. This coverage reached a cumulative potential reach of 2,619,051 and an advertising space rate (ASR) of AUD \$1,297,841.

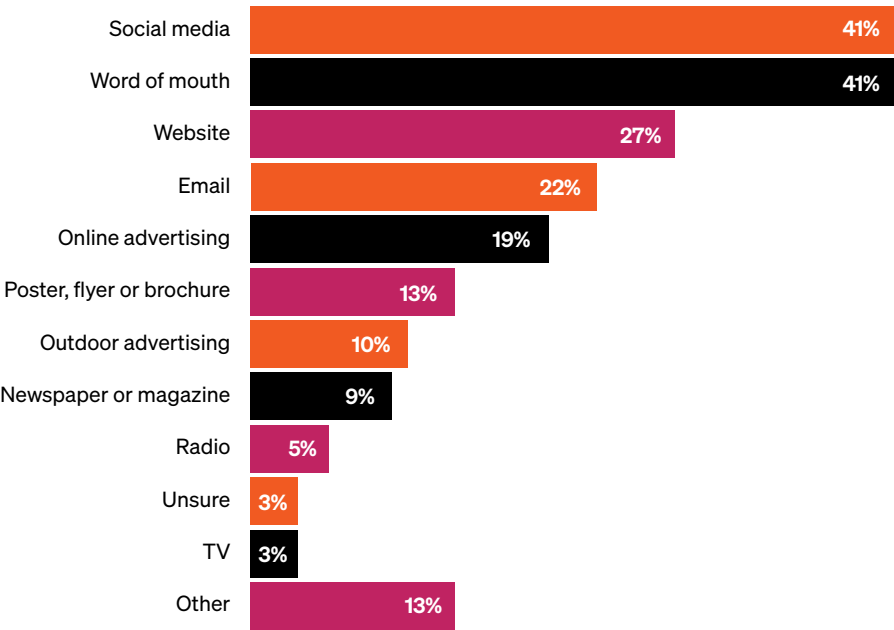
This is compared to 426 mentions during 12 Jan – 22 Feb 2021 and seven media releases being issued.

The largest potential reach occurred in the week commencing 03 October when the festival had concluded, which demonstrates a need for greater PR proactivity earlier in the campaign period.

For a detailed account of New Annual's marketing campaign across all channels, please refer to [Analytics Report - Newcastle City Council - New Annual \(1st Aug - 19th Oct 2022\) MEDIA MENTIONS.pdf](#)

Marketing

How did you hear about this event?



Culture Counts survey respondents indicated that Social Media 41% and Word of Mouth 41% were the most popular channels for discovering New Annual. TV and Radio proved again to be ineffective at delivering awareness at scale and it is recommended alternative more cost-effective channels are bolstered moving forward. Digital and social channels are recommended to continue to capture conversion data and target specific audiences.



Income & Expenditure

DESCRIPTION		2021/22	2022/23	TOTAL
INCOME	Sponsorships - Cash	\$0.00	\$90,000.00	\$90,000.00
	Sponsorships - In-kind	\$0.00	\$92,000.00	\$92,000.00
	Grants (Internal)	\$0.00	\$20,000.00	\$20,000.00
	Other contributions	\$0.00	\$39,175.00	\$39,175.00
	Ticketing Income	\$0.00	\$85,995.91	\$85,995.91
	Income Total		\$327,170.91	\$327,170.91
EXPENDITURE	Ticketing Expenditure	\$0.00	\$106,283.71	\$106,283.71
	Expenditure of In-kind sponsorship	\$0.00	\$92,000.00	\$92,000.00
	Programming / Artist Fees	\$168,000.00	\$176,860.00	\$344,860.00
	Marketing / Brand Development	\$207,550.79	\$33,392.61	\$240,943.40
	Production / Operations	\$248,069.34	\$247,845.15	\$495,914.49
	Consultants	\$69,382.36	\$35,000.00	\$104,382.36
	Staffing costs	\$102,787.67	\$172,618.72	\$275,406.39
	Expenditure Total	\$795,790.16	\$864,000.19	\$1,659,790.35
NET RESULT		-\$795,790.16	-\$536,829.28	-\$1,332,619.44

Income



- Sponsorships - In-kind | \$90,000.00
- Sponsorships - Cash | \$92,000.00
- Ticketing Income | \$85,995.91
- Other Contributions | \$39,175.00
- Grants (internal) | \$20,000.00

Expenditure



- Production/Operations | \$495,914.49
- Programming/Artist Fees | \$344,860.00
- Marketing/Brand Development | \$240,943.40
- Staffing Costs | \$275,406.39
- Expenditure of In-kind Sponsorship | \$92,000.00
- Consultants | \$104,382.36
- Ticketing Expenditure | \$106,283.71

Festival Partners

New Annual wouldn't be possible without our valued partners

Principal Partner

New Annual is the city's flagship arts festival that is owned and operated by the City of Newcastle. The City of Newcastle is a liveable, sustainable, inclusive global city.



Presenting Partners

Guided by their values of equity, excellence, sustainability and engagement, the University of Newcastle has built a strong reputation as a world-leading university making an impact within their regions, in Australia and across the globe.



Hunter and Central Coast Development Corporation (HCCDC) is working to create vibrant places with great public spaces, unlock employment and housing opportunities, and attract and retain innovative talent in the Hunter and Central Coast regions. Their work contributes to diverse and sustainable regional economies with thriving communities at their heart.

HCCDC is thrilled to partner with New Annual, and celebrate their revitalised Honeysuckle and The Station precincts through the festival.



Major Partner

Underpinned by its purpose - being the Airport the region deserves - Newcastle Airport is much more than a key piece of infrastructure. It delivers global connectivity to our region by providing access to major destinations in Australia and the world. Just as importantly it brings the rest of the world to us and with it, the economic benefit the Hunter needs to prosper. Just 30 minutes north of Newcastle city, the Airport is located at Williamstown adjacent to the RAAF Base. The Airport is serviced by Jetstar, Virgin Australia, QantasLink, Bonza Airlines, Link Airways, FlyPelican and Eastern Air Services and connects to 12 direct domestic destinations and onward to more than 65 destinations across the world.



Supporting Partners

An old soul with a fresh twist, Crystalbrook Kingsley is culturally connected, sustainably-led, simply sophisticated and steeped in history. The first luxury hotel to grace Newcastle, Crystalbrook Kingsley sparks vibrant energy and nostalgia in one moment.



Foghorn Brewery was born of a dream to make craft beer the way it ought to be made—from grain to glass all under one roof. The historic building in King Street is the first in Newcastle to brew beer on site, creating over 40 unique varieties of beer. Enjoy a tour and a paddle when visiting New Annual.



Equip Multimedia is the premium provider of live audiovisual production in Newcastle. Whether you need audio, video, visual or staging, they have the expertise and equipment to make your event a success. Combining state of the art technology with superior technical expertise and a genuine desire to exceed expectations, you can rely on Equip Multimedia to interpret your vision and deliver a world-class production experience.



Holiday Inn Express & Suites Newcastle is conveniently located right at the Newcastle Interchange and light rail stop, making it a great place to stay and explore the city. For every night booked in September, they are donating two meals to OzHarvest. Book your stay and help feed 100,000 people in need.



Newcastle Transport is the integrated transport provider for Newcastle, and is responsible for running buses, ferries, the light rail and the multi-modal Newcastle Interchange. Keolis Downer has been awarded a 10 year contract to design and run Newcastle Transport services across all modes of travel. Together they will get you to and from New Annual safely and swiftly. Be sure to scan the Makers x Traders QR code when travelling on the light rail to hear City Stories from our local artisans.





ATTACHMENTS DISTRIBUTED UNDER SEPARATE COVER

**CCL 28/11/2023 -
ADOPTION OF WICKHAM PUBLIC DOMAIN PLAN**

- | | | |
|-----|----------------------|----------------------------|
| 8.6 | Attachment A: | Wickham Public Domain Plan |
| 8.6 | Attachment B: | Summary Engagement Report |
| 8.6 | Attachment C: | Engagement Report Response |

DISTRIBUTED UNDER SEPARATE COVER



Wickham Public Domain Plan

Date: November 2023
Final Report



City of
Newcastle

Client Information and Credits

This Public Domain Plan Report forms part of the deliverables for the Wickham Public Domain Plan. Please refer to the supporting technical reports and engineering reports for further details. This document was prepared by Mott MacDonald, ASPECT Studios, and Bangawarra for Newcastle City Council.

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APPROVED BY	TD



City of Newcastle acknowledges that we operate on the grounds of the traditional country of the Awabakal and Worimi peoples. We recognise and respect their cultural heritage, beliefs and continuing relationship with the land, and that they are the proud survivors of more than two hundred years of dispossession. City of Newcastle reiterates its commitment to address disadvantages and attain justice for Aboriginal and Torres Strait Islander peoples of this community.

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Section 1.0

Introduction



Section 1 – Introduction

1.1 Project Purpose

1.1.1 Introduction

Wickham is changing. Once a semi-industrial area, Wickham is now transforming into a new, mixed-use urban neighbourhood. Building on a master plan produced in 2017, this concept public domain plan sets the groundwork for the delivery of a precinct that is connected, resilient, regenerative, smart and responsive to the needs of the existing and future community.

1.1.2 Project background

Wickham faces both challenges and opportunities. Wickham's opportunities include its location and a number of existing assets. Wickham is adjacent Wickham Transport Interchange and close to Newcastle CBD and the new Honeysuckle development. It stands to benefit from the strategic shift of Newcastle's commercial core from the East End to the west. It boasts an eclectic urban character and the neighbourhood amenities of the waterfront and Wickham Park. Wickham's challenges include Hannell Street (which interrupts active transport connections between Wickham and the water front), exposure to flood events and subsidence, a high car mode share and parking demand, and limited active transport access to the CBD.

Wickham's 2017 Master Plan, prepared in response to the Newcastle Urban Renewal Strategy (first published in 2012, updated in 2014), laid out a vision for the neighbourhood that addresses its challenges and builds upon its opportunities.

This vision for Wickham saw the neighbourhood capitalising on its proximity to the water, the park, and the CBD while prioritising pedestrian safety and active transport, developing a linear 'Village Green' on Union Street, supporting 'smart city' initiatives, and encouraging development that was appropriate for different uses and scale and co-ordinated to best deliver a public domain for the community.

Following stakeholder engagement, the master plan was updated and adopted in 2021. The updated plan identified the opportunities of redevelopment potential and placed renewed focus on greater connectivity, improved public domain amenity, and a built form that reflected a dense and dynamic mix of uses.

1.1.3 Project purpose

Following its monitoring of the implementation of the Wickham Master Plan, the City of Newcastle identified the need to prepare a public domain plan (PDP) that would provide support and guidance to public and private sector organisations involved in designing and implementing public spaces in Wickham, to best realise the vision of the plan.

- The stated purposes of the Public Domain Plan are to:
- enable effective integration with private developments to both influence better design outcomes for the streetscape and save money in the long-term;
 - outline the value of the public realm and optimise built environment outcomes for the community;
 - make Wickham a better and more attractive place in which to live, visit and do business;
 - create an improved perception of Newcastle to visitors;
 - demonstrate how design principles can be applied to streets and spaces, and;
 - better prepare the area for development and changes in the future.

This document, the Public Domain Plan Report, is the fourth in a sequence of deliverables and summarises the overall Wickham Public Domain Plan. It summarises key background documents, collates and synthesises urban analysis, establishes the 'Place Themes' that have been developed across the Country, Place and Landscape team, and distills those findings into a set of Design Strategies. These then inform the frameworks and design elements that have been developed to guide the future design and delivery of the Wickham Public Domain.

1.1.4 Plan delivery

The Wickham Precinct Public Domain Plan (PDP) provides support and guidance to public and private sector organisations involved in designing and implementing public spaces in Wickham. It aims to create better connectivity and experiences of public space within Wickham and surrounding areas.

It is a strategic document that clarifies important requirements for public space and street design to all professionals engaged in the facilitation, design, approvals, and implementation of the public realm.

The PDP has been designed to be a companion document to the City Centre Public Domain Technical Manual which sets out the mandatory technical and documentation requirements for all civil, landscape and infrastructure works occurring in the public realm which will occur throughout future concept and detailed designs.

The PDP is not a final design package, instead it provides a set of achievable aims and objectives to guide future private developer works and identify potential council delivered projects.

Work on the Wickham Precinct will occur in a staged manner as development applications are received in accordance with the zoning within the approved Wickham Masterplan and as council identified infrastructure works are delivered.

This PDP will be followed by concept and detailed designs for each area of staged works. They will be exhibited as a part of statutory planning processes allowing for review and comment by the community.

The aim of the PDP is to provide a framework to achieve the vision of the Wickham Masterplan and allow for the development of future delivery stages.



City of Newcastle Strategic Plans and Policies¹

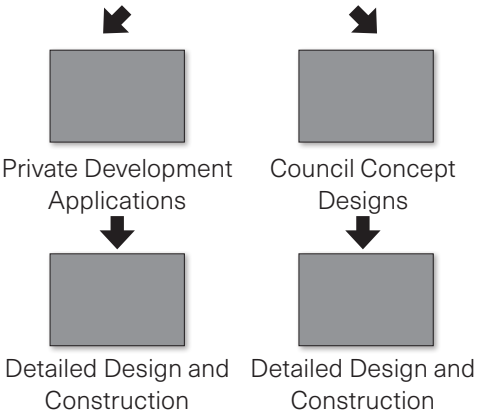
¹Sample only show above



Wickham Masterplan and Local Area Traffic Management Study



Wickham Public Domain Plan (This document)



1.2 Project location



KEY

Boundaries

- PDP Focus area
- Wickham Masterplan area
- Newcastle City Centre area

Transport

- Heavy rail stop and alignment
- Light rail stop and alignment
- Industrial rail alignment
- Ferry wharf
- Channel Berth cruise terminal

Section 1 – Introduction

1.3 Project approach

1.3.1 Country-led

In line with the City of Newcastle and the community's commitment to recognising and building on the rich traditions of the Indigenous Custodians of the land on which the community lives and works, the project has considered Country as a foundational element in the project approach. The Wickham Public Domain Plan aims to recognise key perspectives of Country and use this as a foundation upon which we base subsequent understandings of place.

1.3.2 Integrated

A coordinated engineering, landscape and urban design approach is essential to fulfilling the project purpose. To achieve the outcomes required for this project, no discipline can act in isolation. The integrated design approach has included all disciplines in decision making and key design moves.

This has been critical to the successful delivery of this precinct with integration of not only the internal disciplines but across the relevant City of Newcastle Council departments, Transport for NSW, and the community.

1.3.3 Informed

The Wickham Public Domain Plan has built upon and integrated with the strategic vision and objectives identified in the work that has already been undertaken by the City of Newcastle. The large amount of consultation and planning that has already been done in Wickham and surrounding areas has provided a wealth of community and stakeholder intelligence from the Master Plan.

Previous document have been reviewed along with broader research, site visits, discussion with City of Newcastle working groups and stakeholder consultation.

1.3.4 Place-based

'Placemaking' is a term used to describe the process of creating meaningful and layered places. As designers of the public realm, our training and design process is focused on bringing together and expressing the essential qualities of place, the landscape, its history, social rituals, and cultural significance into the design of place

Placemaking is about designing cities that cater to people. It aims to promote human interaction and connection to the street and allow people to participate in their public domain. Our approach creates lively neighbourhoods and inviting public spaces that attract people because they are comfortable, pleasurable, culturally appropriate, and interesting.

1.3.5 Responsive

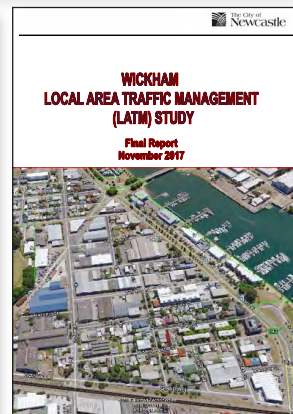
A responsive approach to the qualities of place that support the renewal of Wickham would:

- Address place specific challenges and opportunities.
- Enable transitional change which builds on existing urban structures.
- Improve the perception of place without loss of existing identity.
- Prioritise the retention of aspects of place that people treasure.
- Provide a unique and authentic experience which is distinctively Wickham.
- Provide for the expression of the multiple layers and systems that have shaped the public domain whilst creating more liveable cities.

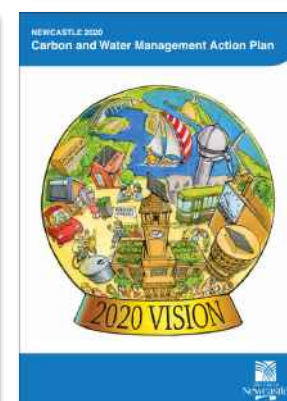
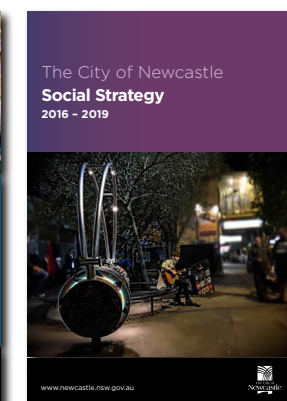
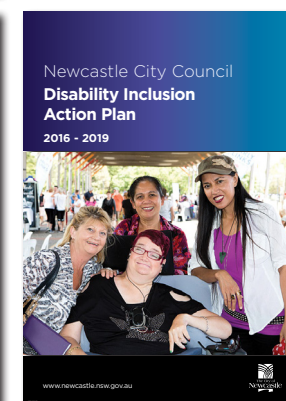
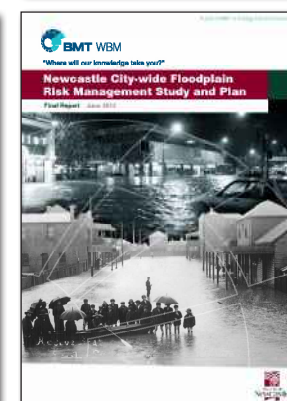
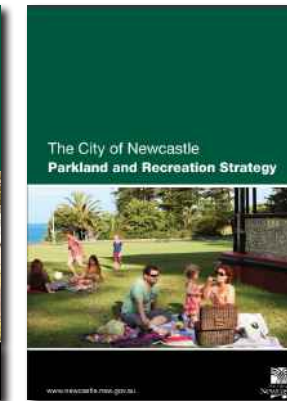
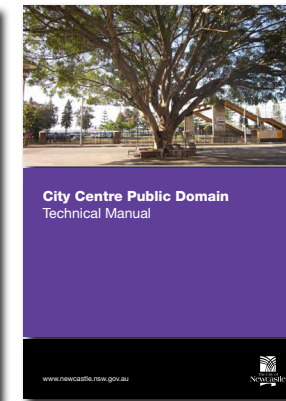
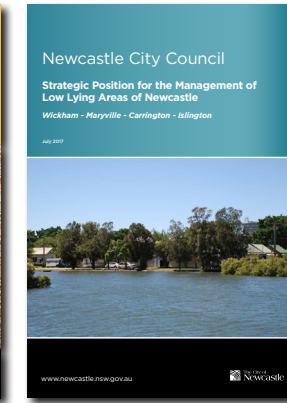
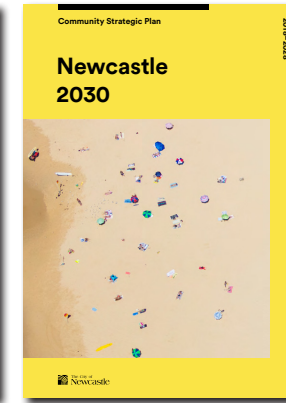
Section 1 – Introduction

1.4 Background documents

1.4.1 Documents specific to the Wickham context



1.4.2 Documents specific to the broader context of Newcastle



Other relevant documents reviewed include:

- *Newcastle Employment Lands Strategy* - SGS Economics and Planning for City of Newcastle, 2019
- *Local Housing Strategy 2020 [updated 2021]* - City of Newcastle, 2021
- *Hunter Regional Plan 2036* - NSW Department of Planning, 2022
- *Newcastle After Dark (2018-2022)* - City of Newcastle, 2018

1.5 Key Place Themes



1.5.3 Water

Wickham lies on Bitterwater Country, where salty water slowly mixes with fresh. For millennia, this process sustained wetlands and mudflats, populated by mangrove, salt rush, lomandra, paperbark, casuarina and countless others.

Within these close-knit ecologies, Aboriginal people have long been able to thrive. The interstitial zones along river banks and on land above historic flooding limits have provided safe places to connect, share stories, and enact ceremony. The presence of water itself has always taught a lesson on the interconnectedness of all things.

Following colonisation, this presence of water came to mean something else. Newcastle's link with the sea made it a viable satellite of Sydney in the early days of the colony and those who had long lived and cared for its Country were among the first to come into contact with the coloniser. By the middle of the nineteenth century, a steady line of ships plied the river, conveying people, news, coal, and cargo upriver, down the coast, or overseas.

Today, the riverbed has been dredged, the river edge has been made hard by walls and levees, and footpaths and roads have made the ground impermeable. These changes have interrupted the flow of the tides and the mixing of water, and if improperly managed, amplify rather than diminish the risk of flooding. These changes force us to ask: how will we (re)learn the lessons of the water of this Country? How will we acknowledge the interconnectedness of all things?



1.5.4 Movement

The Country on which Wickham lies has long been criss-crossed by the movement of both human and non-human beings. These movements have been shaped by the changing of seasons and the patterns of migration, closely related to the turning of the stars and the passage of the sun and moon.

Many of the paths and tracks of this historic movement would be appropriated following colonisation. But even when a track was eventually established, the fastest way to Sydney was still by water. In 1854, work began on the Great Northern Railway, linking Newcastle to Hexham, though it would be decades before the line was connected with Sydney. During the twentieth century, on the Carrington shore, hundreds of ships came off the slips and entered service.

At the centre of this entanglement of travel lines always lay Wickham. The removal of the mainline tracks parallel to Hunter Street, which had long separated Honeysuckle and the harbour from the rest of Newcastle, has shifted this web of lines again.

Now, Wickham is home to Newcastle Interchange, a front door to the city, where travellers change trains and travel speed. But to recognise the Country on which all of this movement occurs is to recognise the flows and traces that have defined it in the past – and will continue to define it in the future.



1.5.5 Connectivity

The Country surrounding Wickham has always been a site of connections: long before the industrial centre was established there, the first peoples of the region made use of its hard stone and abundant coal, trading with other communities far away. People would have connected with Country itself, through storytelling and gathering, and through the knowledge of Country as a means of providing food and medicine.

The colonial city weighed heavily on Country. The city's extraction, manufacture, and transportation of minerals, its shipbuilding and its agriculture, all contaminated the region's earth, air, and water. This was not a connection to Country but connection in spite of Country.

Yet the complicated history of this extraction also deeply informs its sense of place. For a time, Newcastle was the most productive and important node in Australia's industrial net, a status its inhabitants could boast of. It remains the busiest coal port by volume in the world, even as the world turns slowly from fossil fuel consumption altogether. The sheer volume of the city's output demanded thousands of people, gathered and bound together by industries.

The threads of these connections run throughout Wickham. From the former Bullock Island railway, to the sailor's mission, to the Tree of Knowledge, the connections of the people of Wickham have never been confined to the suburb but stretched far away - to the wider city and world beyond.



1.5.1 Gathering

As in countless other places across Country, people have long gathered around fire. The heart of any gathering, fire has been and will continue to be the centre around which food, ceremony and story can be shared.

Colonisation brought its own kind of fire – the fire of industry – which took the region's abundant resource of coal and began to mine and burn it in a destructive way. Yet, these fires also forged close bonds between the people who came to live and work in the area. These bonds only tightened in the face of the economic crisis at the end of the war, the closure of the steel plant, and the dangers of working in the foundries, collieries, and shipyards themselves.

Wickham's popularity as a residential suburb declined following the adoption of private transport and the associated viability of outer city suburbs, along with the wider economic decline of the city. But the area's renewal, now in full swing, stands to reestablish and celebrate places to gather and dwell. In doing so, it will build upon a strong history of community resilience and grit, which persevered both when the city was capital of the world of coal and will continue long after it ceases to be.

Wickham's strong history of community demands the careful protection of its longstanding gathering places, from the pub, to the street, to the backyard. But it also demands the sensitive planning and provision of new places to gather - for Wickham's campfires of the deep future.



1.5.2 Layered stories

All of Wickham's elements, from its water, to its travel networks, to its histories of gathering and making, together represent a multi-layered history. The depth of this history was present before colonisation, informed by memories stretching back through deep-time. To protect and sustain these layered memories, local Aboriginal peoples have always used mnemonic indicators to share and protect their knowledges of geology, hydrology, topography, astronomy and ecology.

But this layering was fundamentally changed by colonisation, a fact to which the earth itself bears witness. While traces of First Nations peoples' manufacture can be found in the fire-reddened sand and the chips of stone axes near hard stone outcrops, rocks from around the world dumped as excess ballast during the nineteenth century can be found in the riverbed. And elevated levels of certain heavy metals – the legacy of the area's industrial use – can still be detected in the sediment of Throsby's Creek.

Today, to celebrate the multi-layered histories and future of Wickham, we must recognise where some traces have been over-written, let Country itself be able to be read, and elevate multiple viewpoints, rather than any one single narrative.

Section 1 – Introduction

1.6 Consultation: What we Heard

Community engagement on the approaches developed for the Public Domain Plan took place between from 22 February to 22 March via an online survey and online feedback form to allow for written submissions. During the consultation period, there were two drop-in sessions to inform the community about the PDP and encourage feedback via the Have Your Say page.

The overarching design elements of the PDP, and changes to vehicle movement, the pedestrian network and cycleway network, were generally well supported. Positive and constructive feedback was also received on the creation of recreation/open spaces and planting of trees/greenery, including direction of use and how best these spaces could serve the needs of the community. The feedback reinforced the value such new spaces and expanded planting would have for the community.

Below is a summary of the key themes of comments where community offered perspectives on the PDP as presented through the engagement process - and how the PDP has responded to the feedback received:

Proposed traffic changes:

- Comments received requested reconsideration of changes and increased safety and information via signage, speed limits and prioritisation of local traffic

PDP response:

- Traffic movements, new signs and speed limits, and a range of engineering measures that help prioritise local traffic have all been considered to create holistic improvements across Wickham*

Proposed pedestrian changes:

- Comments were received on accessibility, uniformity and level of detail on footpaths along with improvements in lighting to ensure good pedestrian experience for all users

PDP response:

- Accessibility consultants have been involved in the PDP and advised on the proposed footpath widths across the suburb. All widths meet their recommendations, except where existing conditions constrained their size.*
- Details of pedestrian elements and lighting will be developed as part of subsequent works in line with the framework established in the PDP*

Proposed cycleway changes:

- Comments were received on prioritising separation of cyclists where possible, consideration of loss of on street vehicle parking, access to cycle parking and more detailed planning of on street cycling including contra flow

PDP response:

- Dedicated cycling facilities have been planned to best capture desire paths through and to/from Wickham and dedicated paths have been proposed for major routes.*
- Traffic calming methods and other traffic changes have been adopted specifically to improve the safety of cyclists*

Stories, landmarks, art, entry points and welcome nodes:

- Comments received clarified key entry/departure locations, the most appropriate elements to include at these locations, and what other histories and objects unique to Wickham that should be celebrated.

PDP response:

- The PDP has adopted the suggestions of the community in the approach and design of welcome nodes, heritage interpretation and other forms of celebrating Wickham's stories in its public realm*

Urban activation areas:

- A range of suggestions for public uses in the urban activation areas were received.

PDP response:

- The PDP have proposed concepts for the urban activation zones reflecting the input received and providing a broad range of provisions.*

Lighting

- Comments received in regards to lighting focused on safety, lighting colour and location to improve pedestrian experience and the impact of light spill from the Newcastle Interchange

PDP response:

- The PDP provides direction for future detailed lighting designs to follow.*

Connectivity:

- Comments noted that Newcastle Interchange needs better connections with bus, train, and ferry terminal nearby and the role of Throsby Street as an emerging area with activation potential should be considered

PDP response:

- Connections and inter modal movements to and from Newcastle Interchange and multi-modal movements to and from Throsby Street have been considered in the PDP*

Trees and plantings:

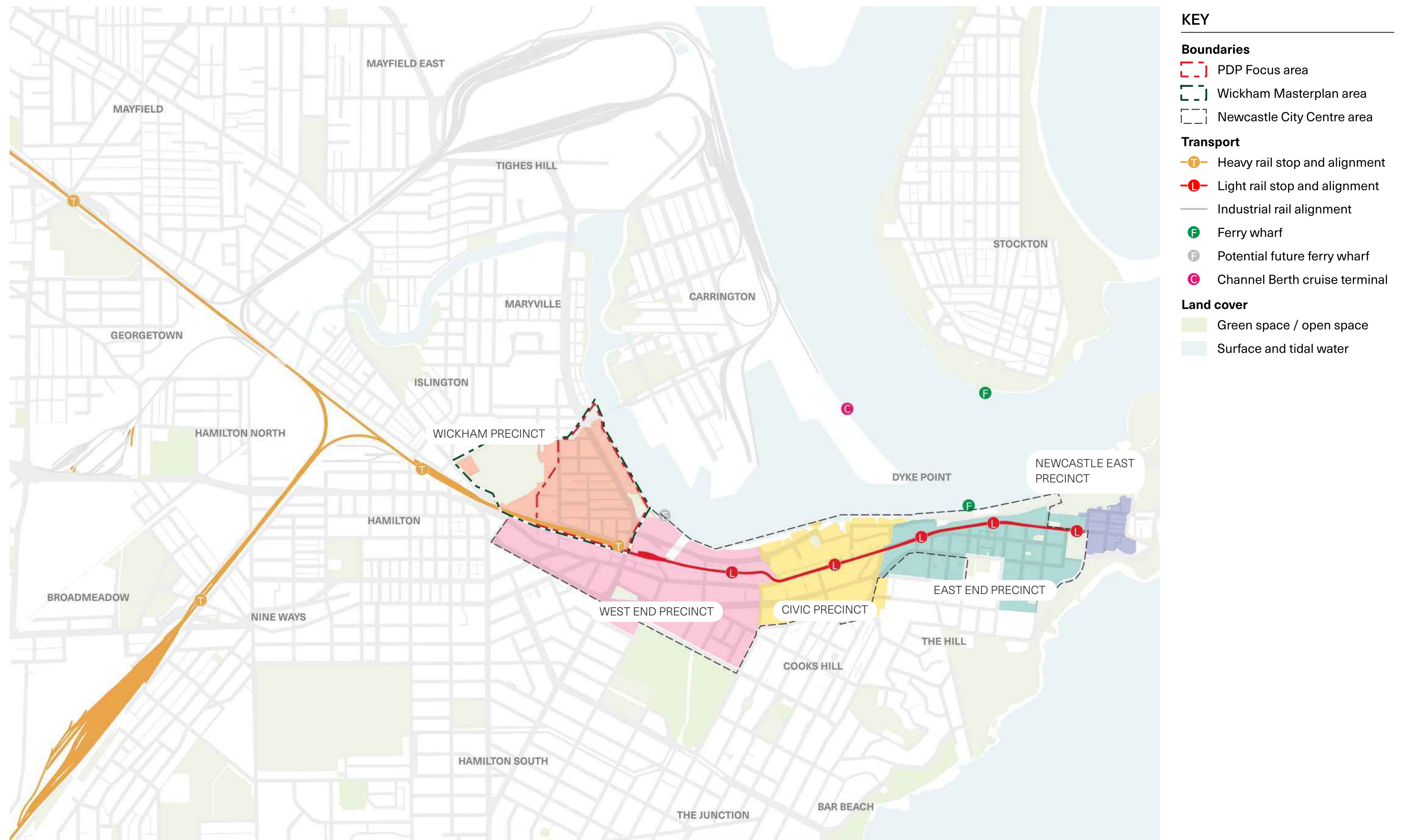
- Comments received noted opportunities to shift garden zones into road corridors to assist in traffic calming and commented on tree selections and placement relative to existing constraints, maintenance impacts of deciduous trees and impacts on sight lines

PDP response:

- Opportunities to locate new planting between parking bays has been integrated into the design.*
- The PDP provides guidance for the selection of endemic planting species with a range of tree sizes to allow appropriate selection, with detailed design when consideration of site conditions is undertaken.*

Please refer to the Community Engagement Report prepared by City of Newcastle for further information on comments received in the consultation process.

1.7 Neighbourhood precincts



Section 1 – Introduction

1.8 Analysis summaries

Analysis work was undertaken in the initiation phase of the Wickham PDP which captured information from the master plan phase and other relevant documents and data. The analysis was reviewed at both a broader urban scale and at the precinct level and from this work and site visits opportunities and constraints were identified. Below is a high level summary of this analysis work. Further details on the analysis undertaken is included as an appendix to this report.

URBAN ANALYSIS SUMMARY

1.8.1 Precinct context

The Wickham Precinct is strategically located and forms the western end of the Newcastle City Centre area. It lies to the west of Newcastle’s West End Precinct and Civic Precinct. Wickham Park forms the western edge of the precinct and to the east the waterfront of Throsby Creek.

1.8.2 Elevation, ecology, and water

Wickham, along with much of the wider waterfront, is very low-lying, with most of its streets only a few metres above sea level. As the historic shorelines of Wickham illustrated in this PDP show, the landscape of Wickham has also been heavily modified over time. All of this makes Wickham subject to flooding and dictates a number of key constraints on its urban form. It also means that it is critical that Wickham’s public domain is designed with and for water, to make it as resilient as possible.

Throsby Creek, despite being an important shipping basin, is also an important ecological habitat, qualifying as a Ramsar wetland.

1.8.3 Transport

Wickham is well connected by the light rail, ample bus connections, and the waterfront of Throsby Creek and the Hunter River. It is also home to Newcastle Interchange, Newcastle’s “front door”, where heavy rail terminates.

Wickham is bounded by a series of arterial roads, including Hannell Street to the north-east and the rails lines to the south. Hannell Street in particular is a major barrier between Wickham and the waterfront.

A modest network on on-road cycleways connect Wickham’s periphery to the wider city, especially along Hannell Street and Hunter Street. Upcoming cycleway improvements will further tie Wickham to the rest of the city more safely than ever before.

Former rail infrastructure is also a key spatial element of Wickahm, with the Bullock Island Rail Corridor a key feature.

1.8.4 Urban heat

Despite its proximity to the water, Wickham is very vulnerable to the urban heat island effect (UHIE) - the major exception being Wickham Park. It is one of the hottest areas in the broader Newcastle area as a result of the larger extents of hard surfaces on streets and parking lots and limited canopy cover. Addressing this challenge will mean increasing canopy cover and increasing the albedo of the suburb.

PRECINCT ANALYSIS SUMMARY

1.8.5 Streets and open space

Streets are currently generally dominated by cars. There are a number of factors which result in pedestrian and cycling conditions being less than ideal, including a lack of footpaths in some places and other barriers to movement and lack of tree canopy and poor / inactive building frontages. The scale of the road corridors varies significantly and impacts on the actions that can be undertaken to address the issues identified.

Exceptions to these conditions do exist such as Union Street where the finer grain of the built form, characterful features and more extensive and established trees provide a far more pleasant experience for all users.

While Wickham Park is an extensive open green space, there is limited open space within the suburb itself, and few small-scale, green and open public spaces for everyday rest, relaxation, and socialisation.

1.8.6 Tree canopy

Existing levels of tree canopy cover to road corridors is extremely low. Within the local Wickham streets the level of cover is only 10%. The waterfront area and Wickham Park are critical in providing some greater canopy cover.

1.8.7 Parking

The local streets of Wickham have extensive areas of on street parking with the majority unrestricted. Greater levels of parking restrictions exist generally to the south-east in closer proximity to Wickham Interchange and there are also high levels of utilisation within this geographic area.

1.8.8 Non-Aboriginal Heritage

Wickham can boast a rich heritage, being the location of important historic railways, including the Bullock Island Railway, and many historic institutions such as sailor’s missions and art schools. These all contribute to the distinctive character of Wickham.

1.8.9 Built form and land use

Wickham’s built form is varied and reflects the waves of occupation and land use that have occurred in the area. The built form is in transition as a result of the changes in zoning that have occurred as a result of the City of Newcastle master plan. As part of its analysis, this document has carefully considered Wickham’s existing patterns of land ownership and areas of potential change, and considered proposed amendments to controls for built form.

1.8.10 Activity nodes

Wickham is home to a number of distinctive art elements, particularly in the form of murals and some smaller sculptural elements. The waterfront area is a key activity zone with the fish markets, the yatch club, the marina, the waterfront walk along Throsby’s Creek and food and beverage offerings. Wickham Park is the other major attractor and this and the waterfront are supported by a range of other food and beverage offerings distributed along Railway Street, the rail corridor, Throsby Street and near the Wickham Interchange.

OPPORTUNITIES AND CONSTRAINTS SUMMARY

Key constraints include the existing utilities, road reserve widths and crossing points on TfNSW controlled roads, significant infrastructure barriers, and flooding.

Key opportunities identified include celebrating the unique features and character areas of Wickham, improved climatic and ecological conditions through increased canopy cover, improved travel experiences for all users and ensuring pedestrians and cyclists have good quality infrastructure to use.

Other key opportunities include unlocking access to Wickham’s two great natural assets of the waterfront and Wickham Park, making Wickham an exemplary site of water sensitive urban design (WSUD), and increasing the number of small and medium-scale open and green spaces that can serve the needs of the community.



Artist's impression of Church Street once PDP initiatives have been fully implemented.

Design Strategies



2.1 Design strategies hierarchy

WICKHAM MASTER PLAN
VISION AND KEY OBJECTIVES

“ Wickham will continue to transform into a dynamic urban neighbourhood that supports a diverse mix of uses, which complement the adjoining commercial core of the Newcastle City Centre located within Newcastle West. Urban renewal within the area is envisaged to build on the existing urban structure to deliver greater connectivity, improved public domain amenity, and a built form reflecting the envisaged function and character.”

Improve accessibility and connectivity within Wickham and to adjoining areas

Create safe, attractive, and inclusive public places

Ensure built environment is functional, responsive, and resilient

PLACE THEMES

WATER

Wickham lies on Bitterwater Country, where salty water slowly mixes with fresh. For millennia, this process sustained close-knit estuarine ecologies, within which Aboriginal people thrived. The interstitial zones along river banks and the land above historic flooding limits provided safe places to connect, share stories, and enact ceremony.

Newcastle's link with the sea also made it an early site of colonisation. By the middle of the 19th century, ships plied the river, conveying people, news, coal, and cargo upriver, down the coast, or overseas. Today, the riverbed has been dredged, the river edge has been hardened, and much of the ground is impermeable, interrupting the flow of the tides and the mixing of water.

These changes force us to ask: how will we (re)learn the lessons of the water of this Country? How will we acknowledge the interconnectedness of all things?

MOVEMENT

Wickham has long been criss-crossed by the movement of both human and non-human beings. These movements were shaped by the changing of seasons and the patterns of migration. Many of the paths and tracks of this historic movement were appropriated following colonisation – first by ship, horse and ox, then trains, trucks and cars.

At one of the intersections of these many travel lines always lay Wickham. The removal of the mainline tracks parallel to Hunter Street, which had long separated Honeysuckle and the harbour from the rest of Newcastle, shifted this web of lines again, making Wickham part of the front door of the city.

To recognise the Country on which all of this movement occurs is to recognise the flows and traces that have defined it in the past – and will continue to define it in the future.

CONNECTIVITY

Wickham has always been a place of connections: long before the city, the first peoples of the region made use of its hard stone and abundant coal, trading with other communities far away. People would have connected with Country itself, through storytelling and gathering, and through the knowledge of Country as a means of providing food and medicine.

Newcastle has weighed heavily on Country. Extraction, manufacture, shipbuilding and agriculture has all degraded the region's earth, air, and water – not a connection to Country but connection in spite of it.

Yet this complicated history also deeply informs its sense of place. The city's industry demanded a huge amount of people, who each made Wickham and wider city their home. Their connections – to each other and to places far away – are still a part of Wickham's history.

GATHERING

As in countless other places across Country, Wickham's first peoples long gathered around fire. And for as long as there is community, fire will be the centre around which food, ceremony and story is shared.

Colonisation brought its own kind of fire – the fire of industry. Yet, these fires also forged close bonds between the people who came to live and work in the area. These bonds only tightened in the face the economic crisis and the closure of the steel plant, producing communities of resilience and grit.

Wickham's popularity as a residential suburb declined following the adoption of private transport, along with the wider economic decline of the city. But with the area's renewal now in full swing, Wickham stands to reestablish and celebrate places to gather and dwell.

LAYERED STORIES

All of Wickham's elements, from its water, to its travel networks, to its histories of gathering and making, together represent a multi-layered history. For Aboriginal people, this history stretches back into deep-time.

While some traces of First Nations peoples' early manufacturing remain, much more numerous are the rocks from around the world dumped in the riverbed as excess ballast during the 19th century. Elevated levels of heavy metals – the legacy of the area's industrial use – can still be detected in the sediment of Throsby's Creek.

To turn over soil in a Wickham backyard reveals the white sand layer that has always been there. To celebrate Wickham's multi-layered histories – and those of the future – we must recognise where some traces have been over-written, elevate multiple viewpoints, and let Country itself speak.

PRINCIPLES

PRINCIPLE 1

Re-establish Wickham's essential relationship with water

PRINCIPLE 2

Unlock delightful and protected movement within and through Wickham

PRINCIPLE 3

Create integrated solutions which incorporate key features of Wickham

PRINCIPLE 4

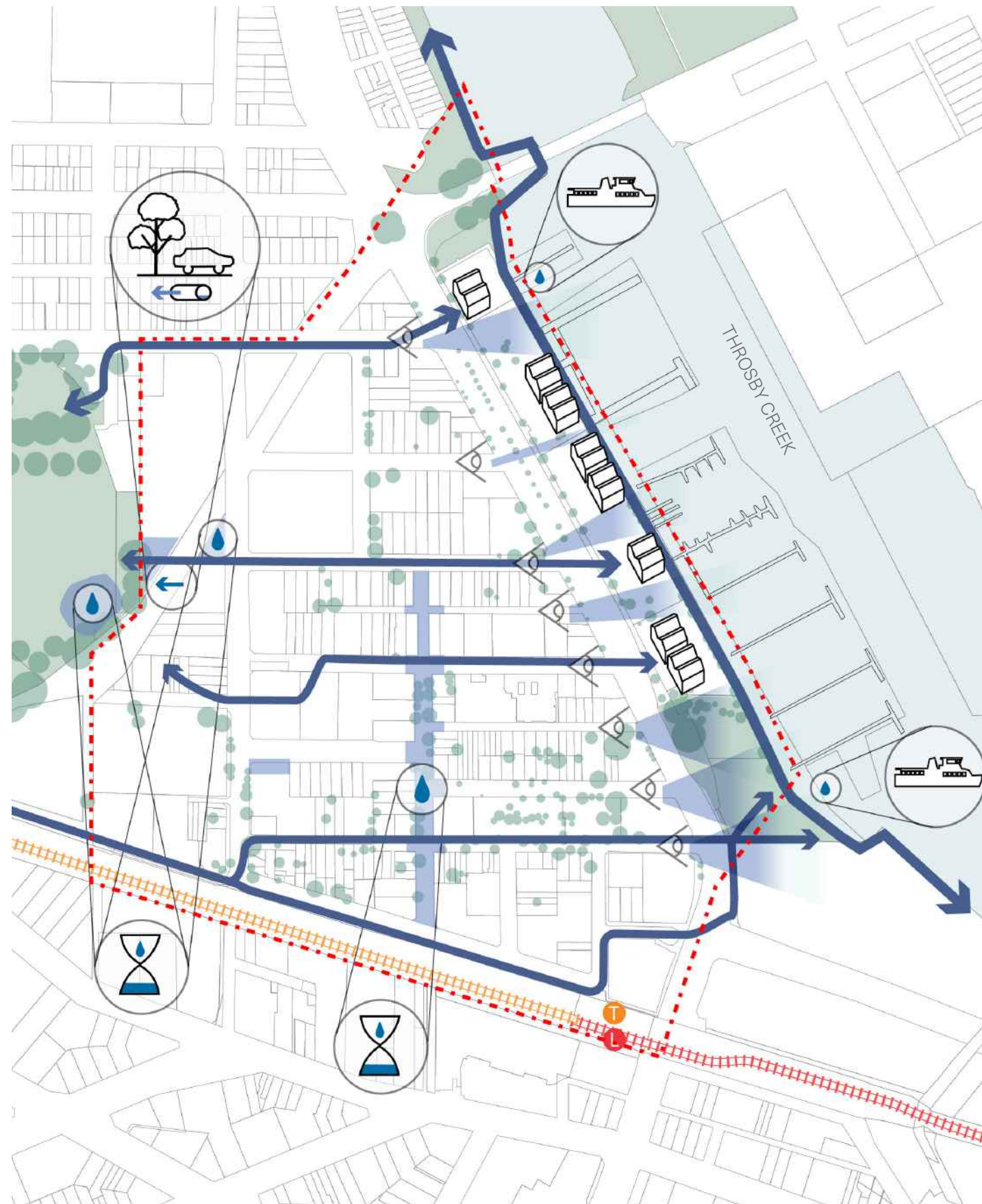
Craft places for continued Wickham community connection and gathering

PRINCIPLE 5

Celebrate the varied, diverse and characterful layers of Wickham

Section 2 – Design Strategies

2.2 Principle 1 - Re-establish Wickham's essential relationship with water



- Improve access to waterfront / water Country from the broader Wickham precinct (including future consideration of a bridge link over Hannell Street) and connect Wickham Park to Throsby creek



- Remove physical and visual barriers between the waterfront and the broader precinct, including the visual barrier of parking along development lots fronting Hannell Street



- Broaden the extent and accessibility of public access to the water along the waterfront, including to the potential new ferry stops and public launch



- Celebrate water stories and features within proposed urban activation areas
- Comprehensively integrate Water Sensitive Urban Design provisions into the streetscape and urban activation areas



- Futureproof the neighbourhood against flooding with the sensitive design of building interfaces with the street

Section 2 – Design Strategies

2.3 Principle 2 - Unlock delightful and protected movement within and through Wickham



- Implement people-focused movement solutions
- Prioritise pedestrians and cyclists and create separation where there are risks or amenity impacts



- Implement abundant green planting for shade and a more pleasant experience for people on foot or bikes
- Balance movement and place outcomes for Union Street as the village spine and protect it from non-local traffic



- Support separated cycling connections linking green spaces in Wickham Park and the waterfront

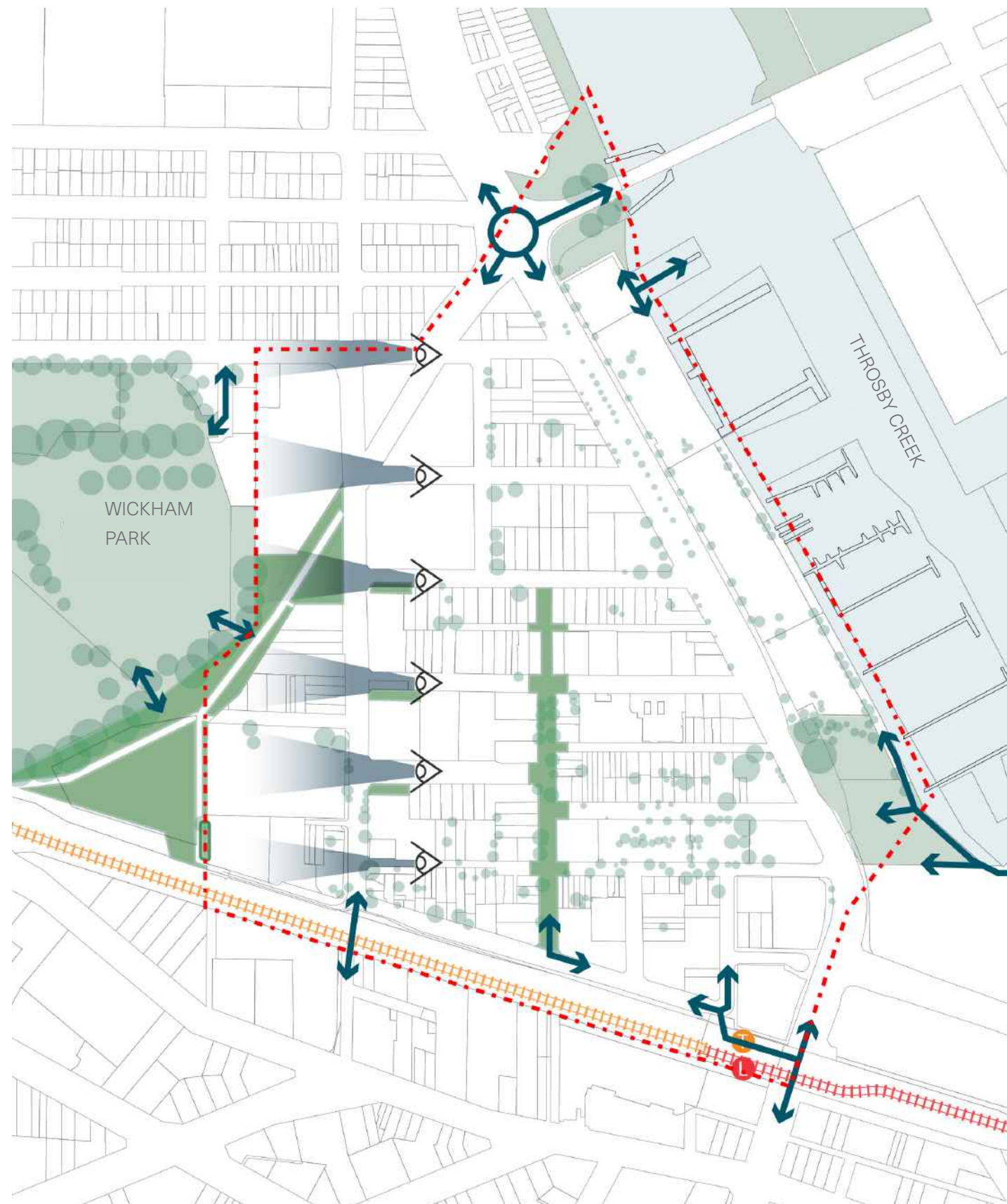


- Create a fine grain network of streets which support walking and cycling
- Creates accessibility throughout the precinct and provide amenity for people whilst moving

- Address needs of commuters, visitors and locals

Section 2 – Design Strategies

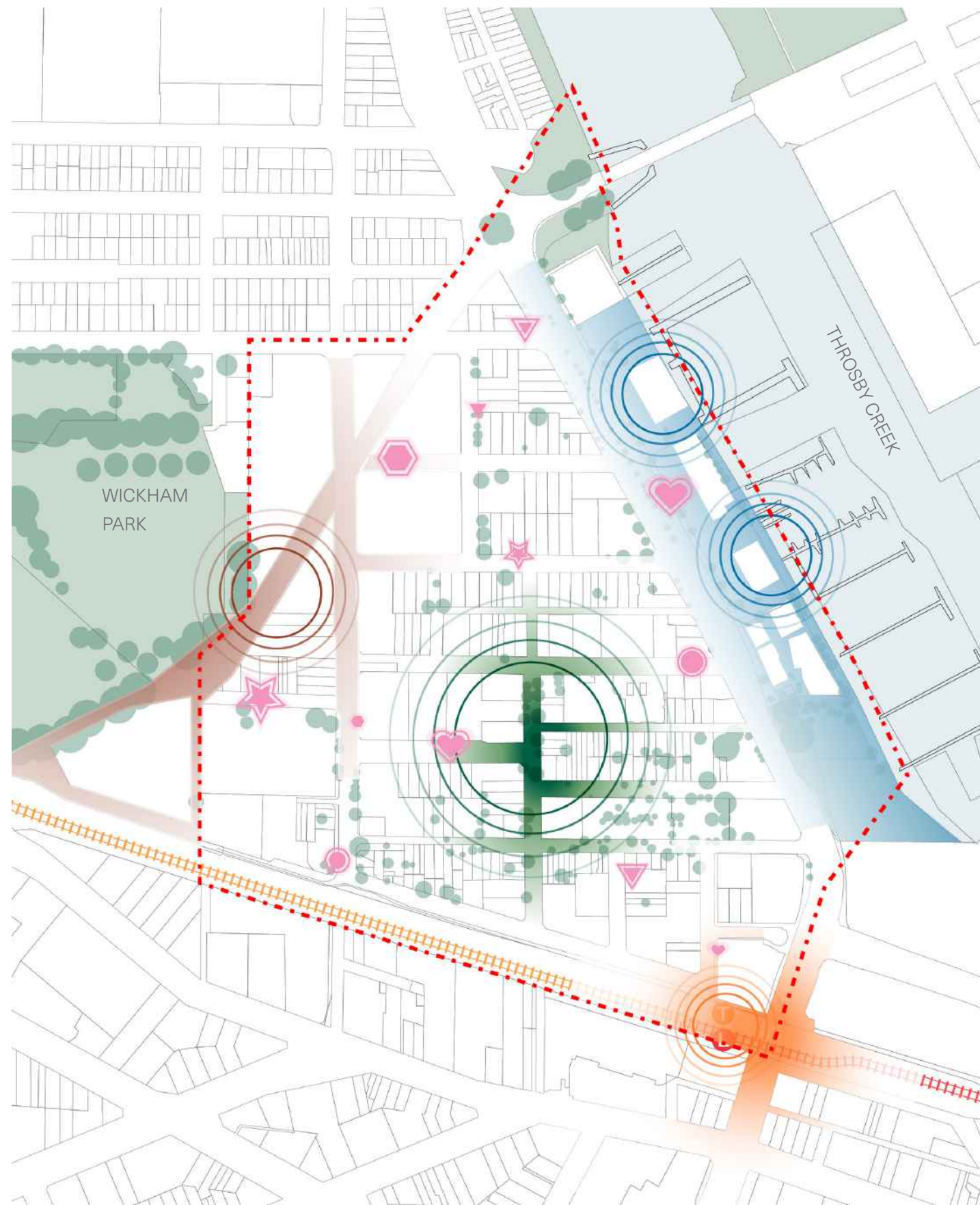
2.4 Principle 3 - Create integrated solutions which incorporate key features of Wickham



- Ensure places are multi-functional and bring together public and place features
- Consider interface to buildings with considered transitions required in levels and from public to private conditions
- Bring together engineering and design solutions into cohesive and place-responsive solutions which unlock access to features of Country
- Support connectivity and the communities' experiences of public space as they transition between Wickham and other parts of the city with consideration of gateway points and features
- Unlocking the edges of Wickham Park particularly along the former rail corridor and integrating to it so that this significant open space supports the community.
- Increase shade and reduce heat impact through generous and careful planting of trees
- Integrate urban activation zones and park edge and consider visual links between Wickham and park

Section 2 – Design Strategies

2.5 Principle 4 - Craft places for continued Wickham community connection and gathering



- Create a safe and vibrant heart to the 'Village Hub'
- Reinforce Union Street as the 'Village Spine' with places to stay, meet and play



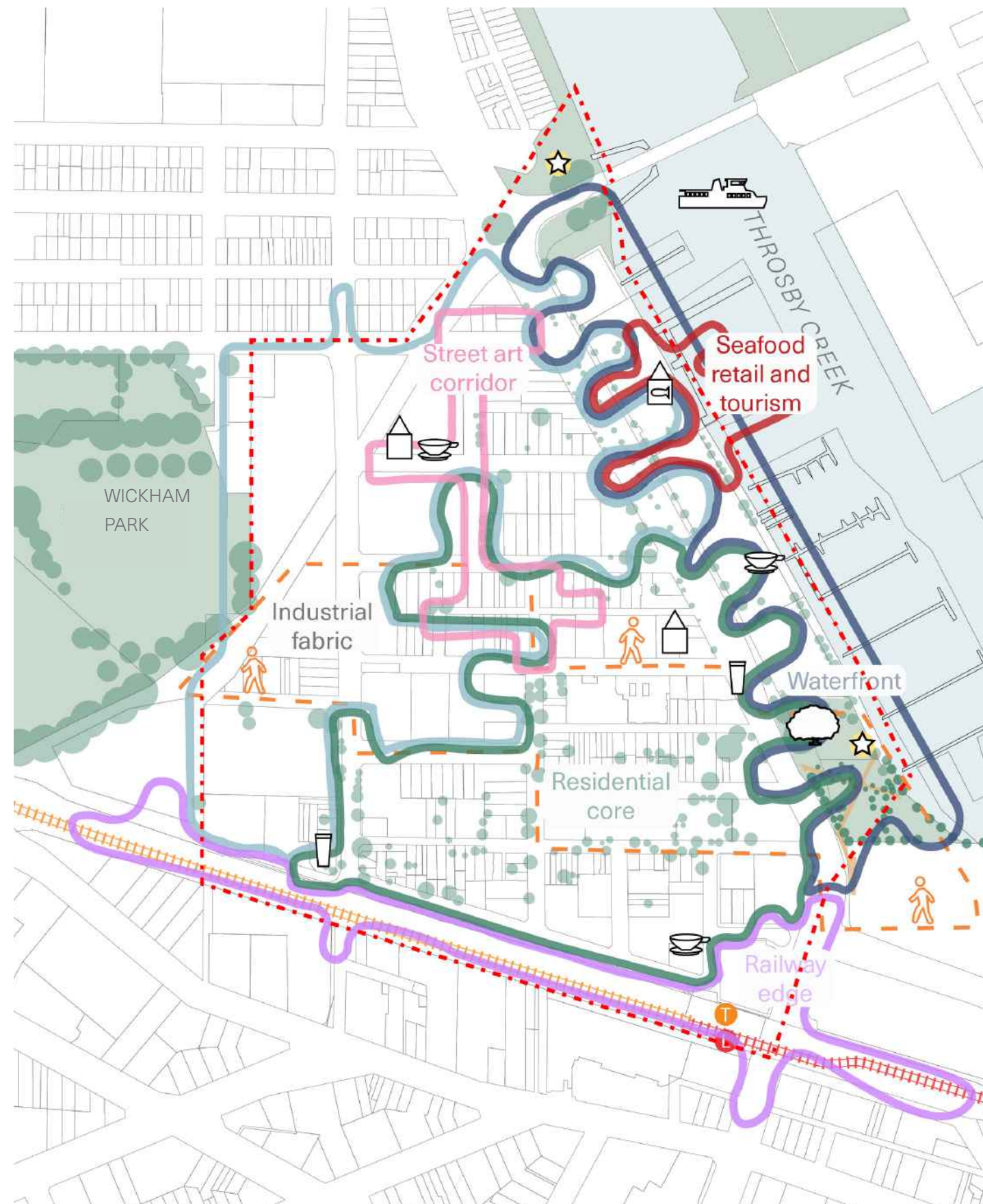
- Establish a hierarchy of scales and uses for other urban activation areas to provide diversity in public space provision
- Enable clear lines of sight to public spaces with planting provisions to support sense of safety
- Ensure high levels of community amenity within new public spaces
- Distribute set of small delightful found elements to be discovered across the precinct



- Continue character as incubator for artistic expression
- Reintegrate with County with a landscape that is both regenerative and biophilic.

Section 2 – Design Strategies

2.6 Principle 5 - Celebrate the varied, diverse, and characterful layers of Wickham



- Reveal traces stories of Wickham across the millennia within the public domain spaces in a layered rather than singular way.



- Undo the over-writing and let layers be read in response to Country
- Public spaces to respond to varied precinct character and recognise natural expansion of character areas as appropriate beyond boundaries given layered nature of evolution of Wickham



- Create opportunities for wandering and material exchange across interpretation across Country and Heritage interpretation proposals, including a foraging trail.
- Retain and celebrate the gritty and quirky character as part of the layering
- Respond to established amenities and existing landmarks, including mature trees

Section 3

Plan & Frameworks



Section 3 – Plan & Frameworks

3.1 Strategic Direction

The strategic directions of the Wickham PDP have been informed by the Principles. These directions represent a multifaceted, cross-disciplinary approach that ensures Wickham is connected, resilient, regenerative, and smart and responds to the needs of its existing and future community.

If carefully implemented, these directions will provide Wickham with a complete pedestrian environment - one in which people travelling on foot or bike or other forms of active travel feel safe, at ease, and prioritised.

Such an outcome will do two things at once. It will provide the best setting for a community to feel comfortable and at home in the street and its adjoining network of public places, driving growth and improving the local and wider economies. This will also best place Wickham’s streetscape to adapt to the challenges of the future, including climate change and the associated increases in risk from the urban heat island effect, extreme precipitation events, and biodiversity loss.

3.1.1 Character & Growth

Country

In line with the City of Newcastle and the community’s commitment to recognising and building on the rich traditions of the Indigenous Custodians of the land on which the community lives and works, the project has considered Country as a foundational element in the project approach. The Wickham Public Domain Plan aims to recognise key perspectives of Country and use this as a foundation upon which we base subsequent understandings of place.

Elements of Place

Wickham’s places have unique and layered meaning to those who call the area home. They are the product of long and deep histories as well as day-to-day activities, rituals, and inhabitation. This PDP has therefore centred the retention of the character of Wickham’s places while enabling the wider evolution of the area and the provision of new places.

Provision has been made for the future integration of public art and interpretation of both Aboriginal living culture and European heritage. Special consideration has been given to the existing uses of Wickham’s places while new places will address the needs of the community.

Changes to traffic access, parking provisions, and increased greening will also result in significantly improved amenity for locals and visitors alike.

Densification

The urban fabric of Wickham is changing in line with the adopted Wickham Master Plan. This change includes an increase in housing density and a proportionate reduction in backyard sizes and private landscapes. This PDP therefore places emphasis on the provision of high quality, publicly available open spaces, which together will form a coherent, suburb-wide network. This network of open space will include new urban activation zones, improvements and upgrades to the greenery and accessibility of streets, and better provision and improvement of existing amenities.

Economy

Careful planning will make sure improvements to Wickham’s public realm also translate into an economic boost. Well designed streets facilitate and encourage movement of pedestrians, cyclists and vehicles, while weather-protected, attractive places encourages people to stay, dwell, and engage with the street. This engagement contributes to the local economy, as cafes and restaurants, retailers, and other vendors see an uptick in traffic.

3.1.2 People-focused Movement

Pedestrian priority

Driving the design outcomes of this PDP is the prioritisation of pedestrian movement. This will be achieved through changes to the street, including kerb build-outs and continuous footpath treatments (CFTs), more wombat crossings, and changes to traffic patterns. Provisions for increases in tree canopy throughout this PDP will also encourage movement by foot, by ensuring the streets stay cool and shaded during hot days.

Wider connections and cycling and public transport

Wickham is very well-connected, through both light and heavy rail networks running out of the Interchange, by bus, and by bike. In addition to reducing carbon emissions, a shift to these modes of transport improves the environment for people moving by foot through Wickham as well. Careful thought has therefore been given to people-focused journeys in and out of Wickham.

This includes dedicated provision for people on bikes, especially through the former Bullock Island Railway corridor, and rationalised and upgraded footpaths to make journeys across the suburb to its key points of arrival and departure safer and more accessible

Accessibility

This PDP has focused on delivering a more accessible Wickham. Accessibility has been reviewed to ensure the best possible outcomes for people of all abilities. This includes balancing footpath widths with vegetation and carefully considering materiality choices and street furniture locations.

3.1.3 Resilience and Sustainability

Climate Change Adaptation

This PDP places great importance on the development of strategies to adapt to climate change. Climate change will continue to present a number of challenges, including increases in extreme weather such as drought, wild fires, storms, and flooding, as well as higher average temperates. These effects can be exacerbated by poor planning and poor public realm design.

At the same time, cities must undertake measures that directly address the root causes of climate change. This includes a just transition from a carbon-intensive economy. City of Newcastle leads in this space, having already committed to the goals of the Paris Climate Agreement and a wide range of initiatives. This PDP

builds on this momentum, not least by improving Wickham’s walkability and encouraging less reliance on vehicles and by embracing green infrastructure.

Urban Heat Island Effect

The urban heat island effect (UHIE) is the result of low levels of greenery and increased heat-absorbing materials causing higher ambient temperatures. This increased temperature puts pressure on building cooling systems, which can increase energy use as well as dump further hot air into the street. UHIE is detrimental to human comfort and can expedite the degradation of infrastructure such as roads and footpaths.

High albedo paving can mitigate the UHI effect by reflecting more heat than low albedo equivalents. Trees and plants also help by cooling through evapotranspiration and through shade. Accordingly, this PDP has carefully chosen materials for their solar properties and provides for the wide introduction of trees and planting throughout the Wickham streetscape.

Biodiversity

As the climate changes, and urban areas expand, it is increasingly important to protect and re-establish biodiversity both in and out of cities. Through the careful design of streets, native flora and fauna can be allowed to thrive - something that is important for Wickham as well as the wider ecological networks of which it is a part. This PDP seeks to maximise planting to best promote biodiversity, not just for the good of Wickham’s human residents but its non-human ones too.

Infrastructure

Essential services infrastructure including water, power, sewer, stormwater, gas and telecommunications run underneath many of Wickham’s streets. This PDP has paid particular attention to the locations of these underground services, especially in the placement of new street trees and water sensitive urban design elements.

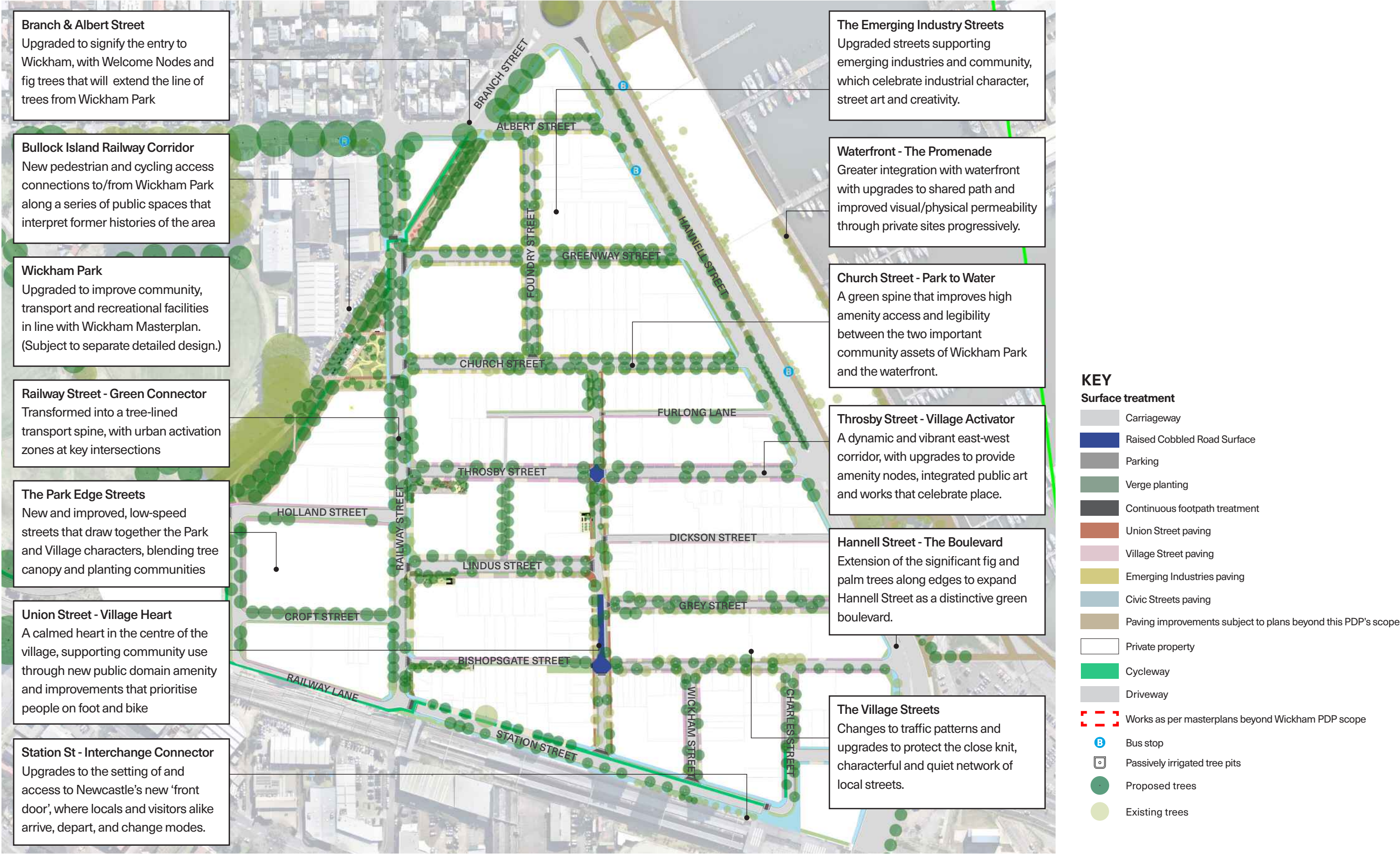
Planting as water detention benefits

In addition to all of the benefits listed above, increased planting can have an important role in managing water. Plants can detain water during precipitation events, slowing down or absorbing surface water. If these plants are properly set within the public realm, they can be passively irrigated, which also lowers overall maintenance costs and improves the health of the plant.

Section 3 – Plan & Frameworks

3.1 Strategic direction

3.1.4 Overview of Public Domain Plan



Section 3 – Plan & Frameworks

3.2 Access & Movement Framework

3.2.1 Transport solutions: summary

The design of the transportation elements within the public domain plan has been informed by the Wickham Masterplan (2017 and 2021 Update), along with input from the Local Area Traffic Management Study (2017) and the Wickham Area Parking Study (2019). Additionally, community consultation has been undertaken by the City of Newcastle Council on the proposed Public Domain Plan.

3.2.2 Pedestrian movement

A key focus of this public domain plan is to enhance pedestrian mobility in and around Wickham. Improving connectivity and accessibility within Wickham plays a pivotal role in realising the City of Newcastle's envisioned transformation of the area. This is an opportunity to enable the activation of the region and the establishment of inviting spaces for the community. This includes introducing 'self-explaining' low speed zones across Wickham, achieved through improvements including:

- Continuous footpath treatments and wombat crossings. These types of crossings calm traffic and ensure pedestrians have priority. They ensure crossing is more accessible for those using wheelchairs or strollers.
- Kerb build outs at intersections. These reduce the crossing distance for pedestrians across side streets and force vehicles to reduce their speed when turning.
- Raised/slow street treatments on Union Street. These treatments act as traffic calming measures and prioritise pedestrian movement in the 'village heart'.
- Improved provision of footpaths and improvements to footpath conditions at locations with high pedestrian activity.

These measures recognise the fundamental role that transport can play in creating pleasant and vibrant urban spaces; ones where people want to spend time in rather than just pass through.

3.2.3 Cycling movement

Wickham's flat topography and its proximity to the waterfront area, the city centre and the Newcastle Transport Interchange makes it an ideal location to promote cycling. The City of Newcastle's On our Bike Plan (2021-2030) states that the majority of car journeys are within 10km, with around 37% covering less than 2km. A cycling trip that replaces a car trip would have positive impacts on level of congestion, emissions, physical activity and transport affordability.

Another key focus of this public domain plan is to facilitate movement in and around Wickham by bike. This will also be achieved through low speed zones, which make cycling a safer and more pleasant experience. Other improvements will include:

- A new separated and bidirectional cycleway along the former Bullock Island Railway Corridor between the Cowper Street/Branch Street roundabout and southern end of Wickham Park.

- A new separated and bidirectional cycleway along the north side of Station Street with wombat crossings (including separated bicycle lanes) providing cycling access to the Newcastle Interchange and connected to the wider Newcastle cycle network by quietways and shared paths.
- An on-street cycleway on the southern side of Railway Lane to connect the Station Street cycleway to Wickham Park and the Bullock Island Railway Corridor.
- A separated and bidirectional cycleway along the southern end of Charles Street connecting the cycleway on Station Street to a new quietway on Dangar Street.

3.2.4 Vehicle movement

Another key focus of this public domain plan is to encourage vehicle movement in and around Wickham in ways that make the area safer, quieter, and with more space for people who are walking or riding bikes. This will be achieved through changes to parking and to vehicle circulation.

Where studies have shown that on-street parking is consistently under-utilised, there is an opportunity to introduce wider footpaths, which will enhance the pedestrian environment and increase space for trees. Changes to vehicle circulation in Wickham are also proposed. These include changes to Union Street in order to prevent it from being used as a northbound through-route by vehicles dropping off or picking up passengers at Newcastle Interchange. Reducing the volume of traffic of Union Street will enhance its role as a safe and green heart of the village.

Some of the general principles for the proposed vehicle circulation changes that has been considered in Wickham include:

- Converting Station Street to two-way between Wickham Street and Railway Street.
- Introducing a one-way eastbound link that connects Maitland Road with Holland Street.
- Introducing a one-way link from Throsby Street into Furlong Lane.
- Providing a new one-way northbound street link between Railway Lane and Holland Street.
- Introducing a one-way westbound traffic link that connects Croft Street to the proposed street link between Railway Lane and Holland Street.
- Operating Railway Lane as one-way westbound.

3.2.5 Union Street

This PDP additionally provides a number of specific measures for Union Street, the key village spine of Wickham. These measures will convert it to a one-way northbound link, and include:

- Changing the current two-way section of Union Street between Bishopsgate Street and Throsby Street to a one-way northbound link.

- Converting the section of Union Street between Throsby Street and Church Street from one-way southbound to one-way northbound.

These routing changes would be done alongside the following additional measures:

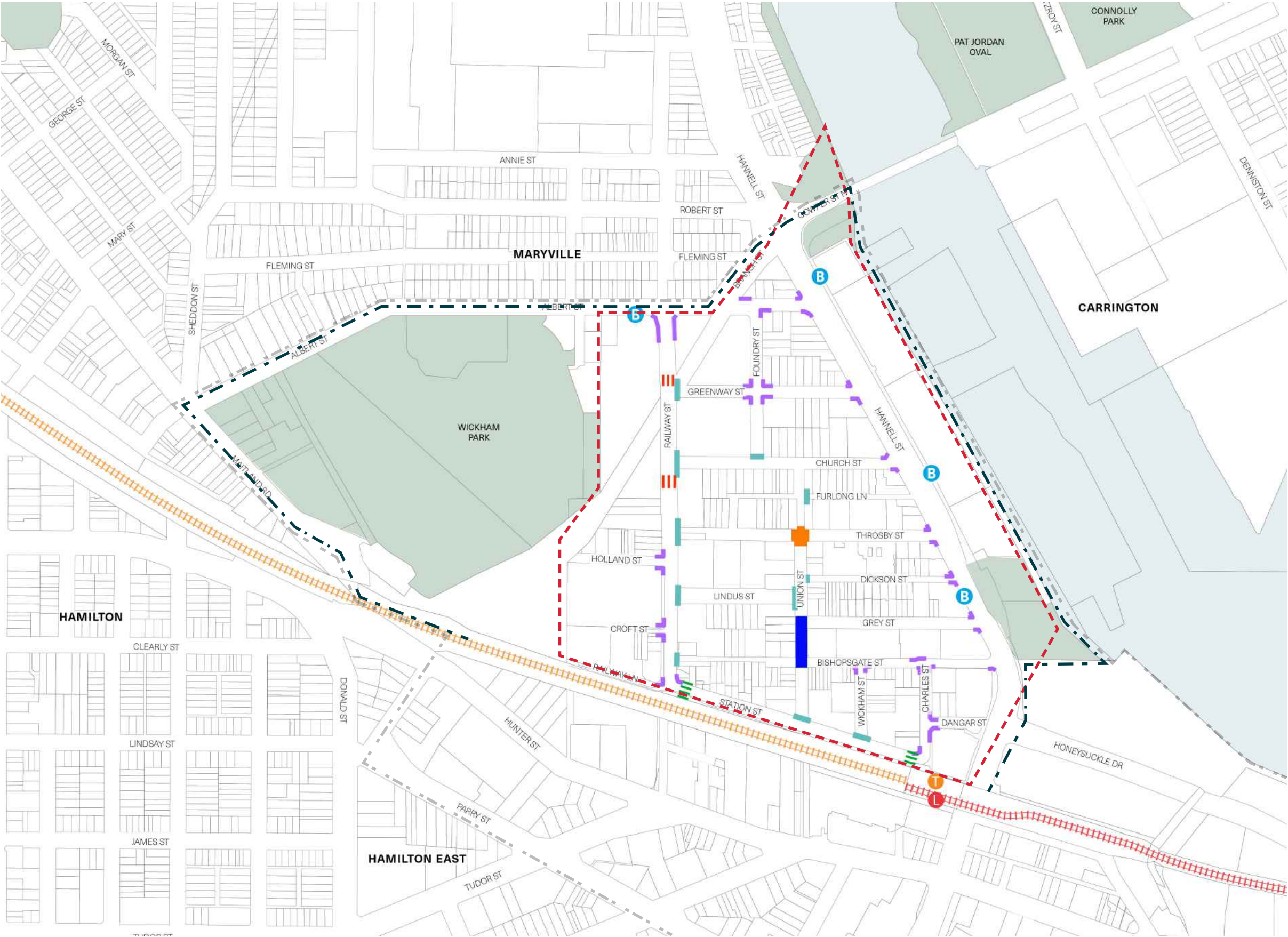
- Providing a raised road treatment between Bishopsgate Street and Grey Street to facilitate traffic calming and improve east-west connectivity for pedestrians.
- Reducing the width of Union Street north of Lindus Street into a one-way network for local vehicle movements only.
- Converting the very narrow section of Bishopsgate Street between Railway Street and Union Street into a one-way westbound link with parking on the northern side.



Artist's impression of Station Street once PDP initiatives have been fully implemented.

3.2 Access & Movement Framework

3.2.6 Pedestrian Movement



KEY

Land cover

- Green space / open space
- Surface and tidal water

Traffic management

- Kerb extension
- Continuous footpath treatments (CFT)
- Raised road platform
- Raised intersection
- Wombat crossing delivered as part of Stage 1
- At-grade crossing delivered as part of Stage 1 and wombat crossing delivered during Stage 2

Transport

- Heavy rail stop and alignment
- Light rail stop and alignment
- Bus stops

Boundaries

- Newcastle City Centre area
- Wickham Masterplan area
- PDP Focus area

3.2 Access & Movement Framework

3.2.7 Proposed bicycle infrastructure



KEY

Land cover

- Green space / open space
- Surface and tidal water

Bicycle infrastructure

- Active transport connection proposed in 2021 Wickham masterplan
- Existing shared user path
- Existing on-road cycle lane
- Proposed routes of Newcastle Cycling Plan

Transport

- Heavy rail stop and alignment
- Light rail stop and alignment
- Bus stops

Boundaries

- Newcastle City Centre area
- Wickham Masterplan area
- PDP Focus area

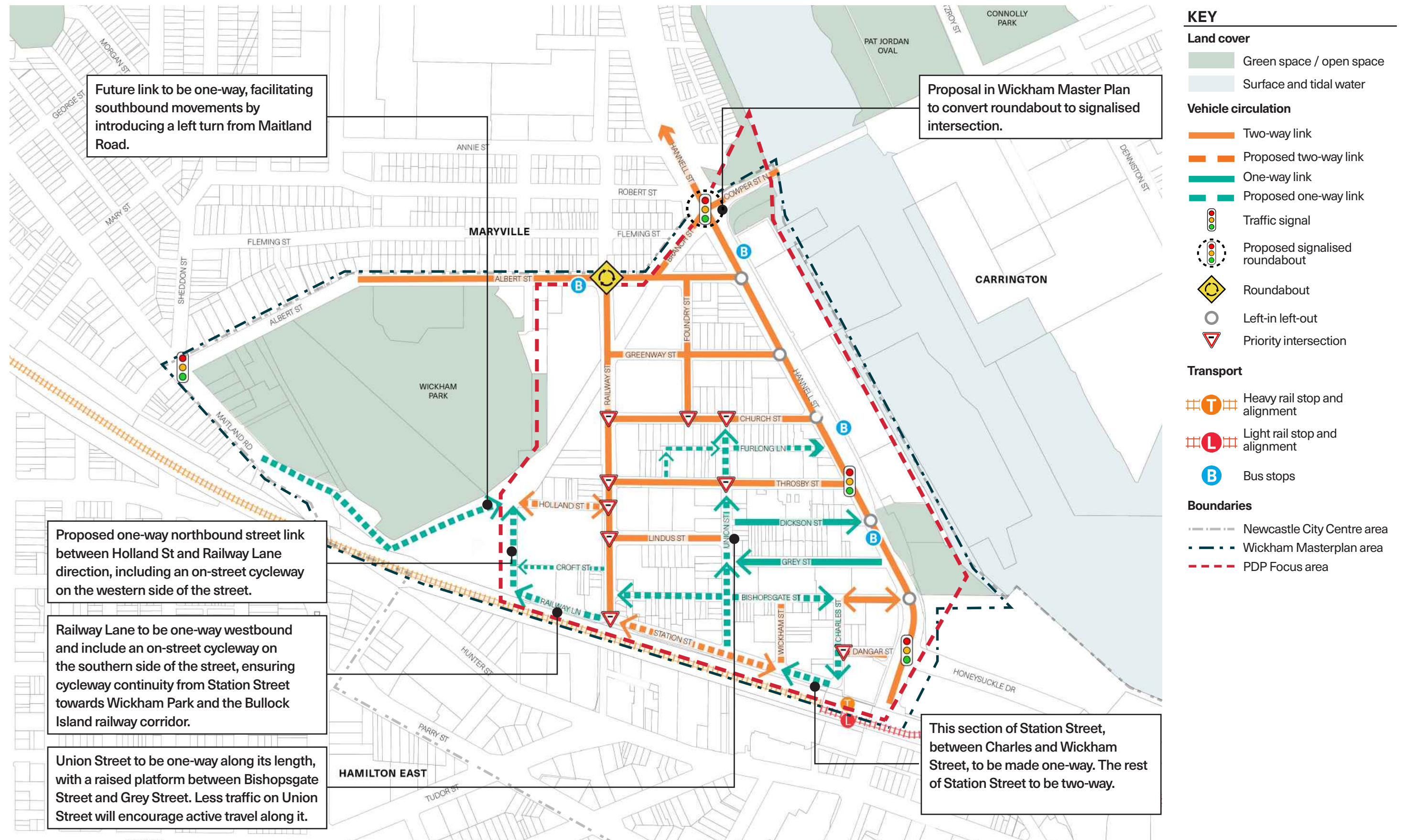
Note: Explore network of quietway designation to all local streets with requirements;

- Bikes travel in the centre of the traffic lane
- Speed is under 30 km/h
- Road width is under 3m per lane

Section 3 – Plan & Frameworks

3.2 Access & Movement Framework

3.2.8 Proposed vehicle circulation



3.3 Street approach

3.3.1 Street approach overview

This PDP has developed a comprehensive approach for an integrated and carefully designed street network. This approach starts by considering the existing scale and use of each street, then classifies that street’s place potential within the Movement and Place framework, before finally assigning each street to a broader “character area”. These characters are govern paving treatment and other design work to best ensure a coherent and structured public domain.

3.3.2 Existing scale

Wickham is a residential suburb bound by the major traffic artery of Hannell Street on the east and Albert Street to the north. Major roads within Wickham itself include Railway Street, which runs north-south and divides most of Wickham from Wickham Park further west, and Station Street, which runs parallel to the heavy rail corridor approaching the Interchange, defining Wickham’s southern edge.

Wickham’s streets can be classified by scale, including 20-metre-wide collector streets (Railway Street, Albert Street, and Branch Street), 15-metre-wide collector streets (Throsby Street and Church Street), local streets between 10 and 15 metres wide, local streets narrower than 10 metres, and laneways.

3.3.3 Movement and place

While important, road type and scale are just one aspect of design for place. Therefore, this PDP has carefully considered the Movement and Place framework, a cross-government framework for planning, designing and managing NSW’s transport networks to best deliver benefits for the people and places they serve.

This PDP has identified how Wickham’s streets could be classified according to this framework’s guidance once the recommended upgrades and improvements of the plan have been undertaken. These classifications are as follows:

Main streets

- Station Street, Railway Street, Branch Street, and Albert Street

Civic spaces

- Union Street

Main roads

- Hannell Street

Local streets

- All other streets

3.3.4 Character areas

In order to provide guidance on how best to realise the place potential of each of Wickham’s streets, this PDP has considered the character of each street and the wider urban context in which it sits. These characters areas directly respond to guidance issued in the Wickham Master Plan.

The result is a number of character areas that build on the Movement and Place framework but also consider the wider context and connections of each street. These character areas are defined by features and characteristics the streets have common - or by the qualities that make some of Wickham’s street unique.

These characters areas are:

- Union Street
- Throsby Street
- The Village Streets
- The Emerging Industries Streets
- The Park Edge Streets
- The Civic Streets

3.3.5 Union Sreet

Running through the centre of Wickham is Union Street, a green, linear “heart” with important community functions and a unqiue character, including heritage paving and sculptures. As the village heart, special attention has been paid to Union Street, and many of its improvements reflect the ambition of the PDP as a whole, from a new urban activation zone, to traffic calming and measures to limit traffic to just local traffic, to improved and upgraded footpaths that celebrate its existing character while increasing its accessibility for all.

3.3.6 Throsby Street

With its intersection with Union Street at the very centre of Wickham, Throsby is an important high street. This PDP provides for a full width pavement, in line with its role for heavier traffic, supporting retail as well as residential.

3.3.7 The Villages Streets

This collection of streets form the fine-grained, residential fabric of Wickham. They tend to be quieter, narrower, and shaded, with a street canopy that merges with that of private front yards and back yards.

3.3.8 The Emerging Industries Streets

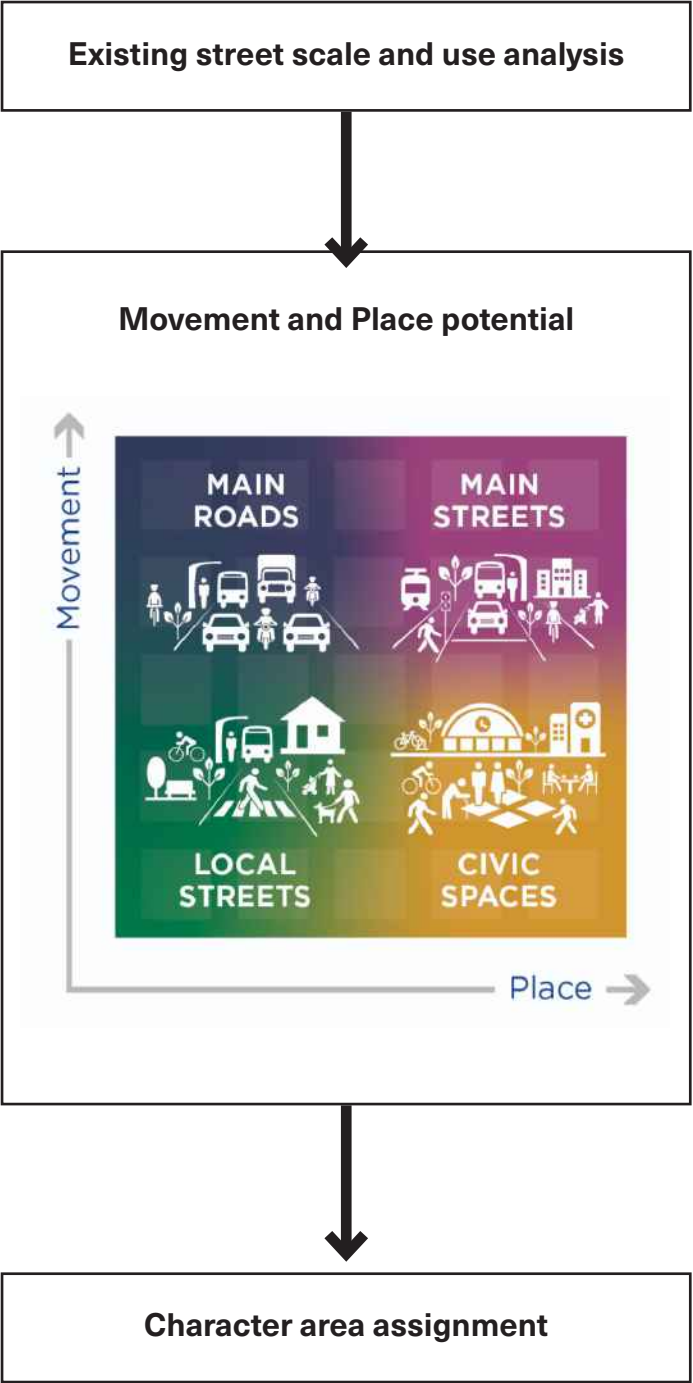
These are the streets in the north of Wickham, whose character is derived from its historic industrial fabric and street art and the emerging creative practices now taking up residence within them.

3.3.9 The Park Edge Streets

These are the streets between Wickham Park and Railway Street, and include new streets that will unlock this portion of Wickham. Their role is to convey people between Wickham’s village heart and its most important green asset, Wickham Park. Taking advantage of their newness, these streets will have generous water sensitive urban design provision, further blending the mature greenery of the park with wider Wickham.

3.3.10 The Civic Streets

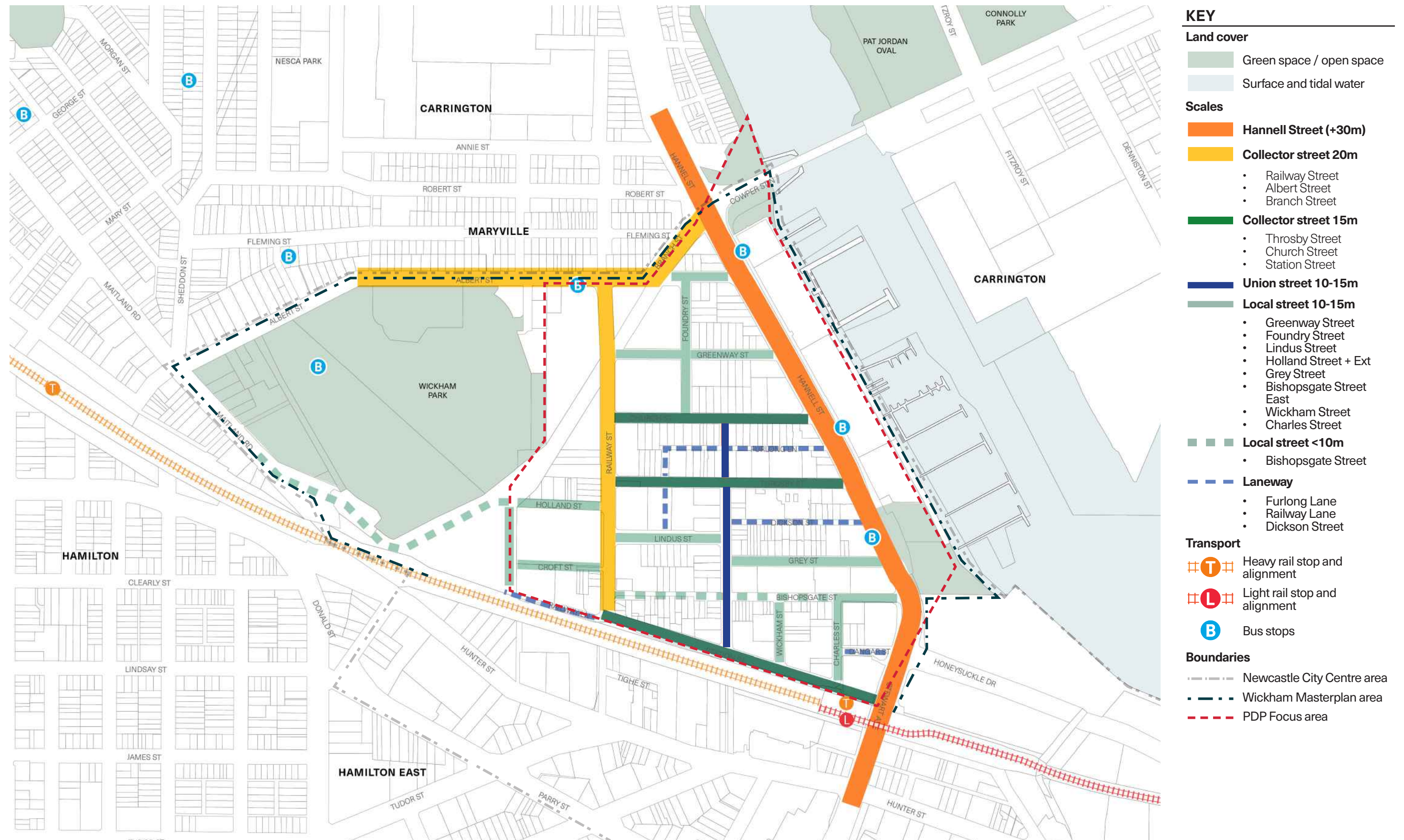
The Civic Streets are the primary streets of Wickham: Railway Street, Dangar Street, Charles Street, Albert Street, Branch Street, and Station Street. These are the busiest routes for both people on foot, on bike, and in cars, and server as major connectors in, out, and across Wickham. Accordingly, these streets have the widest footpaths and a paving palette that is consistent with Newcastle’s other major civic thoroughfares, such as Hunter Street.



Section 3 – Plan & Frameworks

3.3 Street typologies

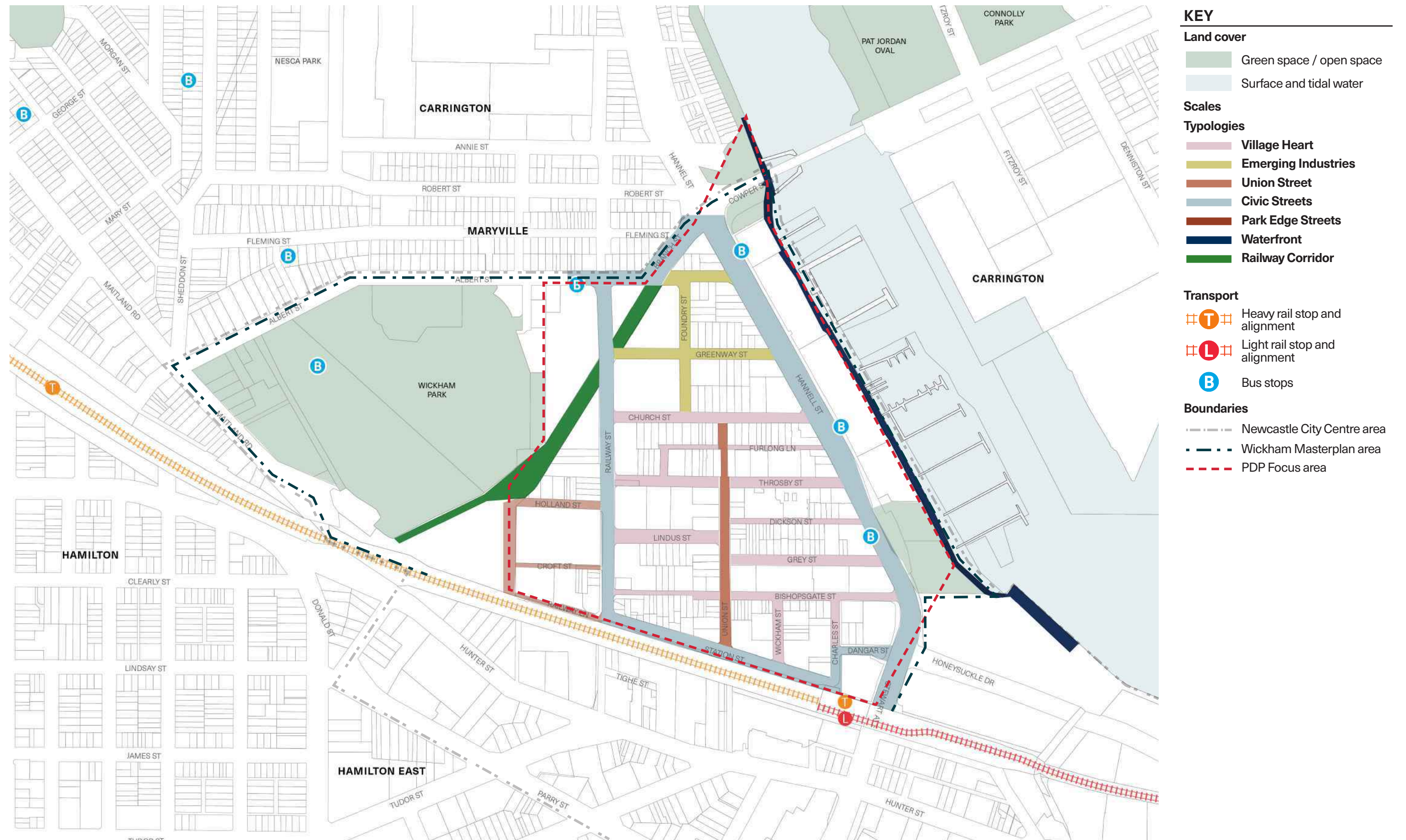
3.3.11 Street Scales



Section 3 – Plan & Frameworks

3.3 Street typologies

3.3.12 Street character by area



Section 3 – Plan & Frameworks

3.3 Street typologies

3.3.13 Union Street south (Village Heart)

A calmed heart in the centre of the Village, supporting community use through new public domain amenity, greening, and traffic changes that prioritise people on foot and bike and restrict car traffic to locals only.

Key Moves:

- Create a green heritage spine
- Improve health of established trees
- Create spaces for rest, dwelling, socialising and play
- Raised section between Bishopsgate St and Grey St
- Enhance setting of existing heritage pavers and bollards



KEY

Surface treatment

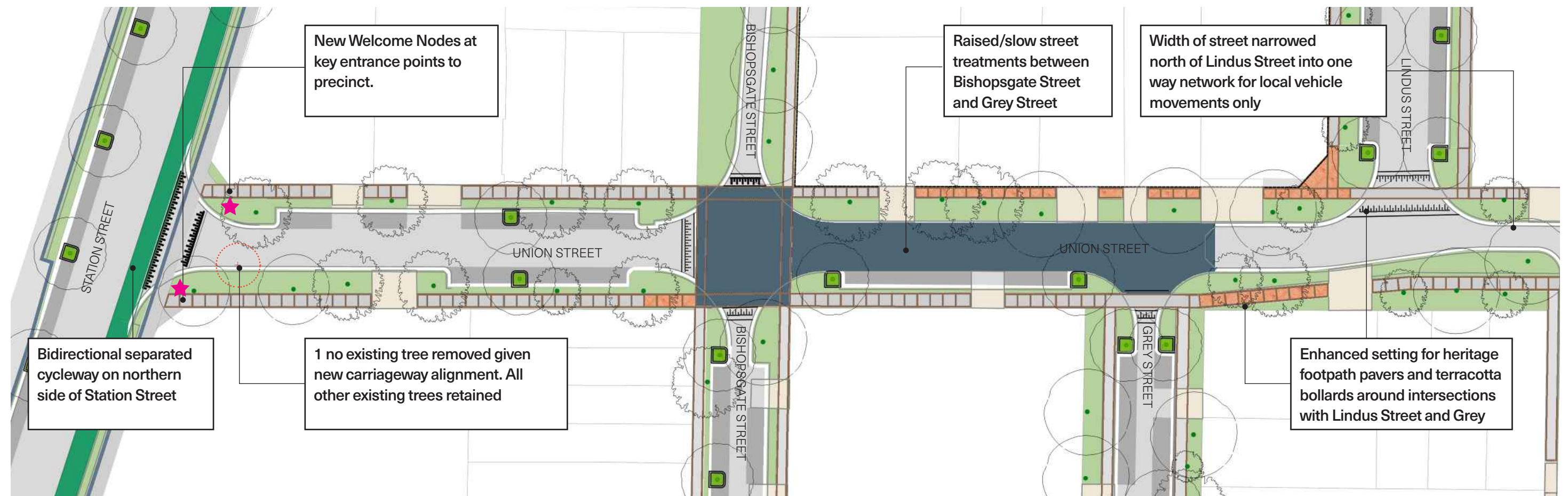
- Carriageway
- Parking
- Verge planting
- Raised intersection paving
- Union Street heritage brick paving
- Union Street standard paving
- Other paving typology with concrete infill and trim material as per paving typology
- Driveway
- Cycleway
- Shared zone
- Private property

Street furniture

- Interpretation Node

Trees and associated elements

- Tree grate
- Passively irrigated tree pits
- Existing small tree
- Existing medium tree
- Proposed small tree
- Proposed medium tree



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DATE 02.11.2023

Section 3 – Plan & Frameworks

3.3 Street typologies

3.3.14 Union Street north (Village Heart)

A calmed heart in the centre of the Village, supporting community use through new public domain amenity, greening, and traffic changes that prioritise people on foot and bike and restrict car traffic to locals only.

Key Moves:

- Create a green heritage spine
- Improve health of established trees
- Create spaces for rest, dwelling, socialising and play
- Primary footpath on east side, with open green space between Furlong Lane and Church Street
- Enhanced setting of existing heritage pavers and bollards
- Raised crossing at intersection of Union Street and Throsby Street



KEY

Surface treatment

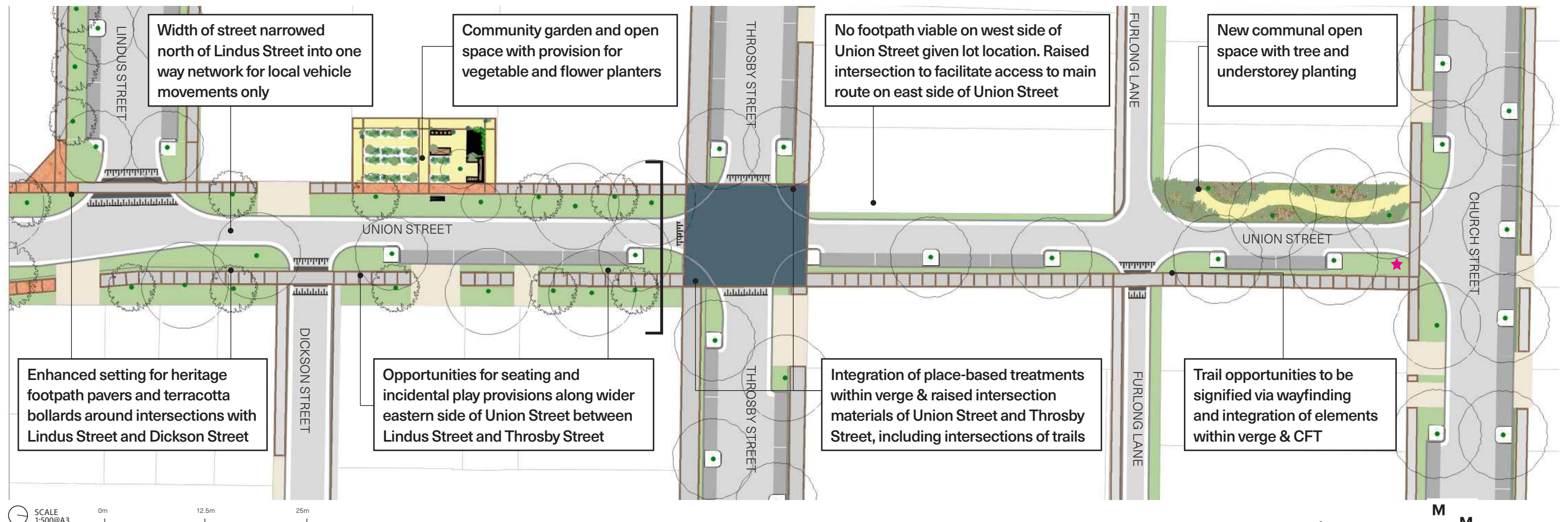
- Carriageway
- Parking
- Verge planting
- Raised intersection paving
- Union Street heritage brick paving
- Union Street standard paving
- Other paving typology with concrete infill and trim material as per paving typology
- Driveway
- Deconstructed granite path
- Cycleway
- Shared zone
- Private property

Street furniture

- Interpretation Node
- Public bench

Trees and associated elements

- Tree grate
- Passively irrigated tree pits
- Existing small tree
- Existing medium tree
- Proposed small tree
- Proposed medium tree



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0m 12.5m 25m

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02.11.2023

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Section 3 – Plan & Frameworks

3.3 Street typologies

3.3.15 Union Street section



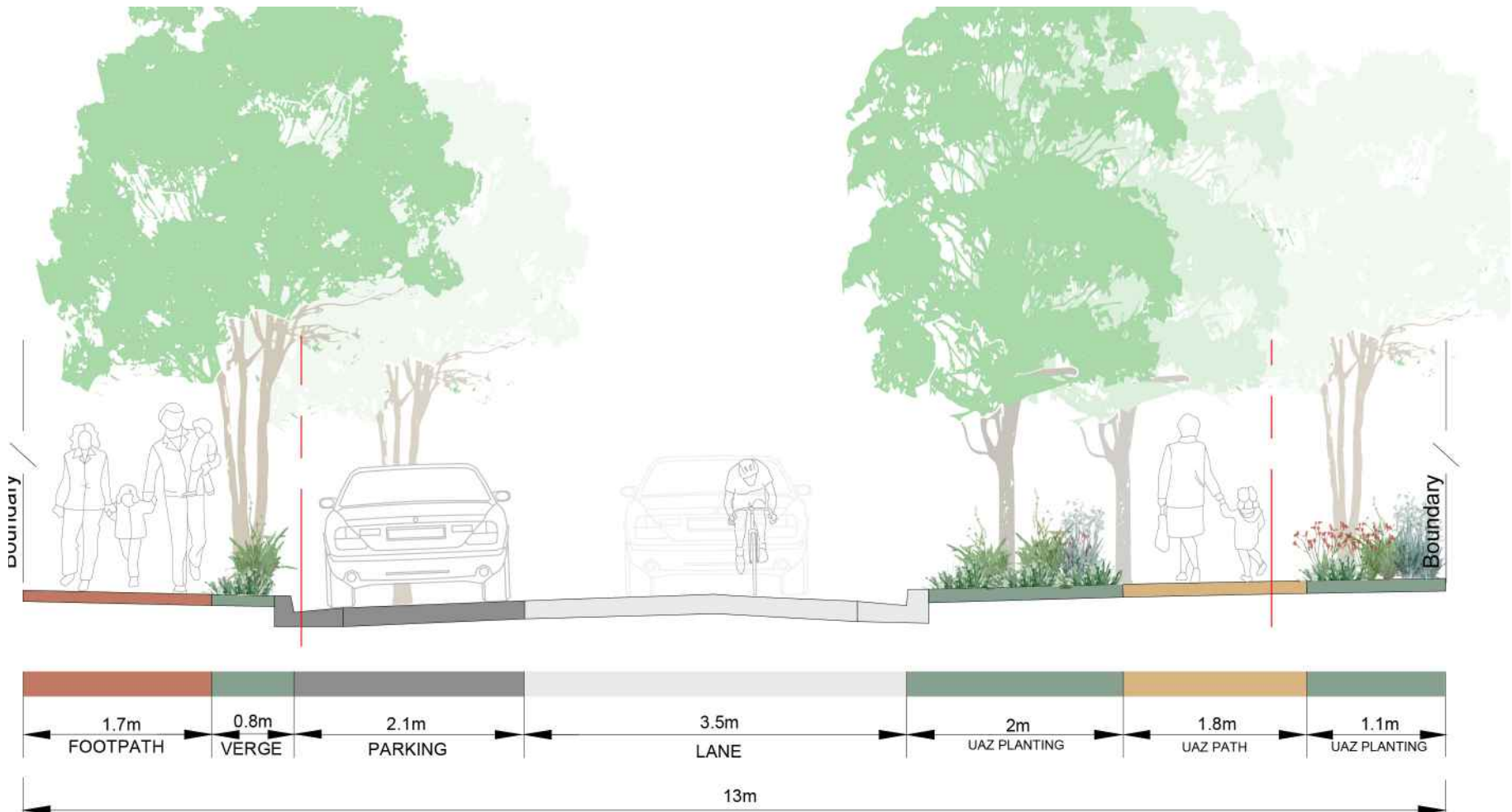
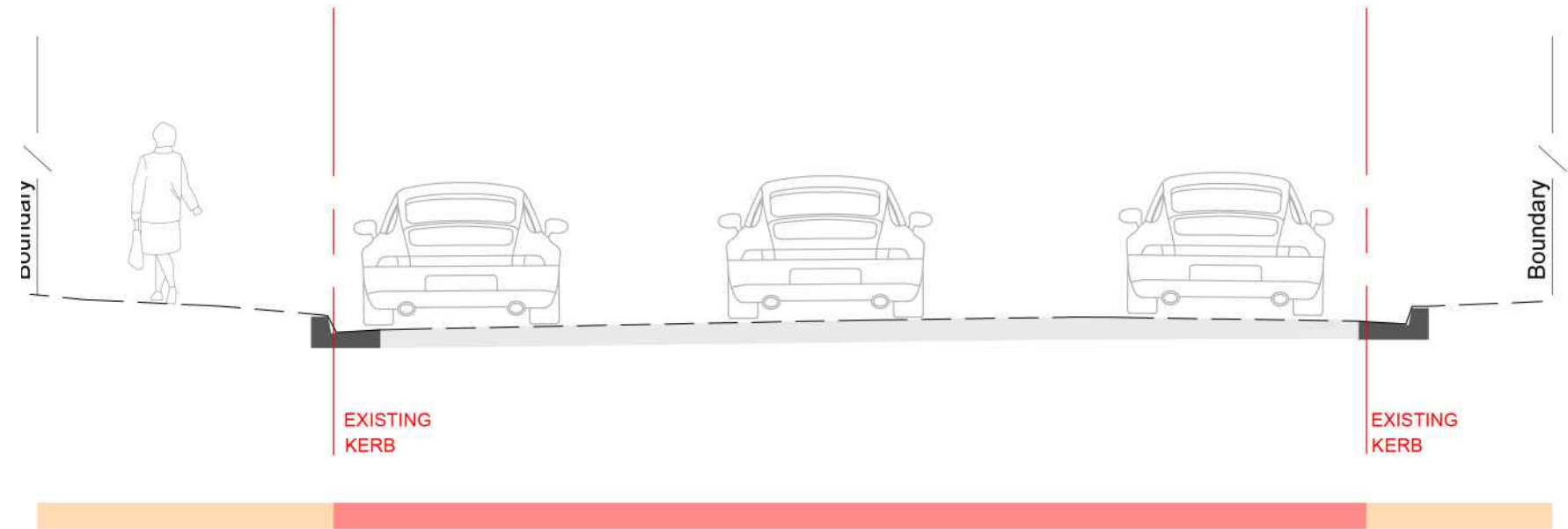
Key map



Existing



Precedent



Section 3 – Plan & Frameworks

3.3 Street typologies

3.3.16 Station Street (Interchange Connector)

Upgrades to the setting and access of Newcastle's new 'front door', where locals and visitors alike arrive, depart, and change modes.

Key Moves:

- Dedicated two-way cycleway along north side on Station
- New planted verge along south side of Station
- Onward cycle connections on Charles St in east and Railway Lane in west
- Accessible parking by station maintained



KEY

Surface treatment

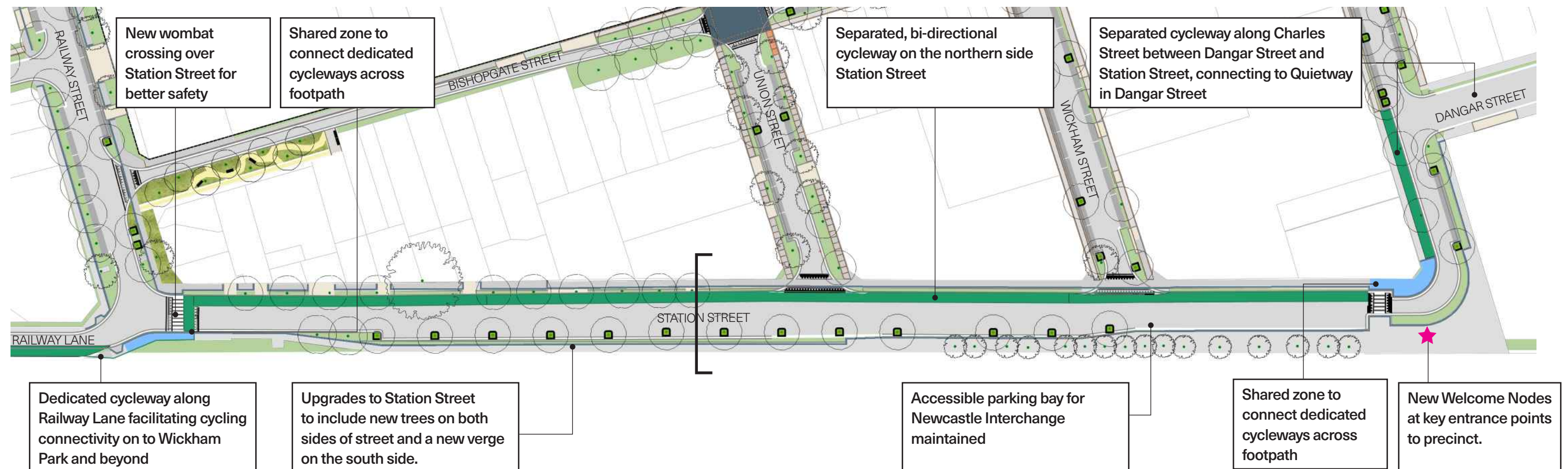
- Carriageway
- Parking
- Verge planting
- Raised intersection paving
- Union Street heritage brick paving
- Union Street standard paving
- Other paving typology with concrete infill and trim material as per paving typology
- Driveway
- Deconstructed granite path
- Cycleway
- Shared zone
- Private property

Street furniture

- Interpretation Node

Trees and associated elements

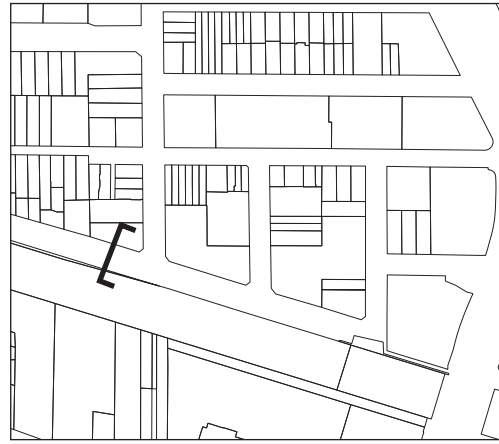
- Tree grate
- Passively irrigated tree pits
- Existing small tree
- Existing medium tree
- Proposed small tree
- Proposed medium tree



Section 3 – Plan & Frameworks

3.3 Street typologies

3.3.17 Station Street (Interchange Connector)



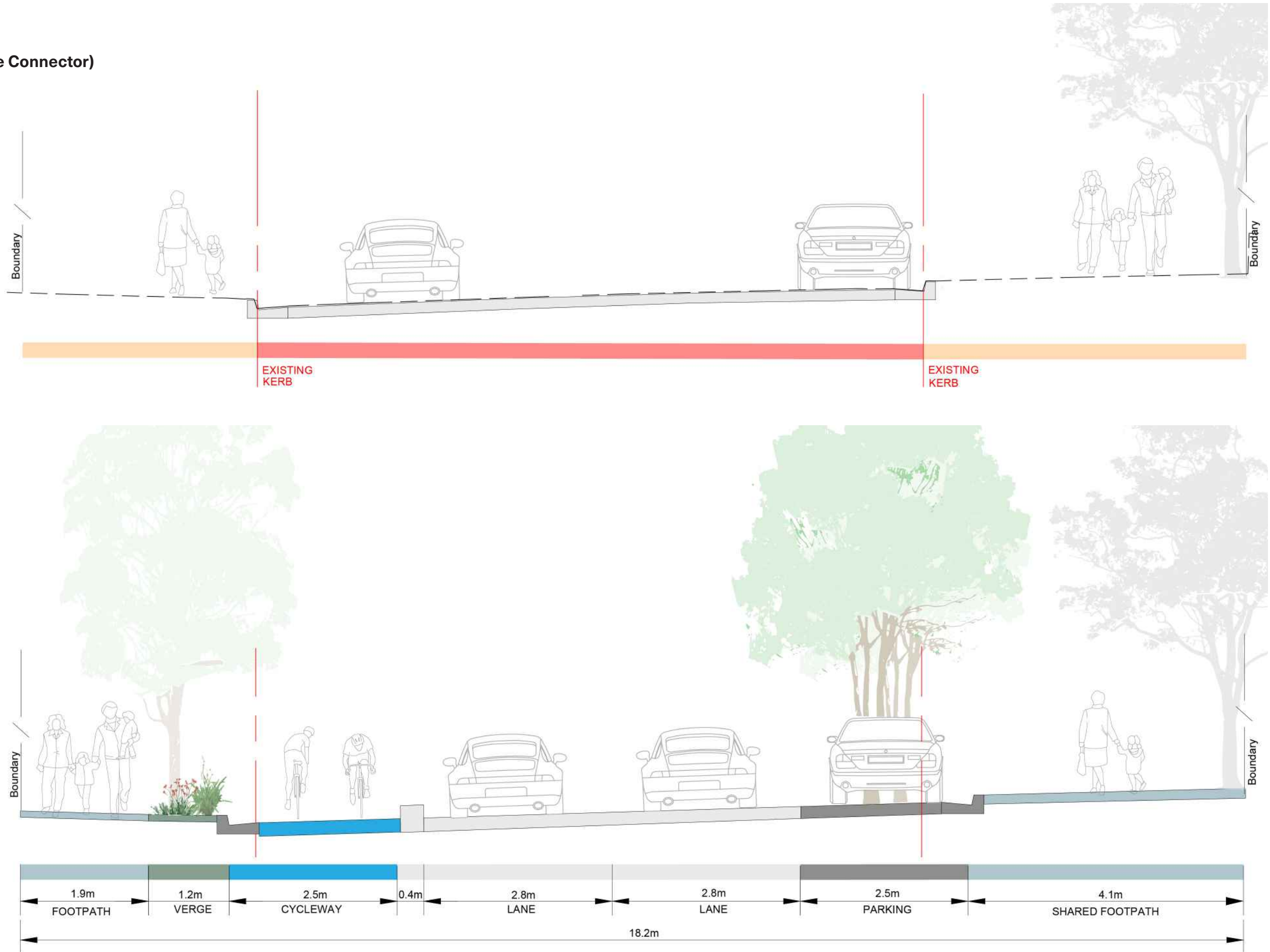
Key map



Existing



Precedent



Section 3 – Plan & Frameworks

3.3 Street typologies

3.3.18 Wide Collector Streets - Railway Street (Green Connector)

Transformed into a green street and key transport spine of Wickham, providing quality multi-modal movements

Key Moves:

- New public spaces at the corners of Throsby Street and Lindus Street plus a new urban wetland and active recreation spaces at the intersection of the former railway corridor
- Traffic calming and CFTs at intersections to ensure a safe and quiet environment



KEY

Surface treatment

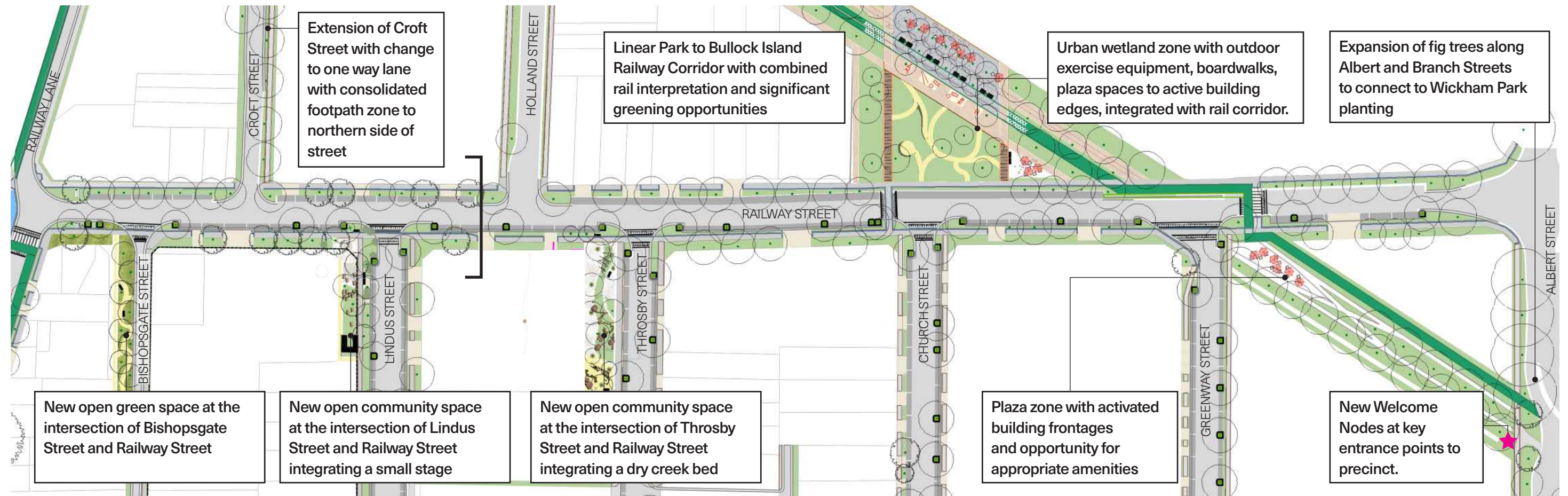
- Carriageway
- Parking
- Verge planting
- Union Street heritage brick paving
- Union Street standard paving
- Other paving typology with concrete infill and trim material as per paving typology
- Driveway
- Deconstructed granite path
- Cycleway
- Shared zone
- Private property

Street furniture

- Interpretation Node

Trees and associated elements

- Tree grate
- Passively irrigated tree pits
- Existing small tree
- Existing medium tree
- Proposed small tree
- Proposed medium tree



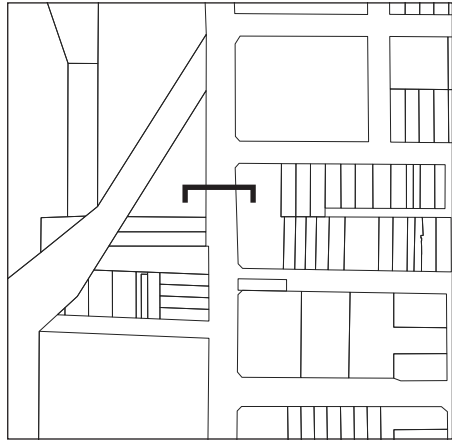
SCALE 1:1250@A3 0m 25m 50m

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Section 3 – Plan & Frameworks

3.3 Street typologies

3.3.19 Wide Collector Streets - Railway Street (Green Connector)



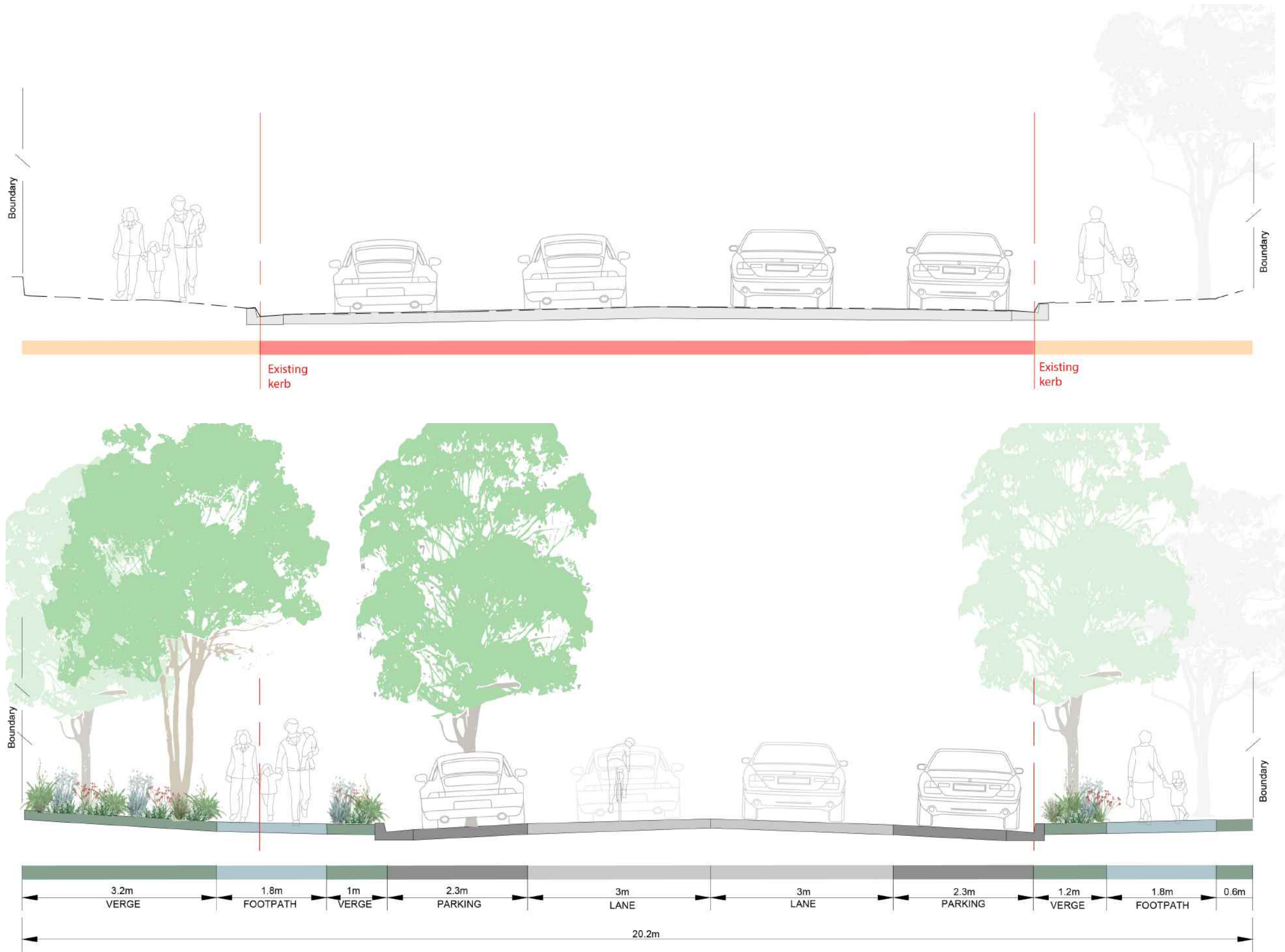
Key map



Existing



Precedent



Section 3 – Plan & Frameworks

3.3 Street typologies

3.3.20 Local Collector Streets - Throsby Street (Village Activator)

Overview

A dynamic and vibrant east-west corridor, with upgrades to provide amenity nodes, integrated public art and interpretation that celebrate place features.

Key Moves:

- Brick trim with standard concrete infill panels
- Full width footpaths in line with Throsby's important and vibrant role as Village Activator
- Raised intersection with Union Street at Wickham's very centre
- New urban activation zone integrating a dry creek and wild play elements at the intersection with Railway Street



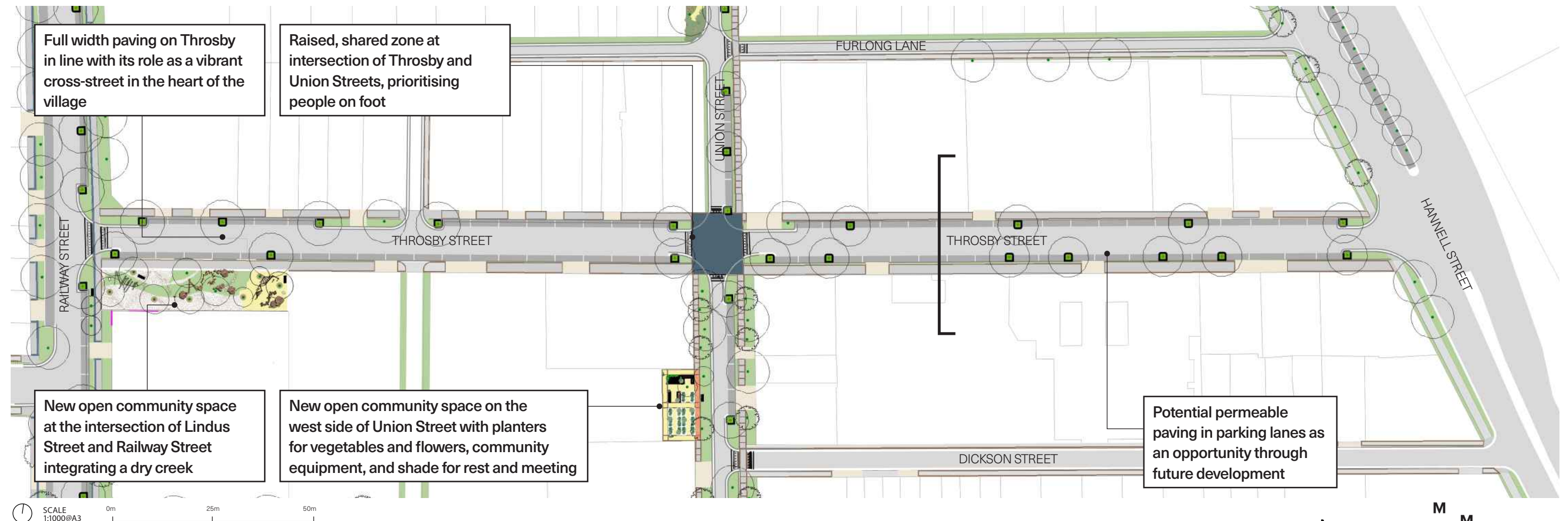
KEY

Surface treatment

- Carriageway
- Parking
- Verge planting
- Raised intersection paving
- Union Street heritage brick paving
- Union Street standard paving
- Other paving typology with concrete infill and trim material as per paving typology
- Driveway
- Deconstructed granite path
- Cycleway
- Shared zone
- Private property

Trees and associated elements

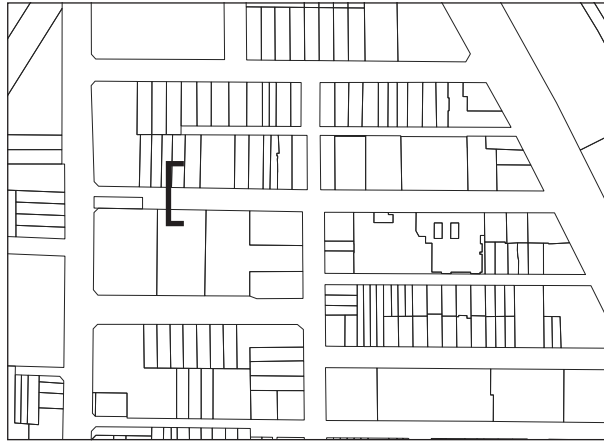
- Tree grate
- Passively irrigated tree pits
- Existing small tree
- Existing medium tree
- Proposed small tree
- Proposed medium tree



Section 3 – Plan & Frameworks

3.3 Street typologies

3.3.21 Local Collector Streets - Throbsy Street (Village Activator)



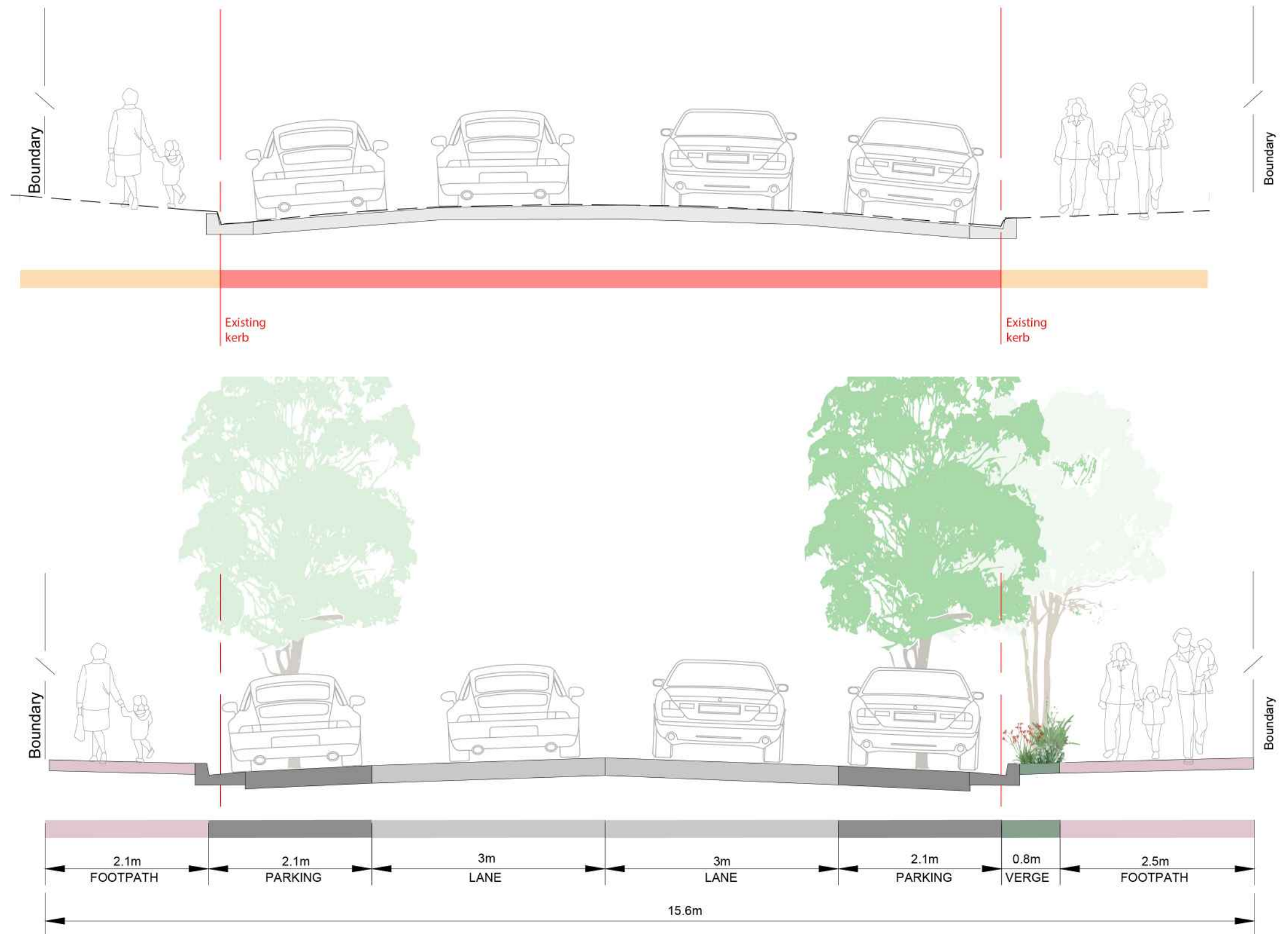
Key map



Existing



Precedent



Section 3 – Plan & Frameworks

3.3 Street typologies

3.3.22 Local Streets and Lanes - One-way

Key Moves:

- Changes to traffic patterns and upgrades to protect the closeknit, characterful and quiet network of local streets
- Careful balance between footpath width, existing constraints, and verge planting

Applicable streets:

- Furlong Lane & Extension
- Dickson Street
- Grey Street
- Bishopsgate Street between Railway Street and Charles Street
- Charles Street
- Railway Lane
- Maitland Road Extension
- New Street between Railway Lane and Holland Street



KEY

Surface treatment

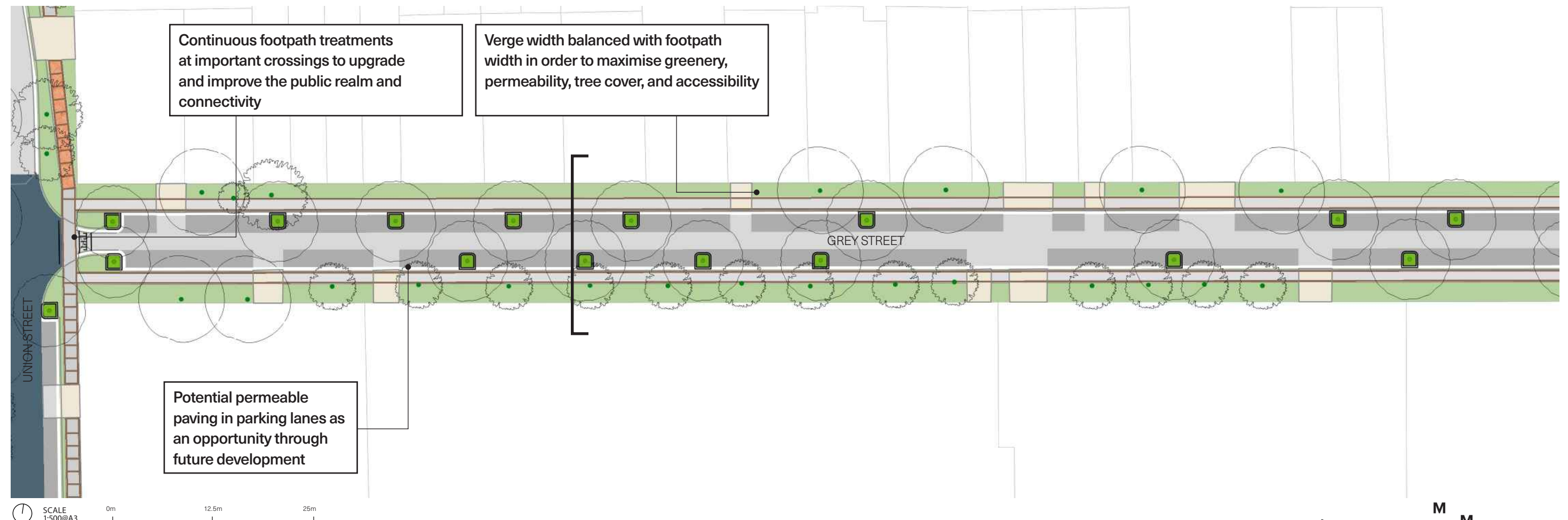
- Carriageway
- Parking (Permeable Paving)
- Verge planting
- Raised intersection paving
- Union Street heritage brick paving
- Union Street standard paving
- Other paving typology with concrete infill and trim material as per paving typology
- Driveway
- Cycleway
- Shared zone
- Private property

Street furniture

- Interpretation Node

Trees and associated elements

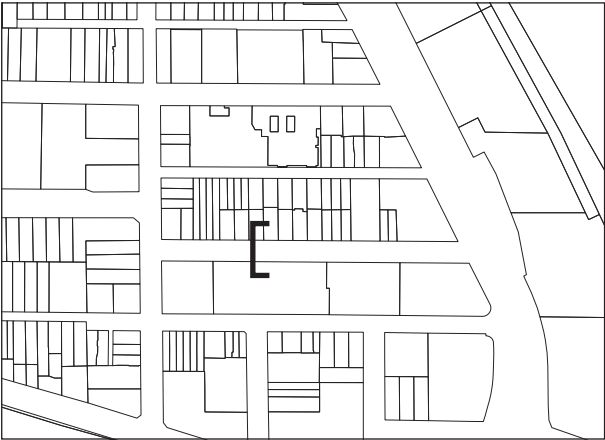
- Tree grate
- Passively irrigated tree pits
- Existing small tree
- Existing medium tree
- Proposed small tree
- Proposed medium tree



Section 3 – Plan & Frameworks

3.3 Street typologies

3.3.23 Local Streets and Lanes - One-way



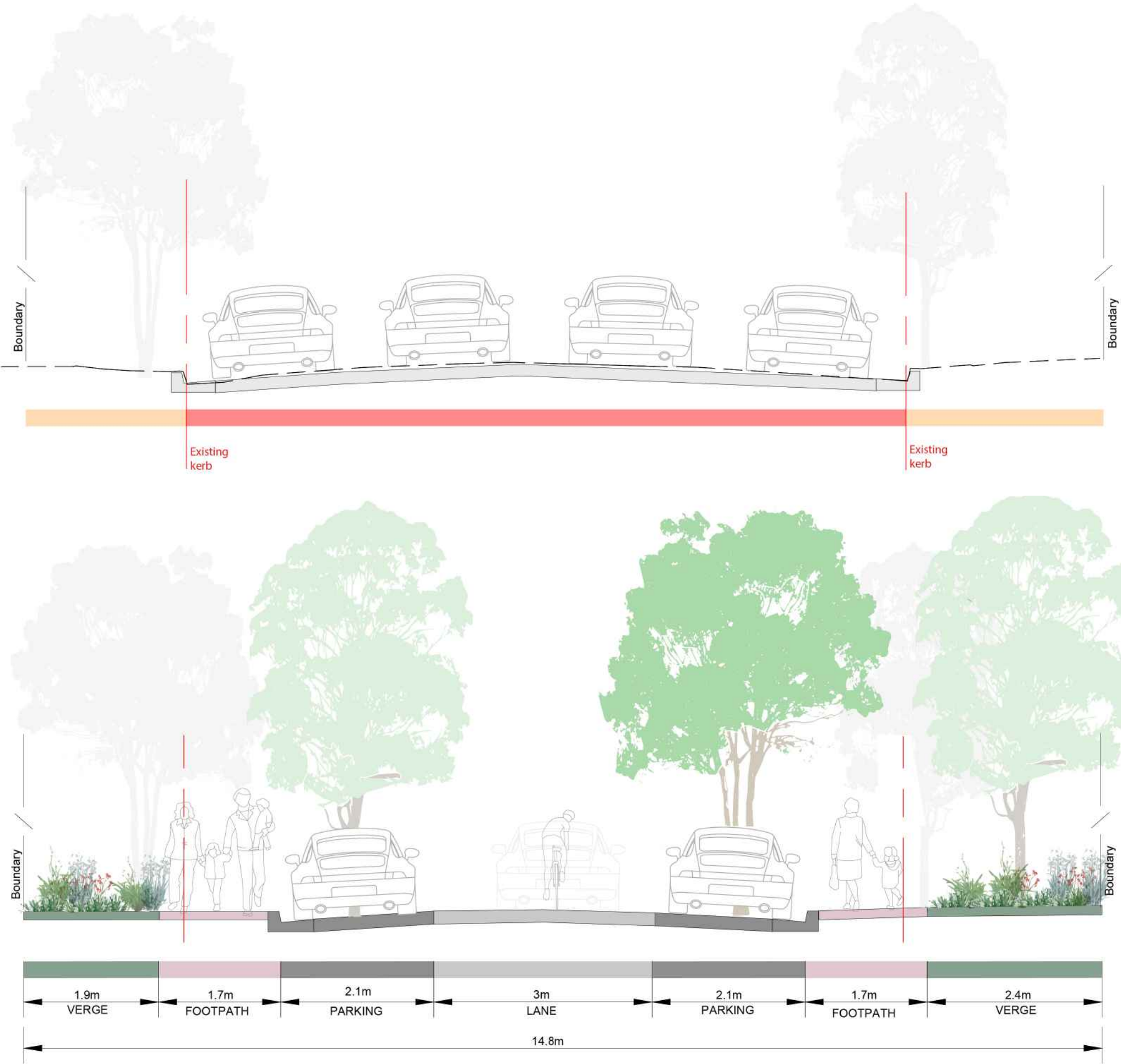
Key map



Existing

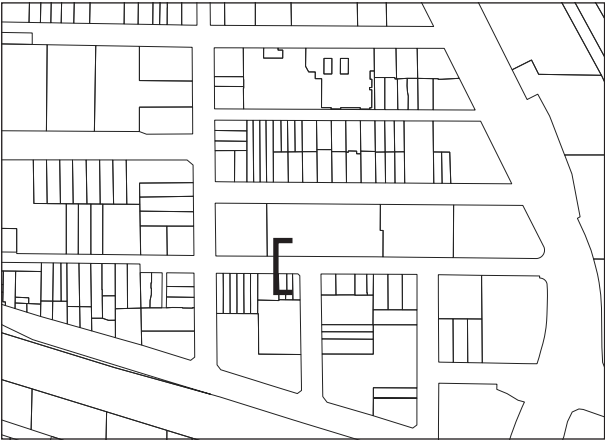


Precedent



3.3 Street typologies

3.3.24 Local Streets and Lanes - One-way



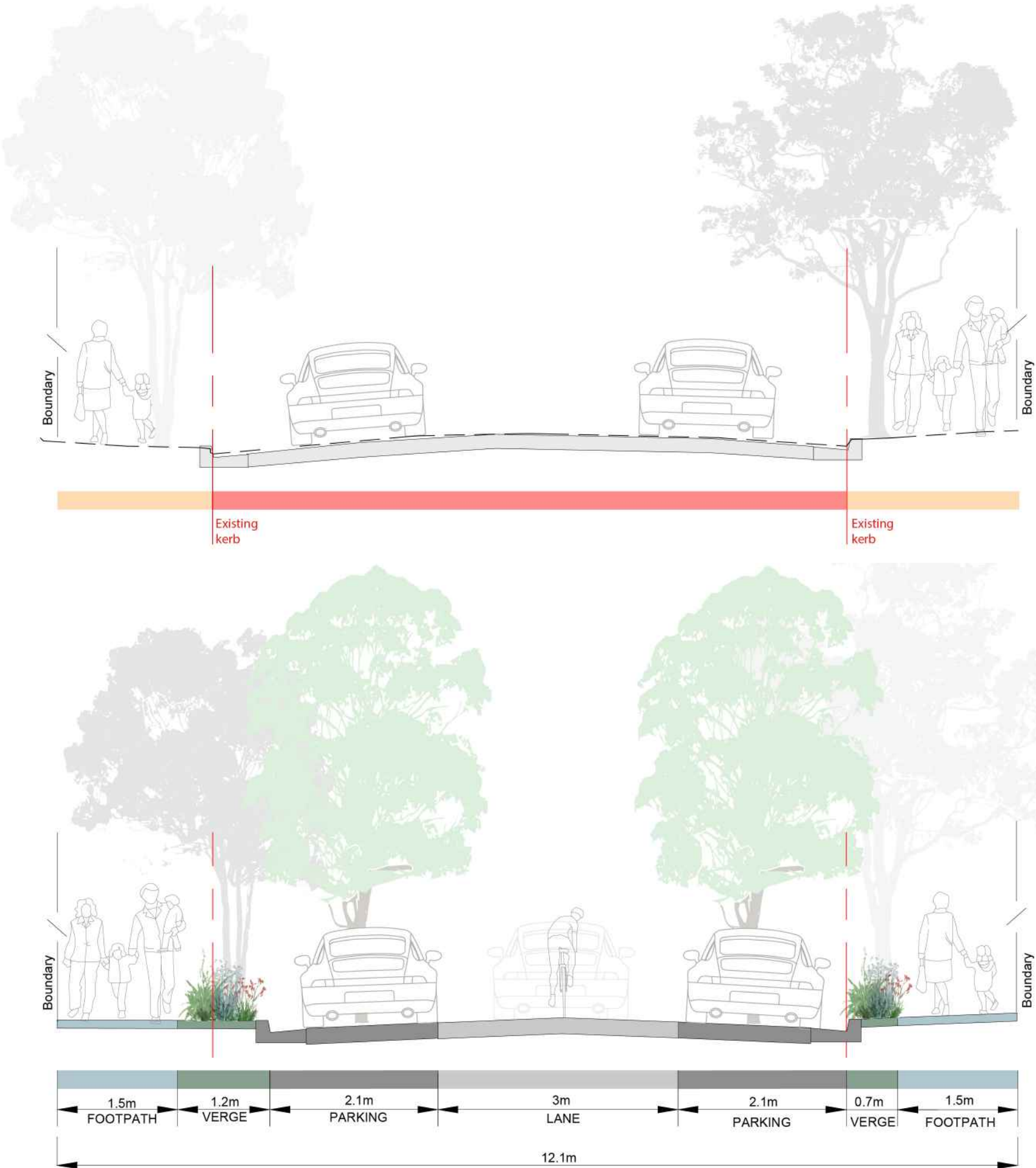
Key map



Existing



Precedent



Section 3 – Plan & Frameworks

3.3 Street typologies

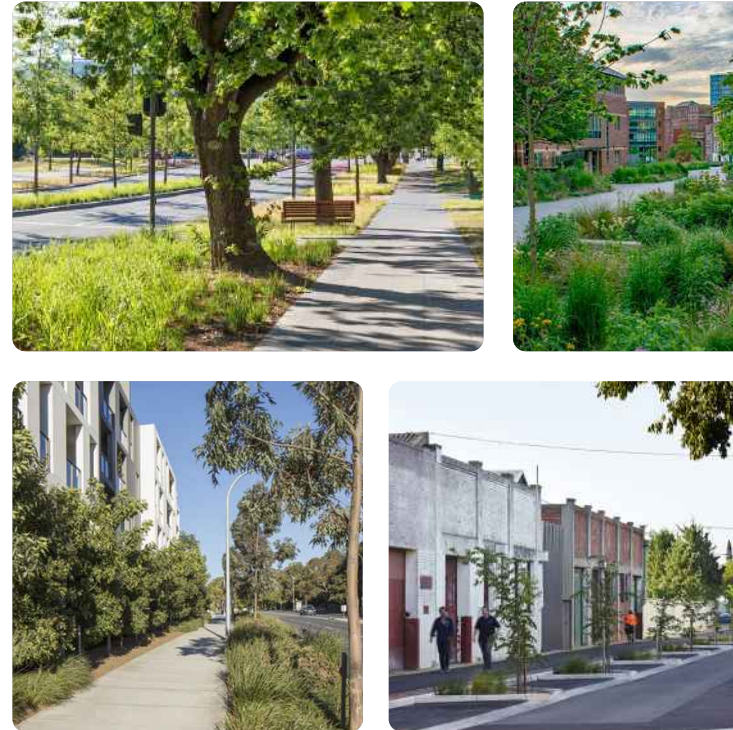
3.3.25 Local Streets and Lanes - Two-way

Key Moves:

- Changes to traffic patterns and upgrades to protect the closeknit, characterful and quiet network of local streets
- Careful balance between footpath width, existing constraints, and verge planting

Applicable streets:

- Lindus Street
- Greenway Street
- Church Street
- Foundry Street
- Wickham Street
- Bishopsgate Street (east of Charles Street)
- Holland Street
- Albert Street (east of Branch Street)



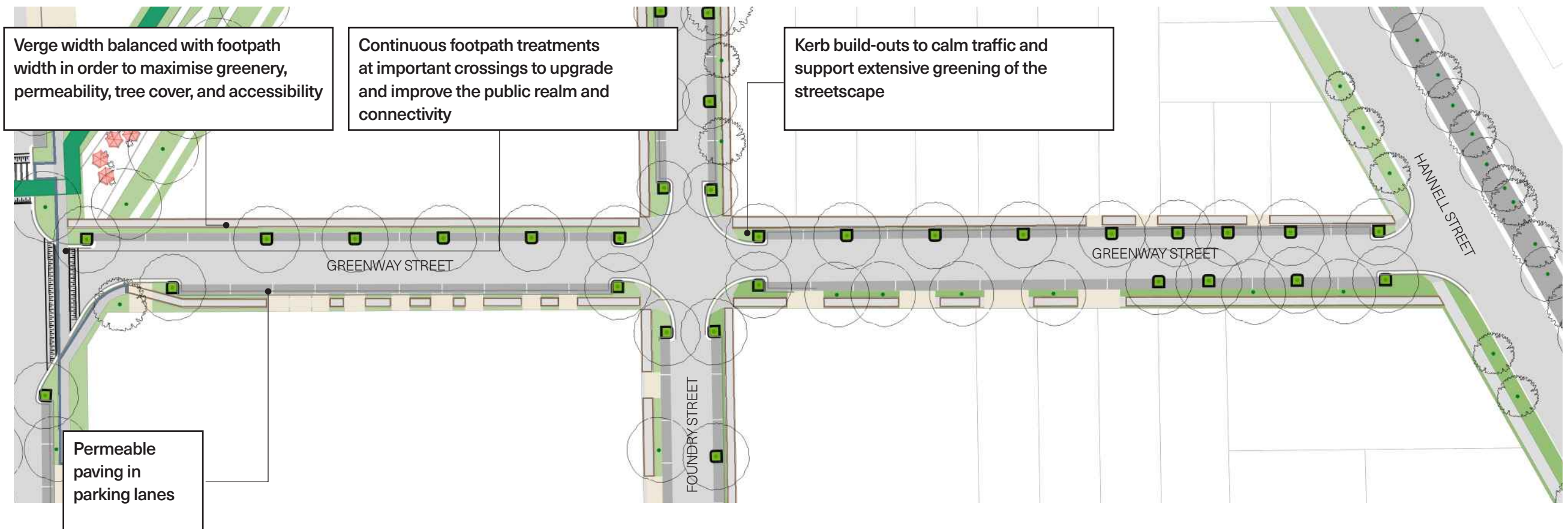
KEY

Surface treatment

- Carriageway
- Parking (Permeable Paving)
- Verge planting
- Raised intersection paving
- Union Street heritage brick paving
- Union Street standard paving
- Other paving typology with concrete infill and trim material as per paving typology
- Driveway
- Cycleway
- Shared zone
- Private property

Trees and associated elements

- Tree grate
- Passively irrigated tree pits
- Existing small tree
- Existing medium tree
- Proposed small tree
- Proposed medium tree



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3.4 Key Public Domain Spaces

This PDP provides for the concept design of a number of key public domain spaces. These concept designs are driven by the Principles we have developed to best respond to the Wickham Master Plan and our guiding Place Themes. While individually unique, these spaces have been considered as parts of whole, working to deliver for all of Wickham.

These spaces will:

- Provide amenity for the community, including play, outdoor recreation and exercise, places for rest, gathering, and socialising
- Contribute to the greening strategy, increasing canopy cover and reducing the UHI effect
- Provide opportunities for WSUD
- Provide opportunities to celebrate both Aboriginal living culture and European heritage
- Celebrate the stories that make Wickham special

3.4.1 Former Bullock Island Railway corridor

This space will be major new green corridor set within the historic former Bullock Island Railway corridor - an heritage-listed with an important part in the history of the suburb.

This PDP's concept design for the corridor provides for an active transport link between Wickham's north and southwest, as well as from east-west journeys via Church street. It imagines an improved and sympathetic setting for the heritage railway and an enhanced connection to Wickham Park.

3.4.2 The Wetland

Attached to the former railway corridor is a new urban wetland and outdoor exercise and recreation facilities. This space provides an ecological habitat that recalls the historic wetlands of the area and offers water detention, which can mitigate the effects of surface flooding. Saltmarsh planting is interwoven by winding paths, while broader boardwalks cross the wetland to the north and south, providing more direct connections through the former railway corridor.

Outside exercise equipment is arranged in a series of linear green spaces to the west of the wetlands, providing free and easy access to healthy activity in a rich and green setting.

3.4.3 Corridor North

Where the former railway corridor continues towards Branch Street on the east side of Railway Street, this space can continue the interpretation of the former railway

lines. At the south-west corner of this space, there is an opportunity for a plaza that would face the urban activation zones on the opposite corner, providing a vibrant and holistic urban environment.

3.4.4 Bishopsgate Green

This is a new, open green space at the intersection of Railway and Bishopsgate. The space provides for new trees and understorey planting, weaved together by a simple winding path, as well as a turfed area and seating - a place that is amenable to all the community and dog-friendly.

3.4.5 Throsby Green

This new space at the intersection of Railway Street and Throsby provides for play, rest, and shade integrating an ephemeral creek. Play equipment is interwoven with the creek bed itself, as well as in a dedicated area to the east of the site, away from the busyness of Railway.

3.4.6 Lindus Green

This new space at the intersection of Lindus Street and Railway Street, provides for play, rest, and shade. The space features a small, permanent stage for impromptu performances, formal and informal, while a mixed surface of brick paving and turf underlies movable public deckchairs and ample permanent seating.

3.4.7 Wickham Waterfront

Water is key to the life of Wickham and is a guiding Principle of this PDP. Accordingly, the waterfront promenade is an key public domain space. This PDP provides for the upgrade of the waterfront's materiality to continue the works undertaken by the HCCDC at Honeysuckle to the south.

3.4.8 Union Street Green

At the northen end of Union Street, existing conditions make an accessible footpath on the north-westernmost block unviable. This PDP therefore includes a concept design for a new green space full of trees and understorey planting that is dog-friendly and contributes to the cooling, green cover of Wickham's green spine.

3.4.9 Union Street community garden

Midway along Union Street is a new community garden. This garden features ample raised planters for vegetables and flowers, storage for communal equipment, bins for composting and mulch, bee hives, and seating.

The specifics of each space are detailed in the following pages.

Section 3 – Plan & Frameworks

3.4 Key Public Domain Spaces



This overview map represents the locations of the urban activation zones of the PDP. These concept designs are driven by the Principles we have developed to best respond to the Wickham Master Plan and our guiding Place Themes. While individually unique, these spaces have been considered as parts of whole, working to deliver for all of Wickham.

Upgrades to Wickham Park are part of a separate master plan to be undertaken by City of Newcastle. Upgrades to Tree of Knowledge Park are being undertaken by the Honeysuckle Development Corporate and fall outside the scope of the Wickham PDP.

Section 3 – Plan & Frameworks

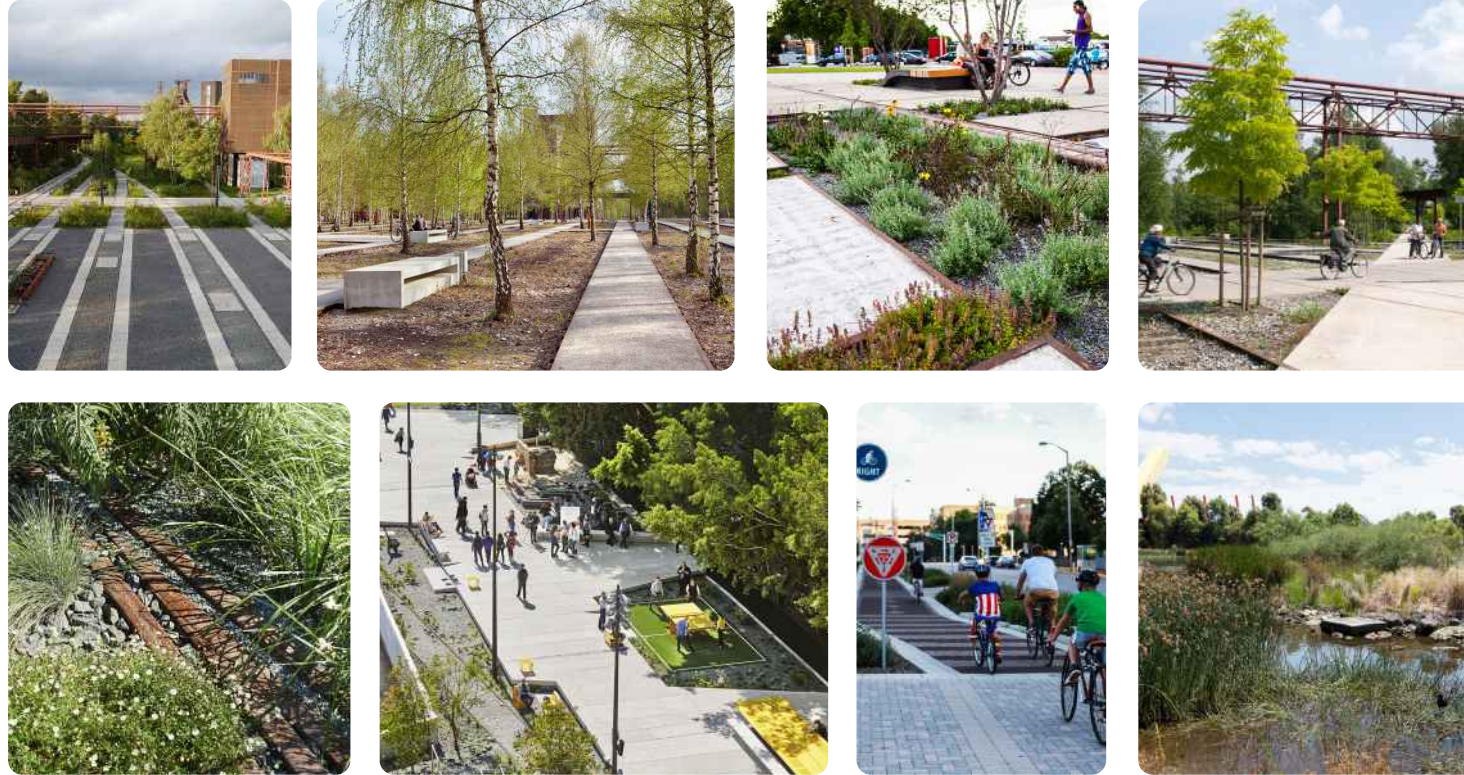
3.4 Key Public Domain Spaces

3.4.10 Bullock Island Railway Corridor

A major new green corridor, providing an active transport link between Wickham's north and southwest, as well as from east-west journeys via Church street, an improved and sympathetic setting for the heritage railway, and an enhanced connection to Wickham Park.

Key Moves:

- A new urban wetland at the junction of the corridor and Railway Street
- Equipment for outdoor recreation and exercise
- Celebration of railway heritage by revealing and enhancing the setting of the old rail tracks and using them to structure paths
- Separated bi-directional cycleway



KEY

Surface treatment

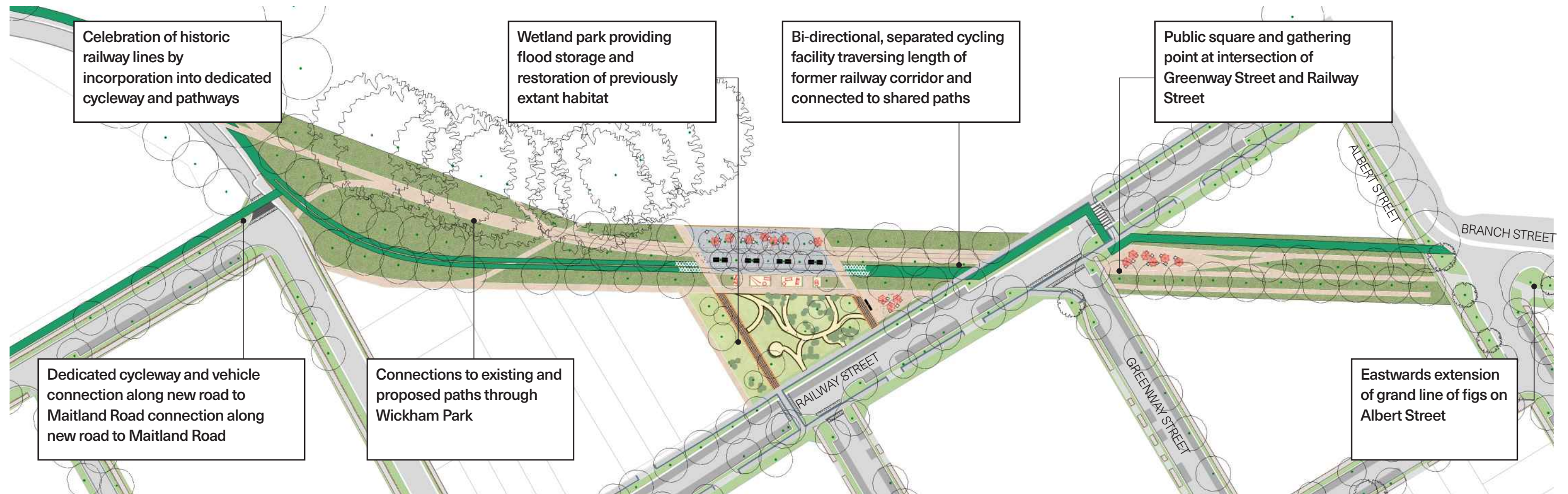
- Carriageway
- Parking
- Verge planting
- Raised intersection paving
- Union Street heritage brick paving
- Union Street standard paving
- Other paving typology with concrete infill and trim material as per paving typology
- Driveway
- Deconstructed granite path
- Cycleway
- Shared zone
- Private property

Street furniture

- Interpretation Node

Trees and associated elements

- Tree grate
- Passively irrigated tree pits
- Existing medium tree
- Existing large tree
- Proposed medium tree
- Proposed large tree



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Section 3 – Plan & Frameworks

3.4 Key Public Domain Spaces

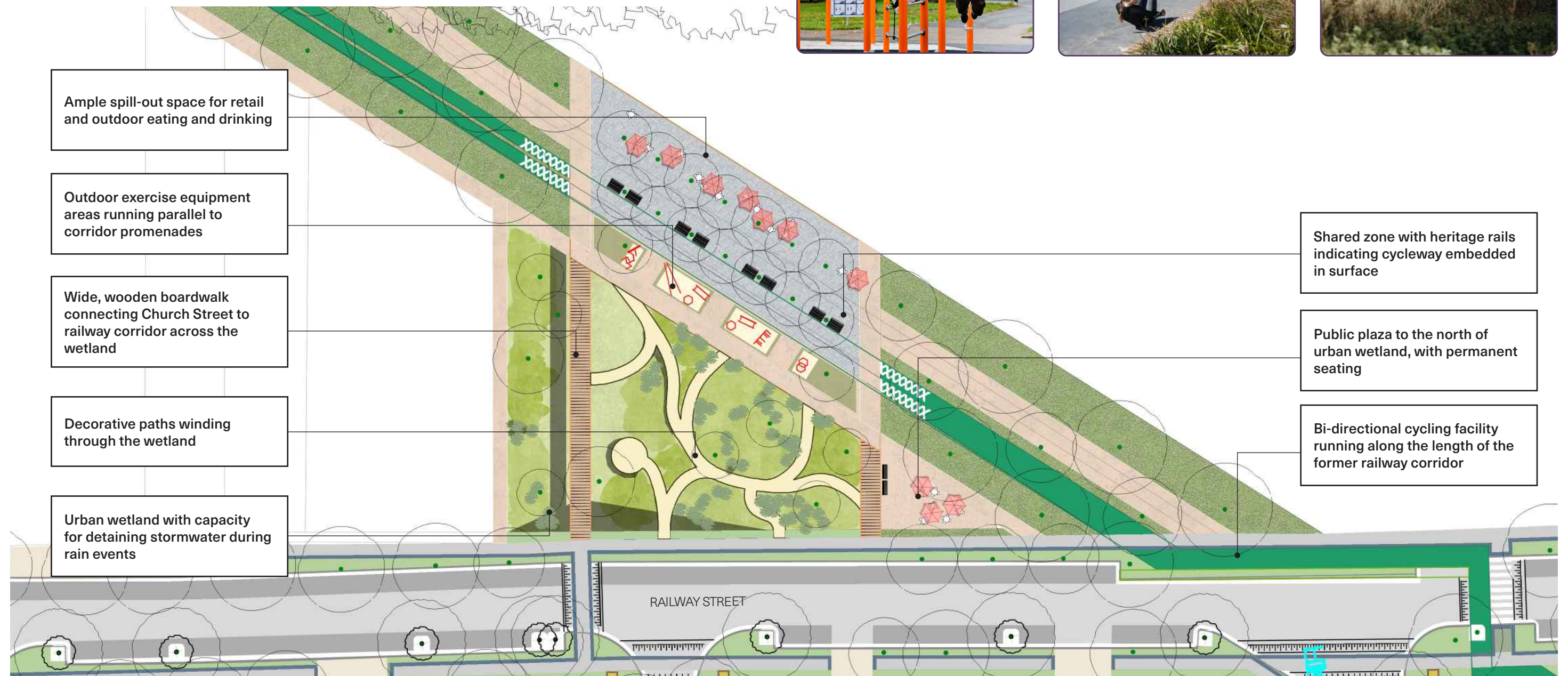
3.4.11 Bullock Island Railway Corridor - urban wetland

Overview

A new urban wetland park and promenade that acts as the interface between the upgraded and enhanced former Bullock Island Railway Corridor, Wickham Park, new developments on the west, and the public realm of Railway Street. This space will provide outdoor exercise equipment in a well planted and carefully designed setting that strengthens biodiversity and detains water.

Key moves

- Urban wetland criss-crossed by paths and boardwalks
- Outdoor exercise equipment
- Large shared zone between urban wetland and interface of develop on west
- Bi-directional separated cycle path set in historic railway tracks
- Extensive tree planting that connects and expands existing canopy of Wickham Park



Ample spill-out space for retail and outdoor eating and drinking

Outdoor exercise equipment areas running parallel to corridor promenades

Wide, wooden boardwalk connecting Church Street to railway corridor across the wetland

Decorative paths winding through the wetland

Urban wetland with capacity for detaining stormwater during rain events

Shared zone with heritage rails indicating cycleway embedded in surface

Public plaza to the north of urban wetland, with permanent seating

Bi-directional cycling facility running along the length of the former railway corridor

RAILWAY STREET

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Artist's impression of Railway Street once PDP initiatives have been fully implemented.

Section 3 – Plan & Frameworks

3.4 Key Public Domain Spaces

3.4.12 52 Throsby Street (Corner of Railway and Throsby Street)

Overview

A new public space at the intersection of Throsby Street and Railway Street, with provision for play, rest, and shade integrating an ephemeral creek

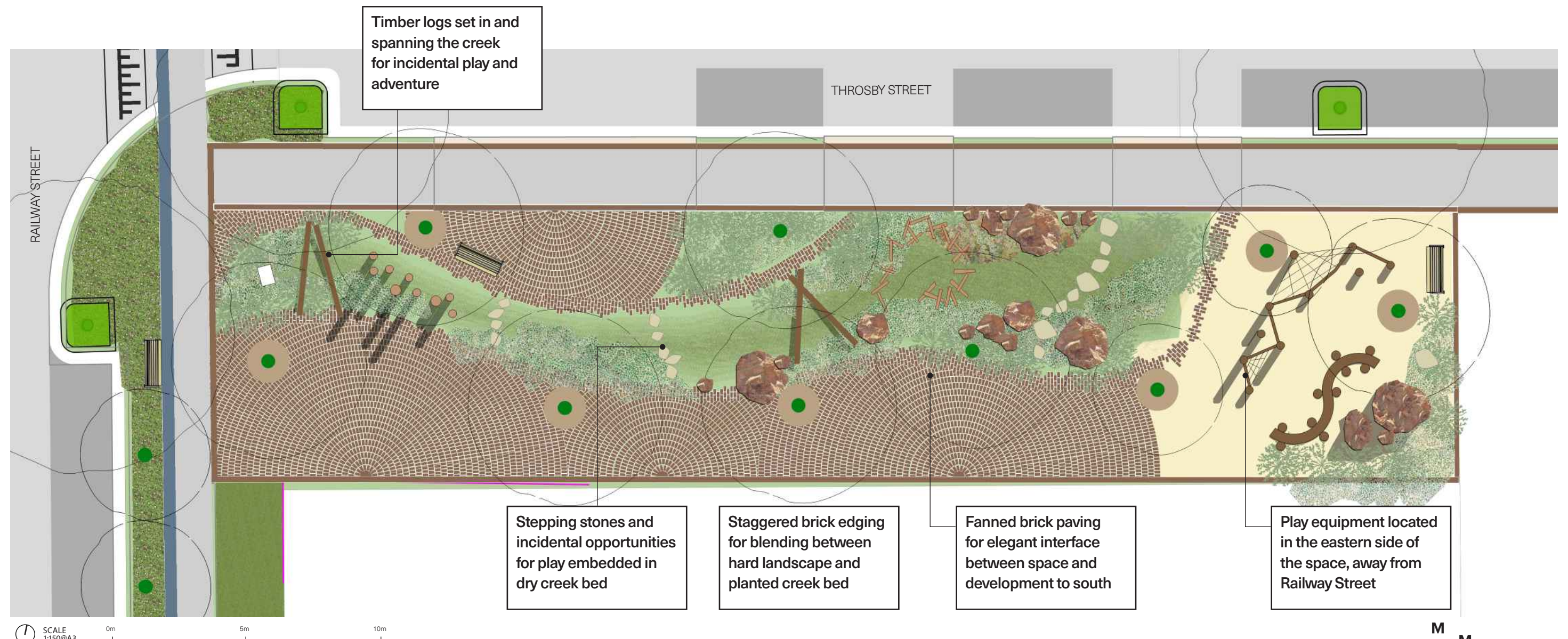
Masterplan vision for this space:

- Category: Public domain activation
- Approx area: 440m²

Key moves

- Ephemeral creek bed integrated into water sensitive urban design elements

- Incidental, natural play in the creek bed as well as in a dedicated space in the east, away from the main road
- Stepping stones and boulders
- Staggered brick trim facing the creek, providing coherence and continuity of public domain look and feel
- Brick paving along the interface with future development for ease of integration



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DATE 02.11.2023

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Section 3 – Plan & Frameworks

3.4 Key Public Domain Spaces

3.4.13 “Wickham Green”

29 Bishopsgate Street (Corner of Railway and Lindus Street)

Overview

A new public space at the intersection of Lindus Street and Railway Street, with provision for play, rest, and shade integrating a small stage for performances

Masterplan vision for this space:

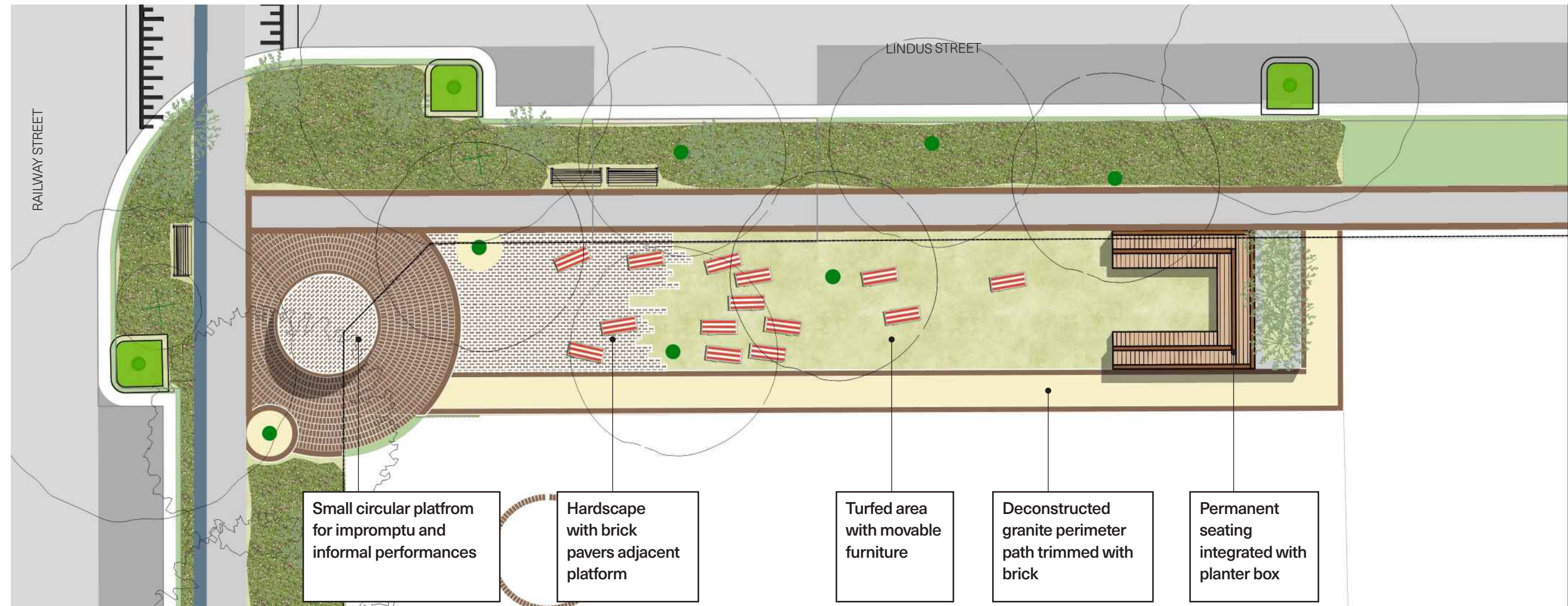
- Approx area: 210m2

Key moves

- Paved, rased podium on corner, signposting the area and maximising visibility across public realm
- Moveable and robust outdoor furniture such as

deckchairs that allow the the space to work in a variety to configurations

- Ample integrated seating on the eastern side for a quieter and calm space of rest, relaxation and dwelling
- Brick trim for coherence and continuity with wider PDP spaces
- Shaded turfed area for play, recreation, and gathering
- Landscaping integrated with verge planting for maximum biodiversity and health of the Wickham-wide plant community



Small circular platform for impromptu and informal performances

Hardscape with brick pavers adjacent platform

Turfed area with movable furniture

Deconstructed granite perimeter path trimmed with brick

Permanent seating integrated with planter box

Section 3 – Plan & Frameworks

3.4 Key Public Domain Spaces

3.4.14 “Project 11”

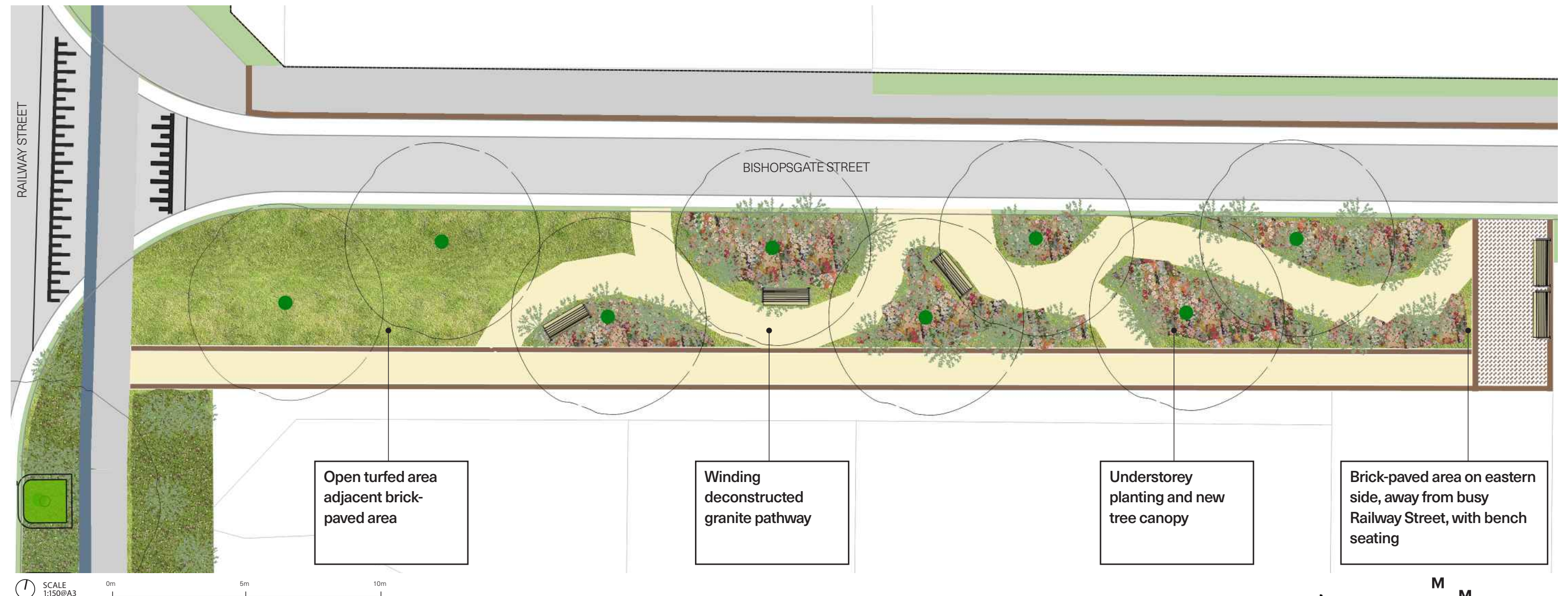
Bishopsgate Street (Corner of Railway and Bishopsgate Street)

Overview

A new open public space at the intersection of Bishopsgate Street and Railway Street, a dog friendly turfed area and a winding path through lush understorey planting beneath trees.

Key moves

- Open green space for the community
- Shaded, turfed area on the western edge for dogs
- Permanent seating on the eastern side for a quieter and calm space of rest away from the busier streetscape of Railway Street
- Winding deconstructed granite paths
- Brick trim achieving palette continuity with wider PDP spaces
- Understorey planting across space integrated verge planting for maximum biodiversity and health of the Wickham-wide plant community



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Section 3 – Plan & Frameworks

3.4 Key Public Domain Spaces

3.4.15 46 Union Street

Overview

A new community garden on Union Street, with planters for vegetables and flowers, communal equipment and bee hives, and seating

- Approx area: 145m²

Key moves

- Raised, irrigated planters for community veggie and herb patches
- A shed for tool and equipment storage
- Native trees for shade and fruit

- Communal beehives
- Open paved area for rest and relaxation leading off the main footpath
- Rich and layered planting along the verge providing habitat for pollinators
- Deconstructed sandstone paths trimmed with brick
- Compost and mulch bins
- Benches and tables for gathering, teaching, eating, and relaxing



Section 3 – Plan & Frameworks

3.5 Green & Blue Infrastructure

3.4.16 A Green Streets approach

This PDP adopts a “Green Streets” approach. This means that it considers Wickham’s streets as one of the most important elements in the public realm of both Wickham and the wider urban context of Newcastle.

Adopting a Green Streets approach in Wickham means prioritising the public realm experience of a street over one purely focused on traffic movement and infrastructure management. It emphasises the contribution good street design can make to the broader livability and sustainability of Newcastle, including providing pedestrian amenity, the activating of commercial and retail spaces, and locking in long-term climate change resilience.

A Green Street includes trees, low plantings and rain gardens where space allows, and prioritises pedestrian and cyclist movement over vehicles where practical and sensible. A Green Street can be considered one part of a larger ecosystem and a broader civic tree canopy.

Benefits

The benefits of a Green Streets approach include:

- The promotion of walking and cycling, which can encourage healthier habits and lifestyles
- Improved safety for pedestrians and children, through the reduction of traffic-related risk)
- The provision of shade for amenity, respite and human comfort
- Integrated storm water management (connecting into other Green Infrastructure approaches)
- Improved urban biodiversity (with appropriate species selections)
- Reinforcement and definition of the unique urban character of the city
- Assistance with navigation and wayfinding

Growing Environments

The success of a Green Streets approach lies primarily in the planning, design and implementation and maintenance of a healthy growing environment for trees and plants. This must be done within the often congested and contested spaces between property title boundaries and with the specific needs of different species of trees. Accordingly, this PDP has been the outcome of the careful observation of Wickham’s existing constraints, cadastral boundaries, and public domain assets.

The success and longevity of Green Streets in Wickham will depend on healthy soils, an adequate growing environment and oxygen supply, appropriate species selection, an understanding and optimisation of microclimate, and the provision of an adequate water supply. The healthy

establishment and maturation of trees also rely directly on the provision of adequate soil volumes – this fact has directly dictated the proposed locations of trees across the PDP.

Passive Irrigation

A Green Streets approach also champions passive drainage systems, which involve less-constructed in-ground works, at all stages and locations where possible. This part of Green Streets marks a distinction between ‘hard’ and ‘soft’ engineering approaches to storm water management. A ‘soft’ approach has potential capital cost savings with the avoidance of underground pits, pipes and other drainage infrastructure. It also provides ‘add on’ benefits over and above the core storm water management function including increased related asset lifespan. For example, adjacent pavements are less likely to be lifted by tree roots as they won’t rise to the surface looking for water. Wickham’s low-lying elevation can make this a challenge – but where possible, and appropriate, passive irrigation has been proposed.

Maintenance

As with all infrastructure systems, Green Streets require a level of maintenance (such as occasional removal of sediment build up in raingardens). However, if a system is well planned and designed, the maintenance level can be similar to a more typical grey infrastructure approach.

Planning, Placement and Character

Street trees are one of the most important organising elements of a streetscape. They help create defining zones of movement, assist with traffic calming, and provide shade for footpaths. They also tell stories and define the character of a place. In Wickham, this especially true of the Tree of Knowledge, a landmark tree – but the many existing trees across Wickham are part of its longer history as well. For all of these reasons, a Green Streets approach has therefore been made an foundational aspect of this PDP.

3.4.17 Water Sensitive Urban Design

Understanding Wickham’s relationship to water is paramount to the success of this PDP and why it is a guiding Principle of the report. In doing so, the PDP builds upon water management policy work done by City of Newcastle to date, including:

- *Newcastle 2020 Carbon and Water Management Action Plan* (2011)
- *Strategic Position for the Management of Low Lying Areas of Newcastle* (2017)
- *Newcastle City-wide Floodplain Risk Management Study and Plan* (2012).

The PDP also reflects the ambition of the Newcastle Climate Action Plan 2021-2025 to “develop and implement strategies and initiatives to tackle climate change head on”.

Accordingly, this PDP has considered water throughout its development, and provides for best practices in water conservation, the protection of water quality and ecosystem health, and integrated water resource management based across Wickham.

Wickham lies entirely within a low-lying, coastal area. While works are underway to protect the area from coastal inundation, stormwater drainage will be an ongoing challenge. It is of great strategic importance therefore that as much stormwater water as possible is intercepted, detained, and re-used before entering the stormwater system.

This PDP seeks to maximise wastewater and stormwater reuse through water-sensitive urban design (WSUD). WSUD strategies adopted as part of this PDP include:

- rain gardens and urban wetlands
- passive irrigation of trees
- careful management of water flow following upgrades to the public realm, e.g. continuous footpath treatments

Complementing this WSUD strategy is the PDP’s ambition to maximise tree and plant cover, which has the following water management benefits:

- the minimisation of impermeable surfaces across Wickham (decreasing surface water run-off)
- cooler ambient temperatures through evapotranspiration, shading, and the creation of microclimates.
- self-irrigation of trees, reducing total potable water use and increasing the health of the trees themselves

These strategies have been considered and applied to each street of Wickham per its own particular requirements and characteristics while contributing to a holistic scheme that results in a more robust and resilient system.

3.5 Green & Blue Infrastructure

3.5.1 Green & Blue Infrastructure Plan



KEY

Green Infrastructure Initiatives

- Proposed and Existing Trees
- Proposed Open Spaces
- Existing Open Spaces

Blue Infrastructure Initiatives

- Wetland
- Passively integrated tree pits

Transport

- Bus stops



Artist's impression of Union Street once PDP initiatives have been fully implemented.

Section 4.0

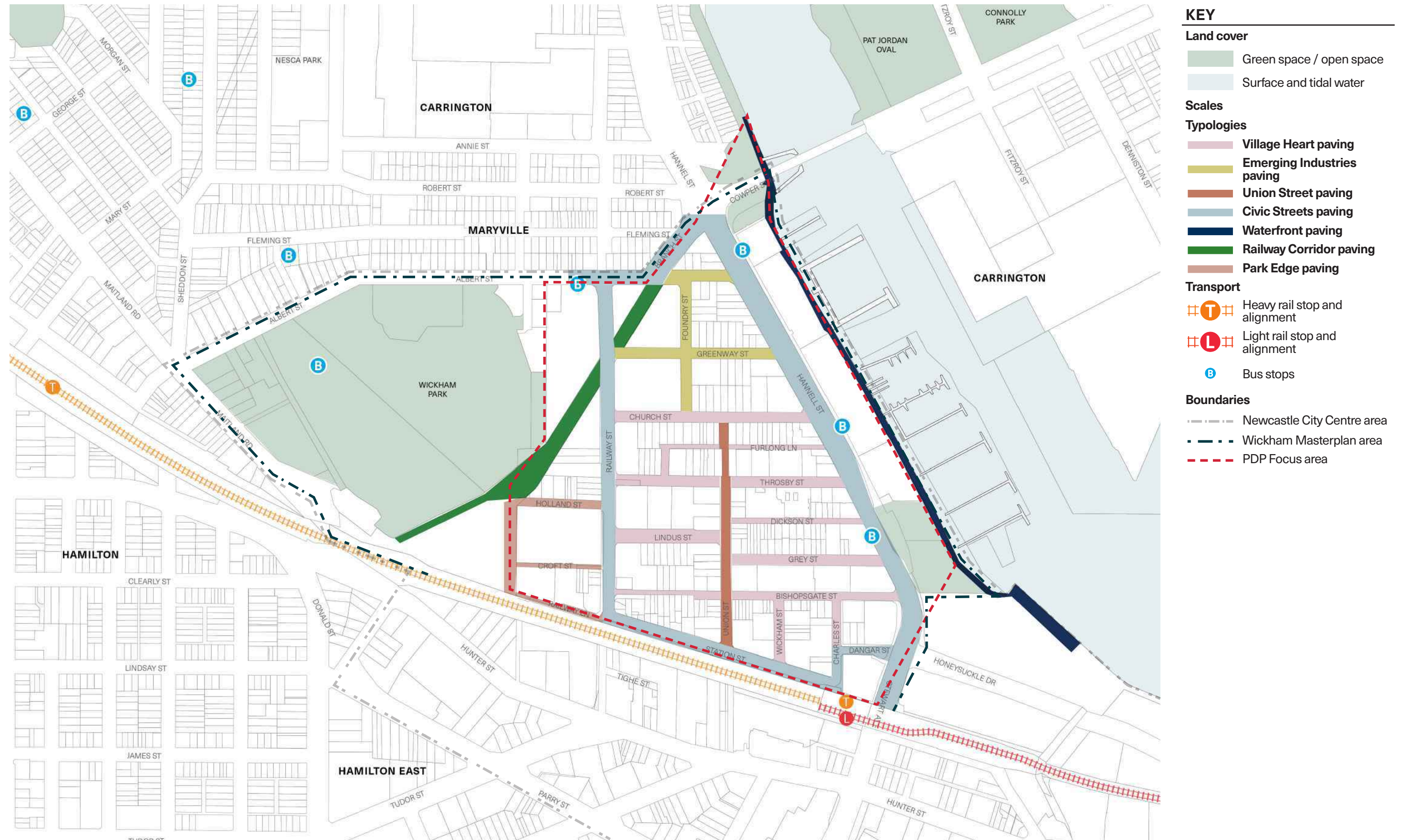
Design Elements



Section 4 – Design Elements

4.1 Materiality

4.1.1 Paving Typologies Plan



Section 4 – Design Elements

4.1 Materiality

4.1.2 Paving Typology Overview

The proposed paving materials selected within the Wickham Public Domain Plan have been informed by the various features and character areas of Wickham as proposed in the Wickham Master Plan. A paving strategy is proposed which also reinforces the hierarchy of the streets within the city and the selections have had reference to the general city centre technical manual parameters. This approach offers a refined approach to bring consistency whilst allowing for some change in materials to suit the local character.

Design Approach

- Materiality to reflect character zone of the street.
- Colour and materiality to complement the palette of Wickham
- Utilise local materials where possible.

Delivery Approach

- Ensure material and design elements meet with Council standards.

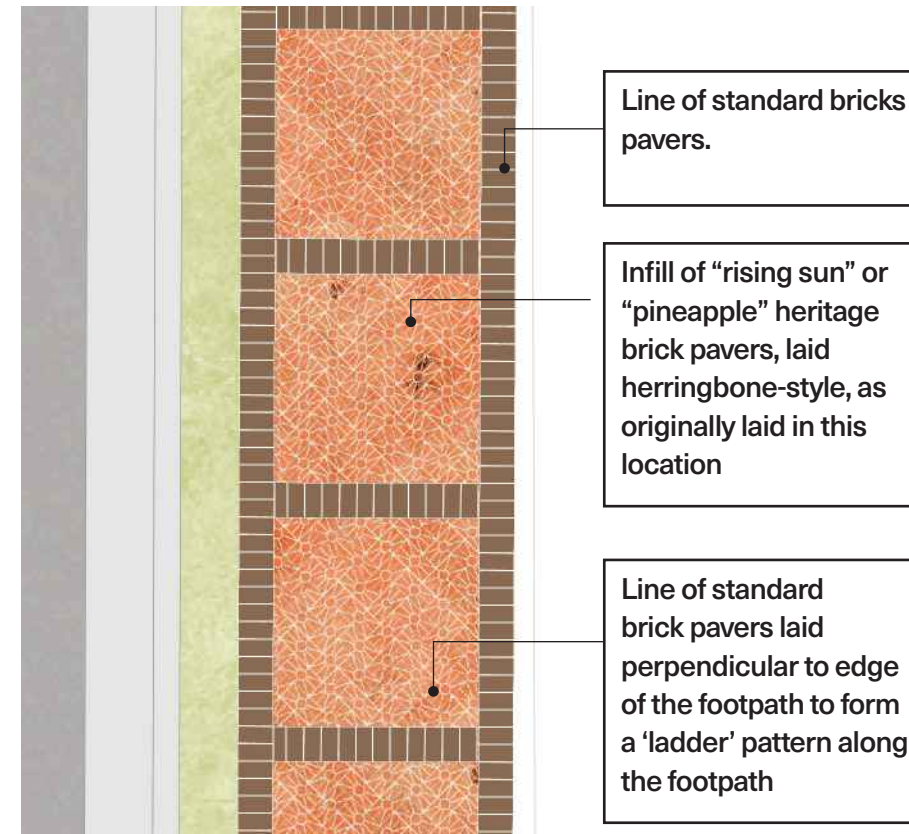
Historic bricks

An important part of Wickham's unique and historic character is the presence at certain locations of heritage brick pavers, either using the "pineapple" or "rising sun" pattern. These bricks are mostly concentrated on Union Street, but can be found in other locations across the suburb.

Union Street therefore has two varieties of paving scheme: one variety to make use of and honour the existing heritage brick pavers, especially around the intersections of Lindus Street and Grey Street, and one more standard scheme for the rest of its length. Where the bricks are found elsewhere, the heritage bricks are integrated into a variation of that paving typology.

These heritage schemes retain the heritage brick pavers in the location in which they are currently found, while enhancing their setting with a new standard brick trim.

4.1.3 Union Street Heritage Pavers



DESIGN FEATURES:

- The heritage scheme retains the heritage brick pavers in the location in which they are currently found, while enhancing their setting with a new standard brick trim, set in a 'ladder' pattern
- The ladder pattern follows the sizing and segmentation for the other paving treatments across Wickham, providing a sense of continuity and consistency throughout the suburb

COMPONENTS:

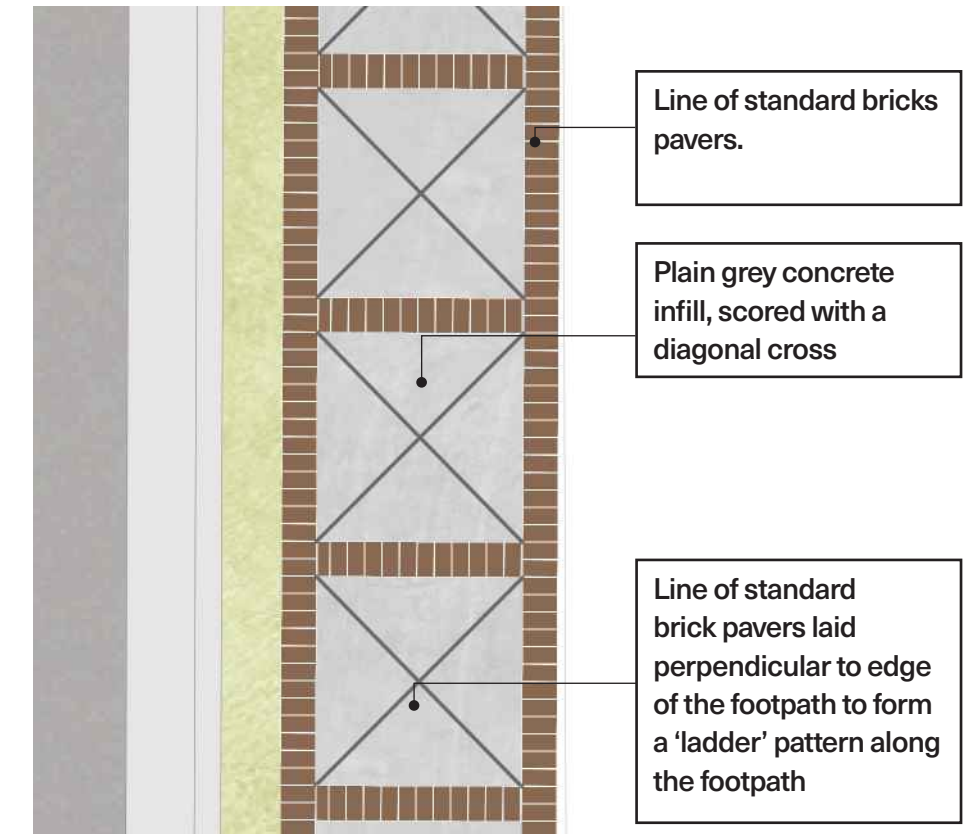
Edges



Footpath



4.1.4 Union Street Non Heritage Pavers



DESIGN FEATURES:

- The non-heritage scheme makes use of the standard brick pavers set in a 'ladder' pattern, infilled with plain grey concrete
- The concrete infill panels could be scored in a diagonal cross, recalling the pattern of the 'rising sun' style heritage pavers that can be found along Union Street
- The ladder pattern follows the sizing and segmentation of other paving treatments across Wickham, providing a sense of continuity and consistency throughout the suburb.

COMPONENTS:

Edges



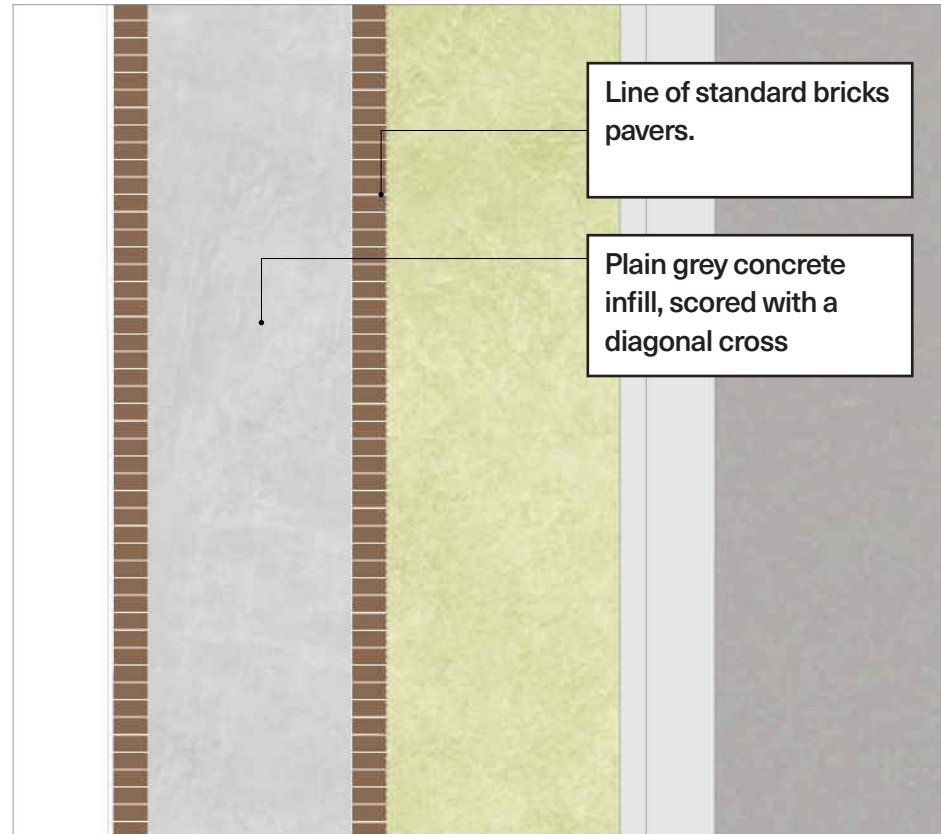
Footpath



Section 4 – Design Elements

4.1 Materiality

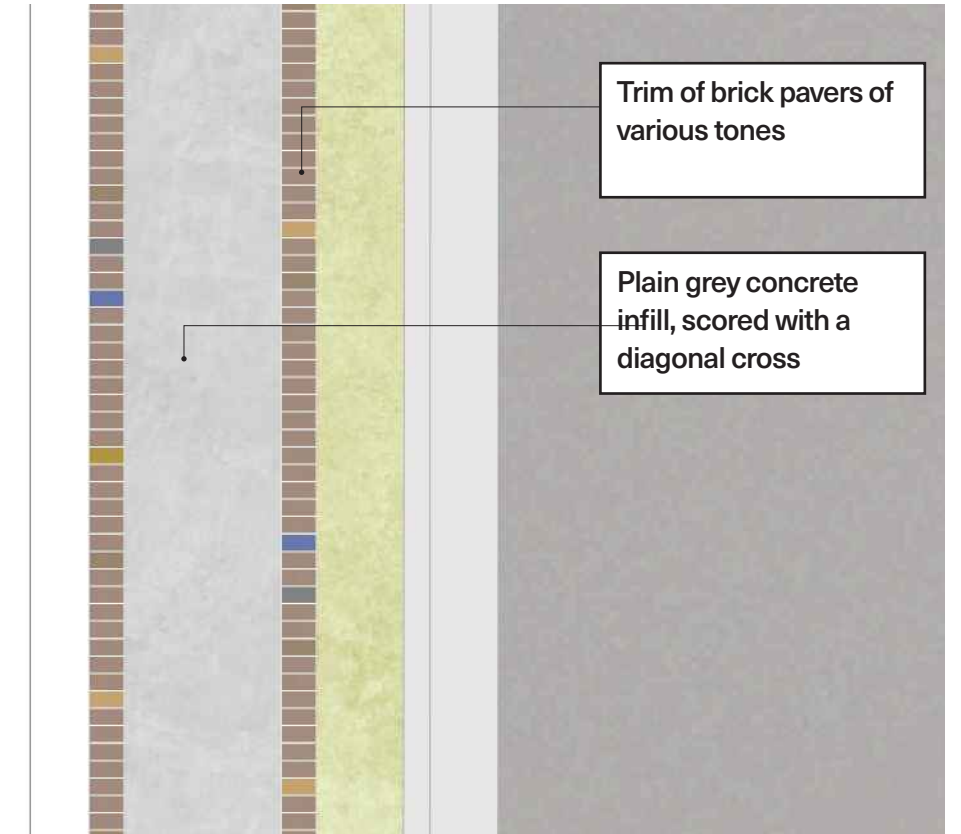
4.1.5 Village and Park Edge Street Paving Typology



4.1.6 Civic Streets Paving Typology



4.1.7 Emerging Industry Street Paving Typology



Standard brick pavers with light brick paver trim (Bolton Street)



Standard brick pavers and concrete (Bishopsgate Street, Wickham)



Standard brick pavers forming a panel with herringbone-laid standard bricks (Grey Street, Wickham)

DESIGN FEATURES:

- This scheme is a simple combination of standard brick paver trim infilled with plain grey concrete
- The standard brick paver trim provides a sense of continuity and consistency within the Village Heart of the suburb. This brick trim is consistent with the Union Street trim.

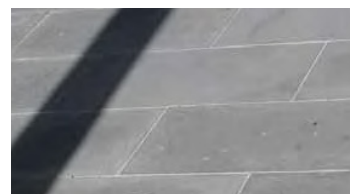
COMPONENTS:

Edges

 Brick pavers

Footpath

 Concrete infill



Pavement type 3 (PT3) stone and concrete



Pavement type 3 (PT3) stone and concrete with just one line of bluestone pavers



Pavement type 3 (PT3) stone and concrete

DESIGN FEATURES:

- Makes use of the City of Newcastle's existing pavement type 3 (PT3), a combination of 450x450mm bluestone pavers and concrete infill
- Hard-wearing and ties Wickham's major streets closely into the wider city's civic thoroughfare
- Scheme is adaptable to street and footpath widths

COMPONENTS:

Edges

 Bluestone pavers

Footpath

 Concrete infill



Varied brick tones

DESIGN FEATURES:

- Celebrates the creative and industrial spirit of the streets of Wickham north of Church Street
- Scheme makes use of the brick paver trim set infilled with plain grey concrete
- Brick pavers are varied in tone and include 'specials' i.e occasional bricks that feature brighter or patterned clays, recycled aggregate or artistic glazing. There is also the opportunity to make use of reclaimed bricks.

COMPONENTS:

Edges

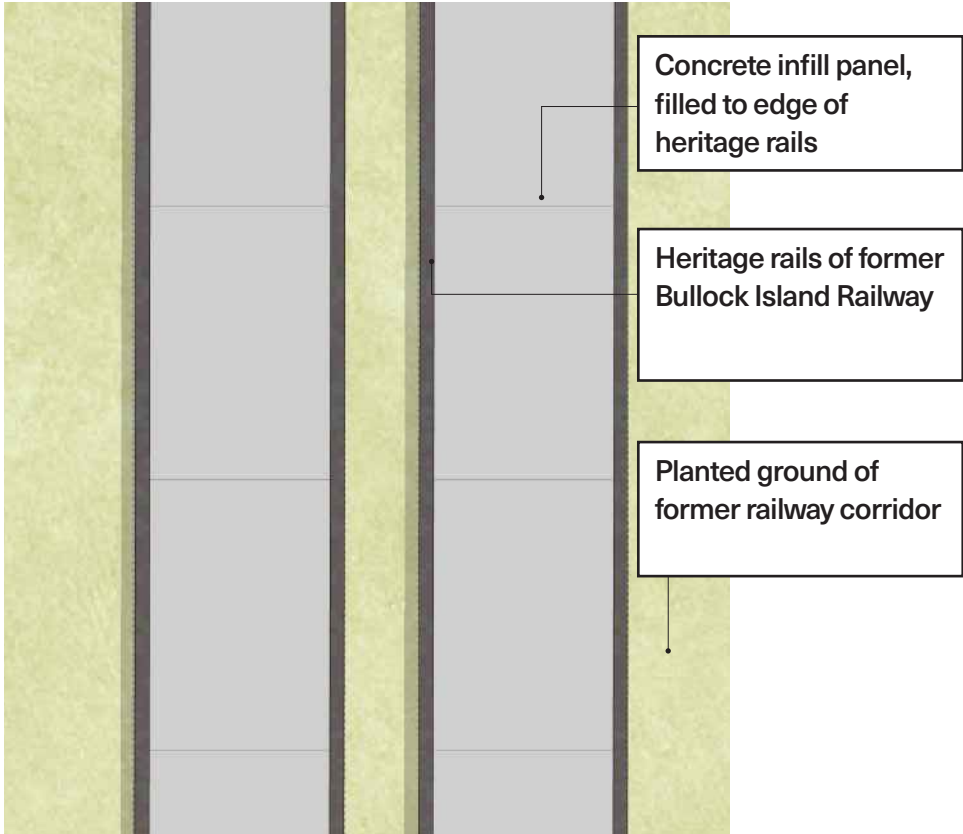
 Brick pavers in various tones

Footpath

 Concrete infill

4.1 Materiality

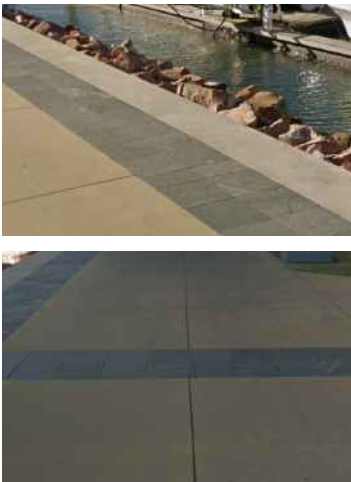
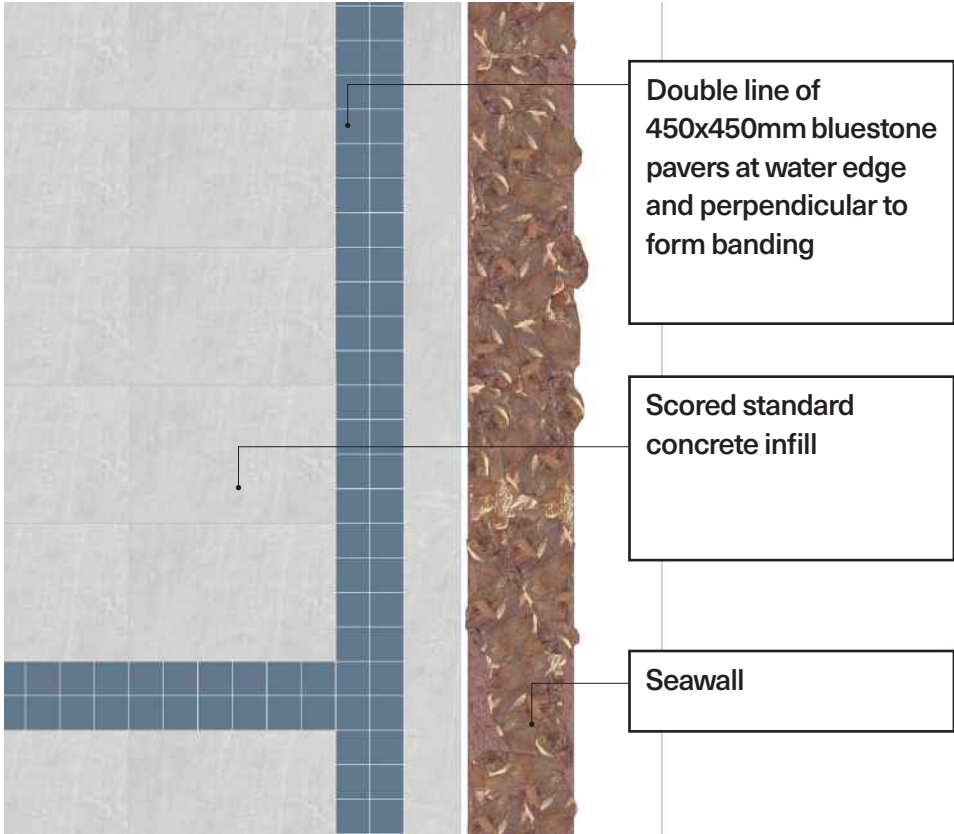
4.1.8 Railway Corridor Paving Typology



- DESIGN FEATURES:**
- Wide central promenade that enhances and celebrates the former Bullock Island Railway corridor's industrial heritage
 - Historic rails set flush into promenade surface

- COMPONENTS:**
- Edges**
- Heritage rail
- Footpath**
- Concrete infill

4.1.9 Waterfront



- DESIGN FEATURES:**
- This paving typology is the existing condition along the waterfront; this PDP proposes the extension of the typology to new spaces in the waterfront character area for maximum consistency
 - A broad, civic palette, this typology makes use of bluestone pavers laid two-deep for a bold, large-scale pattern
 - Standard concrete panels infill this banding

- COMPONENTS:**
- Edges**
- Bluestone pavers
- Footpath**
- Concrete infill

Section 4 – Design Elements

4.1 Materiality

4.1.10 Materiality strategy guidelines overview

The following are guidelines for the establishment of a coherent and structured materiality strategy.

4.1.11 Hierarchy

The PDP paving typologies should be considered according to the following hierarchy. A higher place in the hierarchy means that this typology takes precedence at any intersection across Wickham. Note that Emerging Industries and Village Heart do not interface with each other and are considered equal third place. Note that Park Edge shares the same detail as Village Heart.

1. Civic Streets
2. Union Street
3. Emerging Industries, Village Heart/Park Edge
4. Railway Corridor

4.1.12 Interfaces

T-junction interfaces.

Where typologies interface in a t-junction, the second-order typology must terminate at the outer trim of the first-order typology.

If there is no trim, such as might be the case for the Civic Streets typology, which in some places has only bluestone trim on the carriageway-side, the second-order typology should terminate with a perpendicular line of its own trim material.

At corners with kerb ramps

Where typologies interface at a narrow and contested corner with kerb ramps, the paving of both typologies should terminate at a natural seam, for example, the corner of a property boundary. This seam should be demarcated by a line of brick trim. Concrete infill to the kerbs and kerb ramps should then interface the two typologies beyond this seam.

At raised platforms

At raised intersections, such as at the intersection of Throsby Street with Union Street, the line of trim opposite the carriageway on the approaching road should continue across, forming a square. This square should be infilled by the same material of the whole raised intersection.

4.1.13 Crossings

At continuous footpath treatments, the materiality of the footpath should continue across the carriageway at grade with the footpath.

At wombat crossings, the typology must terminate at the kerb, with the typology's trim following the perimeter of the kerb of any kerb ramp build out.

4.1.14 Driveways

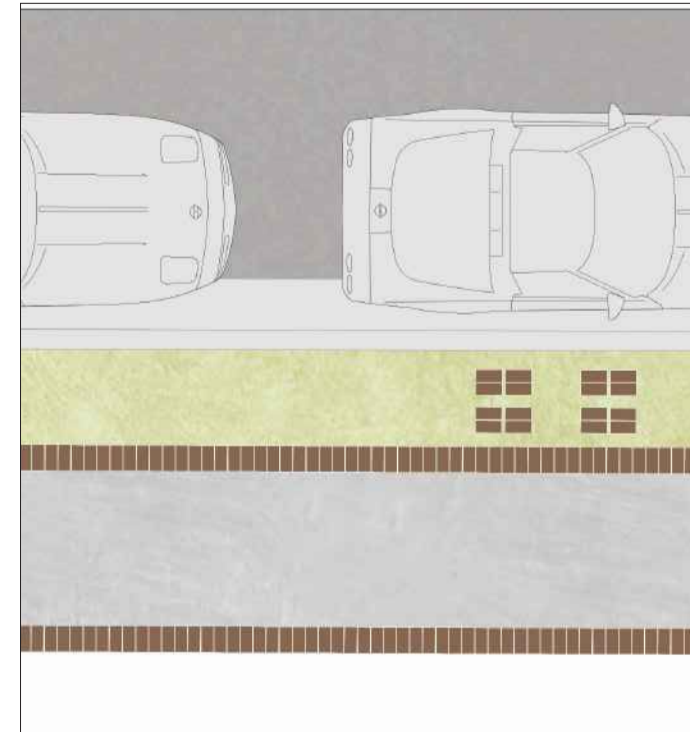
Where a typology meets a driveway, the trim should terminate the footpath neatly and perpendicular to the driveway edge. In the case of typologies with only trim on the carriageway side, such as such stretches of Civic Streets paving, the trim should follow the driveway edge and terminate perpendicular to the property edge or verge.

4.1.15 Step stones

Where a verge lies between the footpath and a parking lane, access through the verge planting should be provided through stepping stones laid in the same material as the trim of that typology.

These stepping stones will allow crossing of the verge while also balancing the needs of the verge planting. They should be laid at rational intervals relative to the verge width to allow easy access to the footpath for passengers exiting a car.

These step stones would be set within a gravel bed to minimise vegetation growth over the steps and maintain visibility and accessibility.



4.1.16 Typical access to parking through verge

4.2 Planting

4.2.1 Street Tree Plan



KEY

Proposed Planting

- As *Acmena smithii* (Lilly Pilly)
- Ah *Araucaria heterophylla* (Norfolk Island Pine)
- Ba *Brachychiton acerifolius* (Illawarra Flame Tree)
- Bp *Brachychiton populneus* (Kurrajong)
- Bc *Buckinghamia celsissima* Ivory Curl Tree
- Cm *Corymbia maculata* (Spotted Gum)
- Ca *Cupaniopsis anacardioides* (Tuckeroo)
- Er *Elaeocarpus reticulatus* (Blueberry Ash)
- Fm *Ficus microcarpa hillii* (Hills Weeping Fig)
- Fa *Flindersia australis* (Crow's Ash)
- Hp *Harpullia pendula* (Tulipwood)
- Ht *Hibiscus tiliaceus* (Sea Hibiscus)
- Lc *Lophostemon confertus* (Brush Box)
- Pe *Podocarpus elatus* (Plum Pine)
- Ss *Stenocarpus sinuatus* (Firewheel Tree)
- TL *Tristaniopsis 'Luscious'* (Water Gum)
- Wf *Waterhousia floribunda* (Weeping Lilly Pilly)

Connection to Country - Bush Tucker

- Backhousia anisata* (Aniseed Myrtle)
- Backhousia citriodora* (Lemon Myrtle)
- Macadamia integrifolia* (Macadamia)
- Podocarpus elatus* (Plum Pine)

Park Edge

- Corymbia maculata* (Spotted Gum)
- Corymbia eximia* (Yellow Bloodwood)
- Banksia integrifolia* (Coastal Banksia)
- Geijera parviflora* (Wilga)

Section 4 – Design Elements

4.2 Planting

4.2.2 Planting overview

Street trees and mass planting form a significant part of the Urban Forest network for Wickham where trees provide numerous environmental, social, and economic benefits. The Wickham Street Tree Plan provides a masterplan of street tree planting which reflects the hierarchy of streets and character of the unique precincts within the suburb.

4.2.3 Design Considerations

Existing Environment

Site appropriate selection of tree species is critical to providing a healthy asset. Plant species must be able to cope with extended dry periods and the existing site conditions and coastal environment.

Species selection

A Street Tree Plan has been developed from the retention of existing streets, street upgrades and new streets. Tree species have been selected tolerating urban conditions and a wide range of verge widths.

Many streets within Wickham feature quality existing trees which provide a good level of existing landscape amenity to the suburb and reinforce street character. Wherever possible, existing quality street trees have been retained, where additional species of the same species or complementary species are proposed. Existing tree species for retention include Araucarias, Tuckeroos, Lilly Pillys, Spotted Gums and Hills Figs. Where there are whole-scale street updates or where new trees are required to replace removed trees due to poor health or other reasons, new tree species should be in accordance with the Wickham Street Tree Plan.

A 100% native tree palette has been developed with a variety of species which provide seasonal interest throughout the year. Connection to Country is referenced in appropriate locations for bush tucker education and use, which align with segments of the Connection to Country Trail through Wickham.

The size of selected street trees must consider available soil volumes, above ground space available, including the presence of overhead powerlines and underground service locations. The potential for root impact on adjoining pavements should also be considered.

Tree selection has also been informed by the Newcastle Street Tree Selection Manual where specific tree palettes have considered the character and identity of the Wickham precincts, as well as providing amenity and biodiversity features. These include Union Street, Emerging Industry, Civic Streets, Village Street and Park Edge.

Access

Street trees should be located and maintained to ensure suitable clearances for pedestrian, bike and cyclist movement where required.

Services

Services coordination and management will need to be given to underground and overhead services and how these can be accommodated in the tree pits design, or located in a suitable services zone adjacent to the tree pit.

Passive irrigation


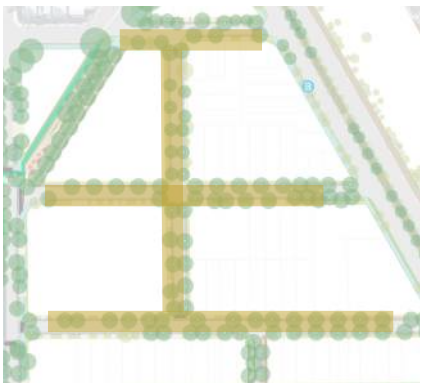


Opportunity for passive irrigation should be considered for tree and understorey planting to reduce water consumption and reliance on irrigation systems. Using passive irrigation to maximise the availability of water to the tree root zone will ensure that the street tree will establish more quickly and be more resilient in the long term.

Maintenance Considerations


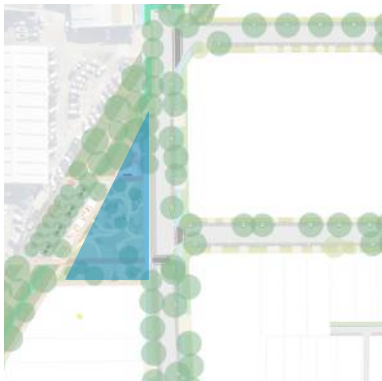
As with trees in the natural environments, trees within passively irrigated tree pits will need to be monitored for health and vigour. Due to the increased fluctuations in the moisture conditions of the sites, it is important that any factors that adversely impact the tree are picked up early and adjustments made.

VERGE WIDTHS			
LARGE	MEDIUM	SMALL / NARROW	PASSIVE IRRIGATION
Includes structure palette and bush foods, low shrubs, grasses and groundcovers with great diversity, foliage interest, texture and colour	Clusters of grasses with groundcover surround	Tough grasses which can tolerate tough and constrained conditions	Tolerant of temporary inundation and water treatment
Westringia ‘flat and fruity’	Lomandra verday	Lomandra verday	Lomandra verday
Myoporum yareena	Myoporum yareena	Dianella cerulea	Carex appressa
Westringia ‘grey box’	Dianella revolta		Ficinia nodosa
Lomandra verday #	Dianella longifolia		Juncus usitatus
Dianella revoluta	Goodenia ovata ‘Gold Cover’		
Imprrata cylindrica			
Carpobrotus aussie rambler#			
Rhagodia ‘Aussie flat bush’#			
Plectranthus graveola#			
Bush or oregano dwarf size			
Plectranthus argentatus			
Melaleuca ‘Ulladulla Beacon’			
Goodenia ovata ‘Gold Cover’			
Westringia fruticosa ‘Mundi’			

4.2 Planting

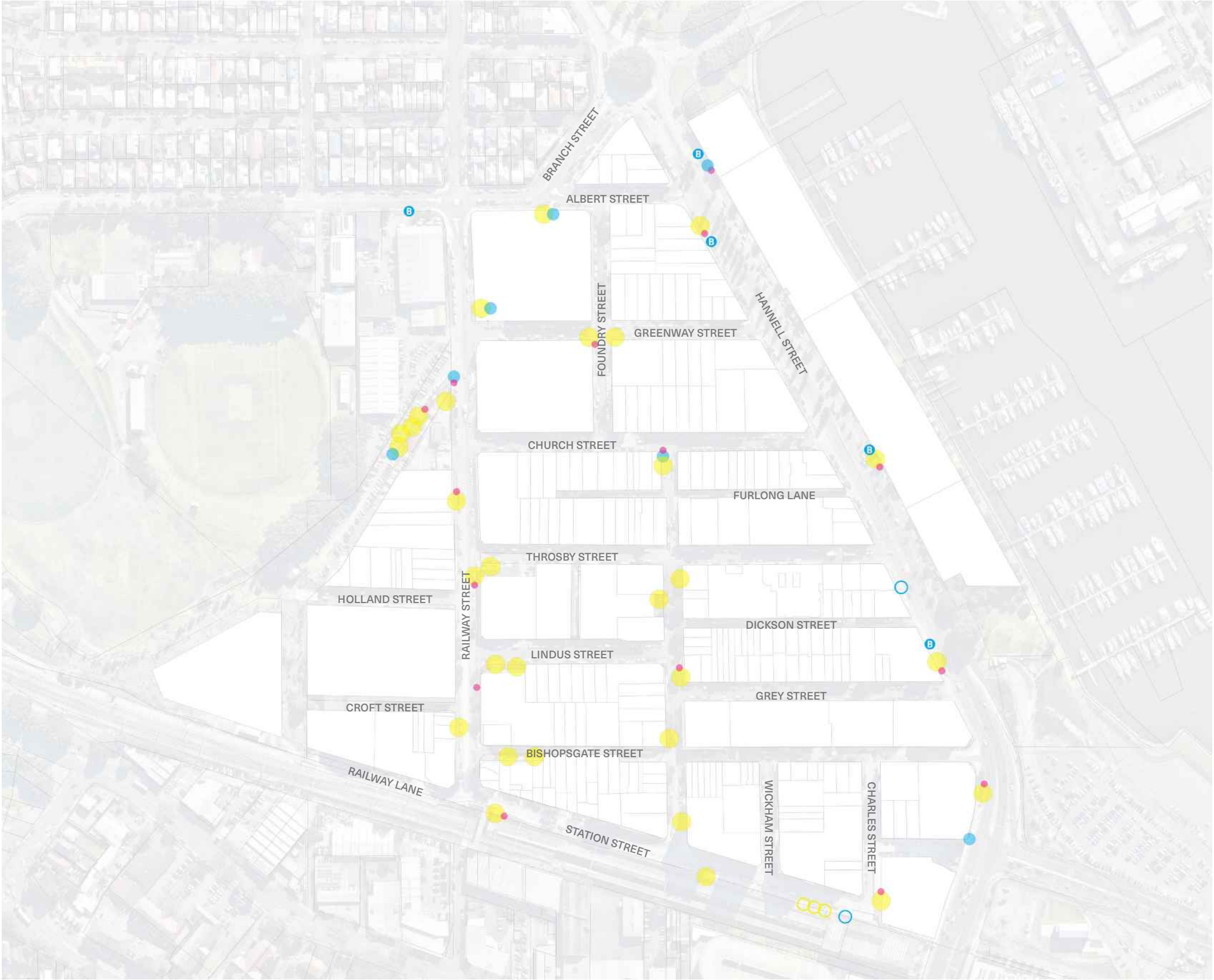
KEY PLAN	CHARACTER AREA	SPECIES BY SCALE		VERGE WIDTHS
	<p>UNION STREET</p> <p>Union Street consists of a feature street with a north to south orientation. Verges range in width from large to medium in size with opportunities for trees in verge and blister plantings. Small and medium trees are proposed for this precinct including the Kurrajong, Lilly Pilly and Firewheel Tree.</p>	<p>Medium Trees</p> <p>Brachychiton populneus</p> <p>Acmena smithii</p> <p>Small Trees</p> <p>Stenocarpus sinuatus</p>		<p>Medium to large</p>
	<p>EMERGING INDUSTRY</p> <p>This precinct features mostly east to west street orientations with medium to narrow verges. There are opportunities for tree plantings in blisters. Small to medium trees are proposed for this precinct including Plum Pine, Water Gum, Tuckeroo, Sea Hibiscus and Tulipwood. Existing Brush Box are proposed to be retained with additional species suitable for blister plantings.</p>	<p>Medium Trees</p> <p>Podocarpus elatus</p> <p>Lophostemon confertus (continue existing)</p> <p>Small Trees</p> <p>Tristaniopsis 'Luscious'</p> <p>Cupaniopsis anacardioides (continue existing)</p> <p>Hibisus tiliacious</p> <p>Harpullia pendula</p>		<p>Medium to narrow</p>
	<p>CIVIC STREET</p> <p>This precinct is the perimeter edge to Wickham with various solar orientations. Verge widths vary in size from small to medium however a range of tree sizes are suitable from large, medium and small. Tree species include continuing the existing tree plantings of the Norfolk Island Pine, Spotted Gum, Crow's Ash and Brush Box. Additional new trees include Water Gum and Tuckeroo.</p>	<p>Medium/Large Trees</p> <p>Corymbia maculata (continue existing)</p> <p>Flindersia australis (continue existing)</p> <p>Lophostemon confertus (continue existing)</p> <p>Small Trees</p> <p>Tristaniopsis 'Luscious'</p> <p>Cupaniopsis anacardioides (continue existing)</p>		<p>Medium to narrow</p>
	<p>VILLAGE STREETS</p> <p>This precinct features mostly east to west orientated streets with medium to narrow verges. There are opportunities for blister plantings to allow for larger tree sizes. Tree species include continuing the existing tree plantings of Lilly Pilly. Proposed tree species include the Weeping Lilly Pilly, Brush Box, Water Gum, Tuckeroo and Blueberry Ash.</p> <p>This precinct also features the Wickham Connection to Country trail providing opportunity for tree bush tucker education and use. Tree species include Aniseed Myrtle, Lemon Myrtle, Macadamia and Plum Pine.</p>	<p>Medium/Large Trees</p> <p>Waterhousia floribunda</p> <p>Lophostemon confertus</p> <p>Small and Narrow Trees</p> <p>Tristaniopsis 'Luscious'</p> <p>Cupaniopsis anacardioides (continue existing)</p> <p>Elaeocarpus reticulatus (narrow back lane)</p>	<p>Bush Tucker Trees</p> <p>Bachousia citriodora</p> <p>Podocarpus (female and male for fruit)</p> <p>Backhousia anisata</p> <p>Glochidion fernandi</p>	<p>Medium to large</p>

4.2 Planting

KEY PLAN	CHARACTER AREA	TREE SPECIES	UNDERSTOREY SPECIES	VERGE WIDTHS
	<p>RAILWAY CORRIDOR</p> <p>This precinct features the existing and retained Hills Figs, where planting is proposed to complement the existing trees and public site reuse. A range of tree species for this open space environment include Spotted Gum, Yellow Bloodwood, Coastal Banksia, Wilga, Plum Pine and Lemon Myrtle.</p>	<p>Feature Trees</p> <p>Ficus rubiginosa</p> <p>Native Dry Bushland Trees</p> <p>Corymbia maculata</p> <p>Corymbia eximia</p> <p>Banksia integrifolia</p> <p>Geijera parvifolia</p> <p>Shady Gully Trees</p> <p>Glochidion fernandi</p> <p>Tristaniopsis ‘Luscious’</p> <p>Podocarpus (female and male for fruit)</p> <p>Backhousia citriodora</p>	<p>Shrubs</p> <p>Westringia sp.</p> <p>Grasses</p> <p>Lomandra verday</p> <p>Carex appressa</p> <p>Ficinia nodosa</p> <p>Juncus usitatus</p> <p>Imperata cylindrica</p> <p>Lomandra longifolia</p>	<p>Medium to narrow</p>
	<p>BASIN</p> <p>Attached to the former Bullock Island Railway Corridor is an urban wetland, providing important greenery, outdoor recreation and exercise amenity, and water sensitive urban design.</p> <p>The planting approach builds on the wetland’s exploitation of existing topography to detain and hold on to water during precipitation events. This water storage will allow a wetland biome to develop, and recall the ecologies of Wickham before the development of the city.</p> <p>This approach has a number of advantages. It will establish a strong local character through the use of water tolerant native species. It will improve water quality through use of sedges and reeds. And it will create a habitat and food source for native fauna species - to benefit of both the human and non-human residents of Wickham.</p>	<p>Medium/Large Trees</p> <p>Corymbia maculata (continue existing)</p> <p>Flindersia australis (continue existing)</p> <p>Lophostemon confertus (continue existing)</p> <p>Small Trees</p> <p>Tristaniopsis ‘Luscious’</p> <p>Cupaniopsis anacardioides (continue existing)</p>	<p>Shrubs</p> <p>Westringia sp.</p> <p>Melaleuca thymifolia ‘Pink Lace’</p> <p>Hardebnergia ‘Meema’</p> <p>Callistemon ‘Little John’</p> <p>Plectranthus ‘Blue Spires’</p> <p>Grasses</p> <p>Imperata cylindrica</p> <p>Themeda australis</p> <p>Danthonia tenior</p> <p>Lomandra longifolia</p> <p>Themeda ‘Mingo’</p> <p>Isolepsis nodosa</p> <p>Groundcovers</p> <p>Viola hederacea</p> <p>Dichondra repens</p> <p>Myoporum parvifolium</p> <p>Myoporum ‘Yareena’</p> <p>Carpobrotus ‘Aussie Rambler’</p> <p>Brachyscome ‘Break O Day’</p> <p>Casuarina ‘cousin It’</p> <p>Hardenbergia ‘Flat White’</p>	<p>Medium to narrow</p>

4.3 Furniture

4.3.1 Furniture Typologies Plan



KEY

Existing Furniture

- Benches
- Bicycle Parking Racks

Proposed Furniture Zones

- Benches
- Bin
- Bicycle Parking Racks
- Bus stops

4.3.2 Street Furniture overview

Street furniture is an integral part of the public realm. This PDP proposes a suite of well designed street furniture that is consistent with the wider urban context of Newcastle.

Proposed furniture is drawn from the City of Newcastle’s *City Centre Public Domain Technical Manual*. Additional desktop-based research has been done to confirm furniture selections and siting are consistent with existing provisions. It is noted that bespoke places, where identified, may require alternate treatments such as highlight paving and custom designed street furniture.

City of Newcastle standard drawings apply to any new furniture elements.

4.3.3 Street Furniture selection

It is important that a consistent and coordinated suite of elements extends over the site as a whole to provide a homogenous appearance and also minimise maintenance requirements. Per the technical manual, street furniture in Wickham will be chosen according to consideration of:

- a consistent suite of materials and furniture for the public domain within Newcastle city centre as defined by the LEP 2012 boundary (Refer to City Centre Boundary Figure 1)
- efficiencies in the documentation, procurement and construction of streetscape and public domain works
- sustainability in material selection including life cycle costing and provenance;
- consistency with heritage elements within our city centre;
- sensitivity to the various precincts, activity nodes and distinctive places within our city centre.

4.4 Lighting

4.4.1 Approach

During consultation, the community indicated that information on lighting should be included in the PDP and that it should detail how lighting will be used to provide a safe after-dark environment without being too harshly lit.

Lighting is critical in the development of liveable and active places. This PDP considers lighting a fundamental component of the public realm and adopts a holistic approach. This includes safe and sensitive street lighting for pedestrians, people on bike, and vehicles.

Lighting should be incorporated into future upgrades into the public domain and minimise the impact of obtrusive lighting to residential amenity.

To minimise the impact on street trees and existing infrastructure through additional conduiting, the adoption of Ausgrid-endorsed light poles and fittings should be utilised within road corridors allowing for long term maintenance and management.

Additional light fittings intended for distinctive places and community land will require consultation and approval of the City of Newcastle Asset Management Team.

Section 4 – Design Elements

4.5 Public Art & Interpretation

4.4.2 Art approach

Wickham is home to a number of unique works of varying media, including the clay art “bollards” on Union Street, large-scale public sculptures on the waterfront, and street art, especially to the north of the suburb. It can boast a wealth of European heritage objects in its public realm, including historic steel rails and brick pavers.

This PDP safeguards these items by proposing the improvement of their setting within the public realm - so that they are more visible, accessible, and better part of the ongoing story of Wickham.

During the engagement process, Wickham's residents said that the best ways to welcome others to Wickham at its entrypoints were natural elements (62%) and art elements (also 62%). Of the stories, art, and other elements that should be celebrated, the community pointed to the history of Wickham, Aboriginal living culture and sites of significance, local landmarks such as the Tree of Knowledge, heritage buildings, local art, and the natural landscape.

This PDP’s art approach has learned from and built upon these important insights from the community. It recognises the critical role public art can play in the public realm. This PDP therefore provides for how best to celebrate existing works and integrate new works.

4.4.3 Proposals by this PDP

Complementing this strategy for existing elements, this PDP proposes a number of different walking trails that would serve as organising structures for the siting of new public artwork and heritage interpretation.

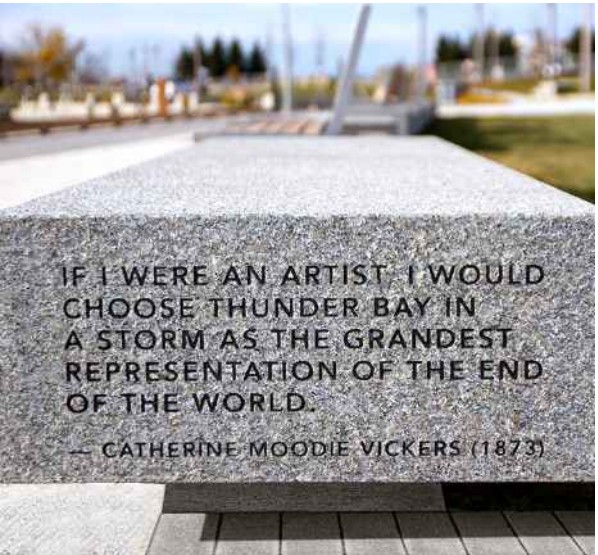
These trails are:

- 1. The Connecting with Country trail
- 2. The European Heritage trail
- 3. The Street Art trail

A detailed map locating the arrangement of these trails and other key heritage landmarks and art objects can be found on the following page.

Note that the commissioning of any new works in subject to further development by City of Newcastle and a separate art procurement process that lies beyond the scope of this PDP.

Any new public art will not pose a risk to the public, be able to withstand vandalism and be long lasting with minimal maintenance requirements.



4.6 Public Art & Interpretation

4.5.1 Public Art & Interpretation Opportunities Plan



4.5.2 The Connecting with Country trail

This trail, developed in close co-ordination with First Nations knowledge holders, would celebrate Aboriginal knowledge of Country. It would loop between the two important sites of Wickham Park and the water front and act to restore and retell histories that have been overwritten since colonisation.

4.5.3 The European heritage trail

This trail would celebrate the stories of people that came to live and work in Wickham following the establishment of the city. It would run along the former railway corridor, take in a number of important heritage buildings, and highlight the heritage brick pavers of Union Street.

4.5.4 The Street Art trail

The street art trail links together existing works of street art, mostly concentrated in the Emerging Industries, and could integrate new works on new developments, ensuring the continuity of this practice even as the suburb changes.

KEY

- Land cover**
- Green space / open space
 - Surface and tidal water
- Paving Types**
- Connecting with Country trail
 - European heritage trail
 - Street art trail
- Public art locations**
- Public sculptures
 - Yum Yum Tree (Tree of Knowledge)
 - Potential heritage panel, artefact, or artwork at existing European heritage site
 - Potential public art, footpath mural or sculpture
 - Street art locations
 - Welcome nodes
 - Art bollards on Union Street
 - Bus stops

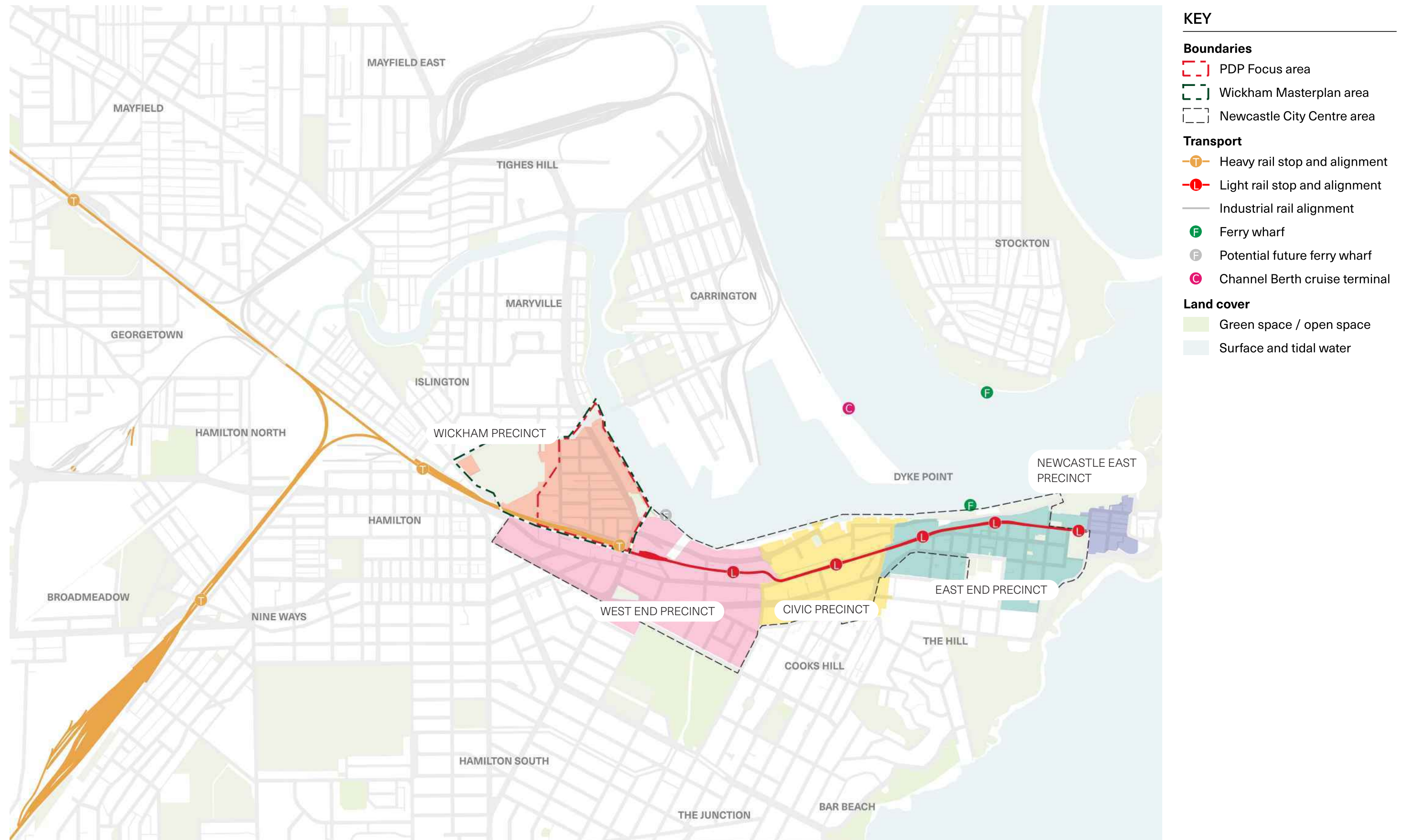
Appendices



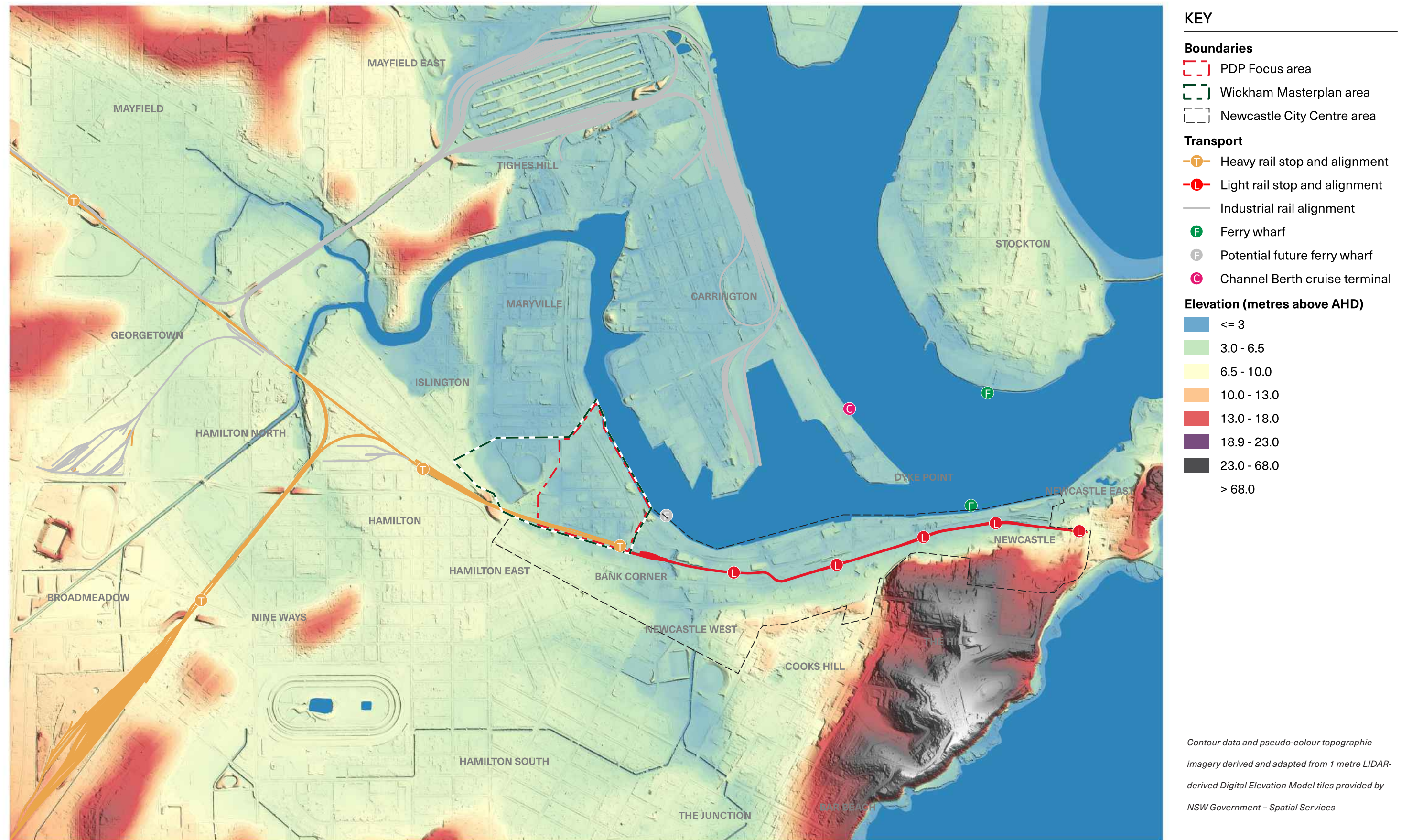
Urban Context Analysis



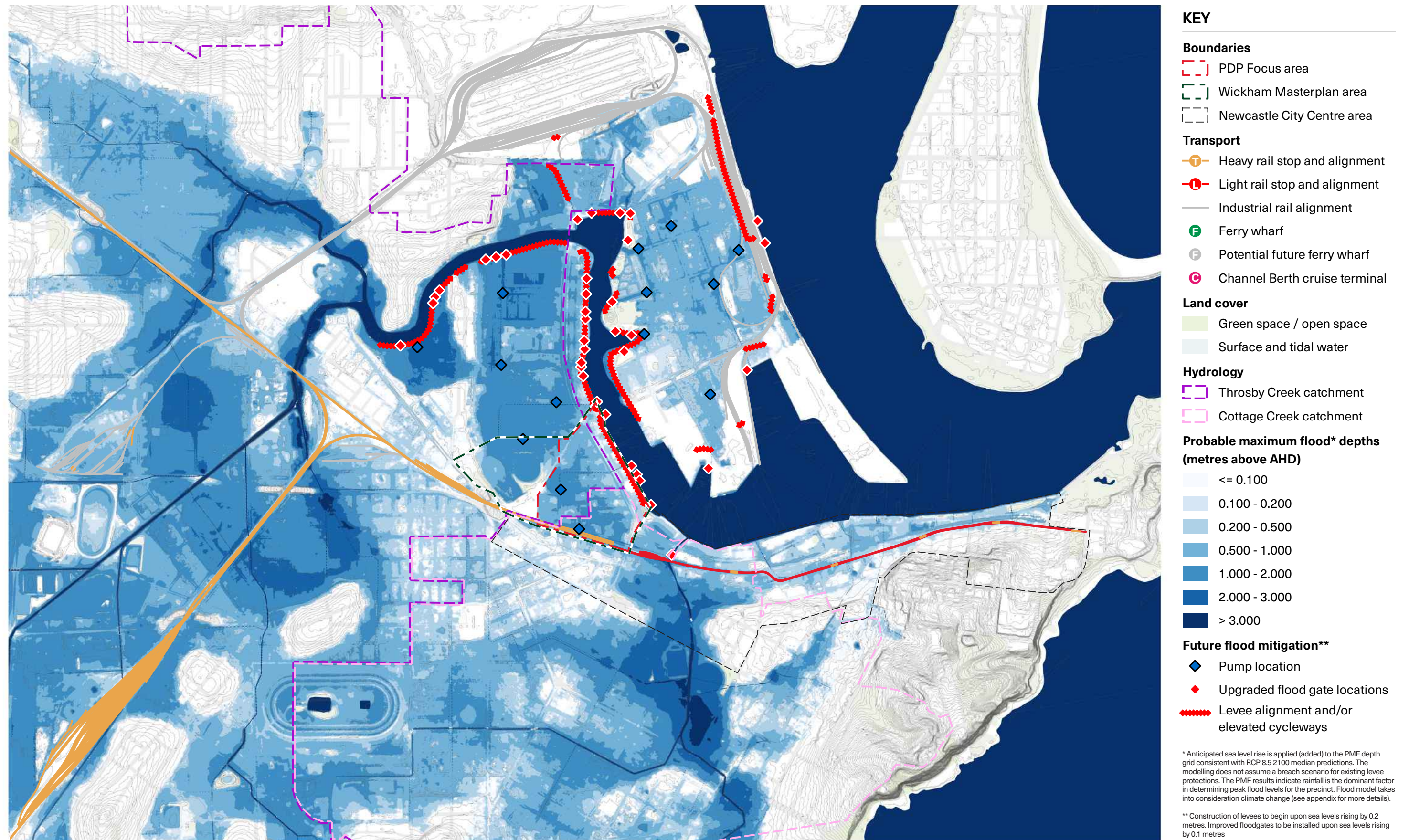
5.1 Neighbourhood precincts



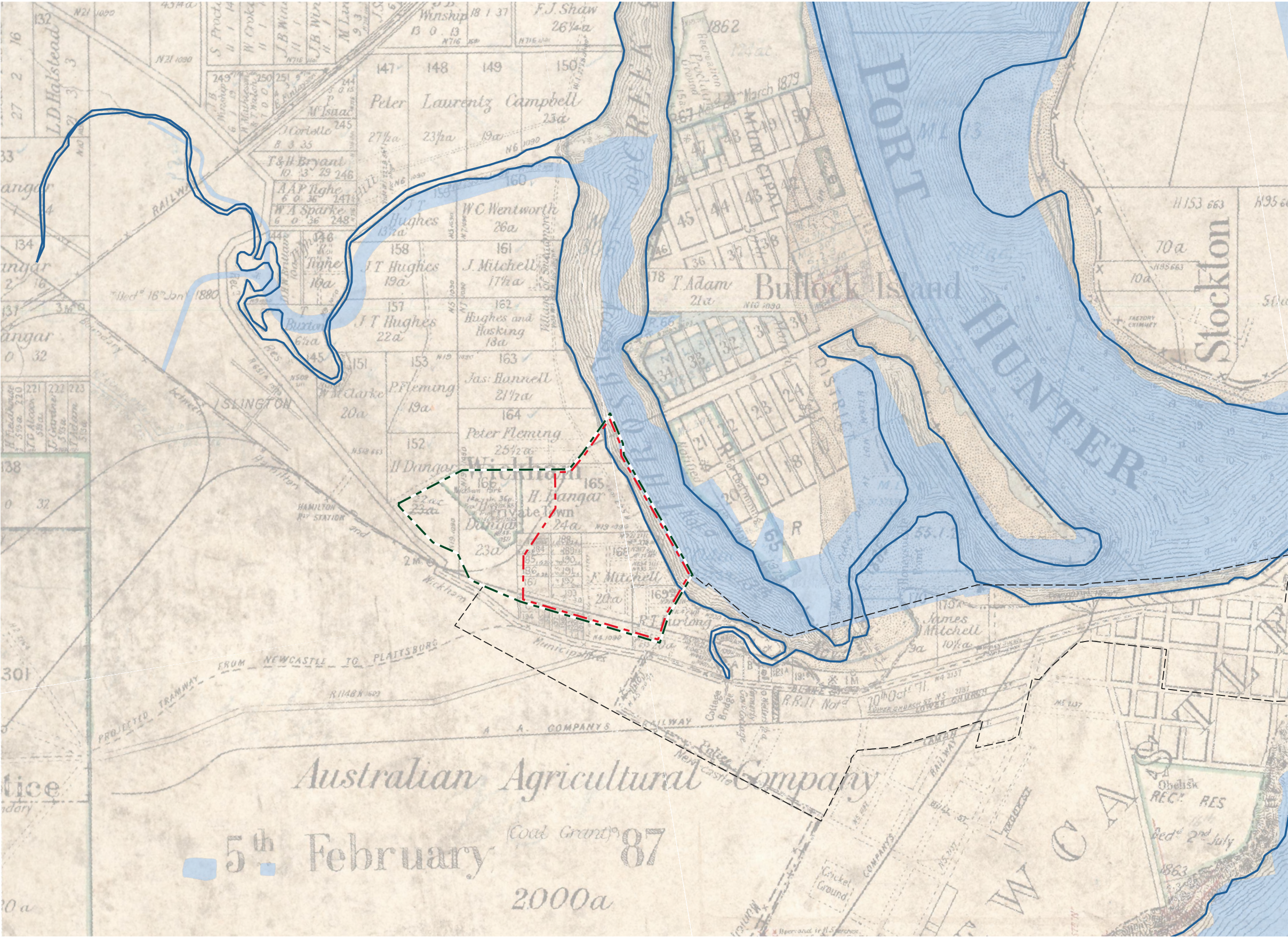
5.2 Elevation



5.3 Hydrology: waterways and flooding - probable maximum flood



5.4 Historic shoreline mapping



KEY

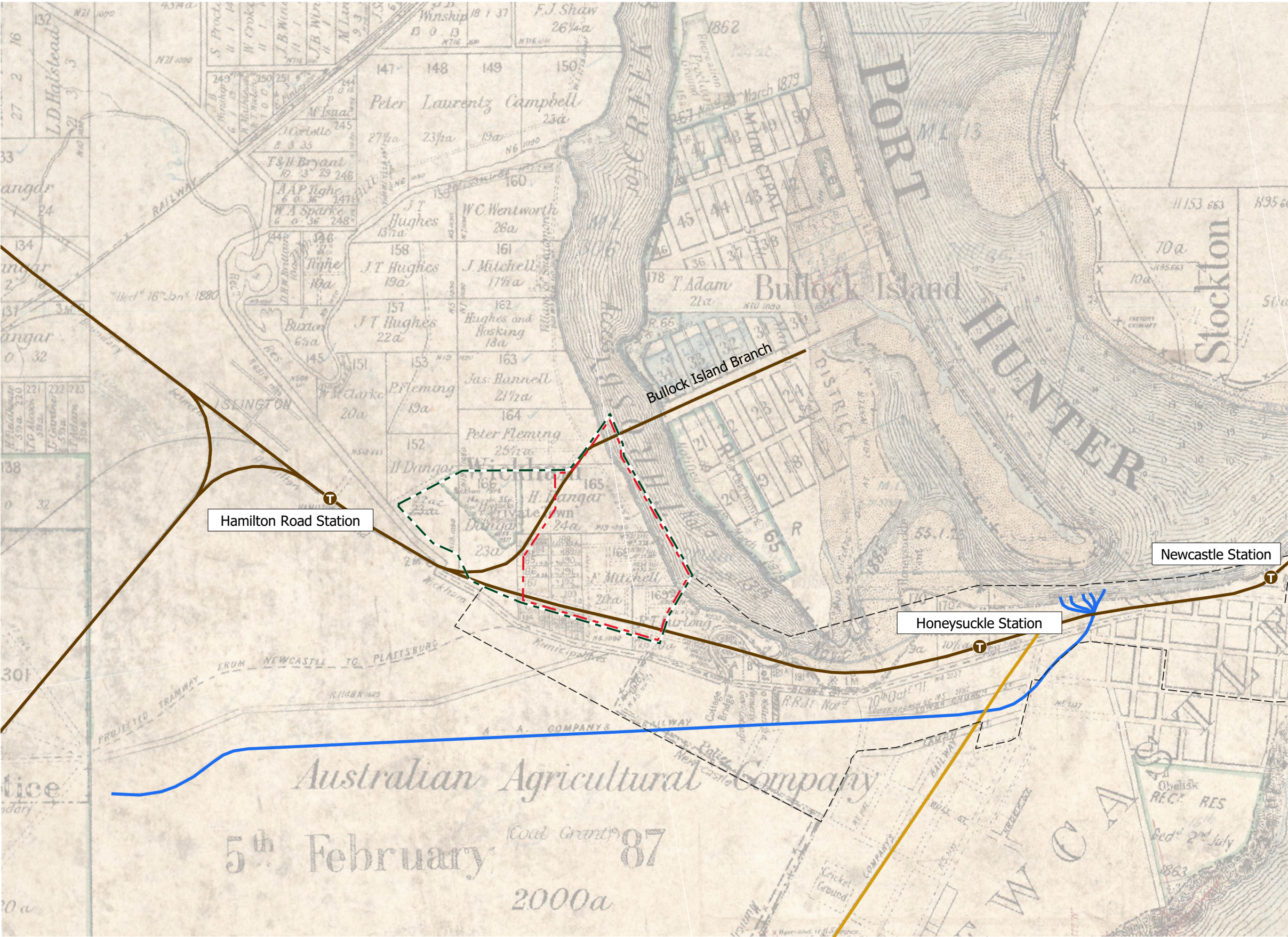
Boundaries

- Project site
- Wickham Masterplan area
- Newcastle City Centre area

Shorelines

- Shoreline February 1887 (indicative)*
- Current shoreline

5.5 Heritage rail mapping



KEY

Boundaries

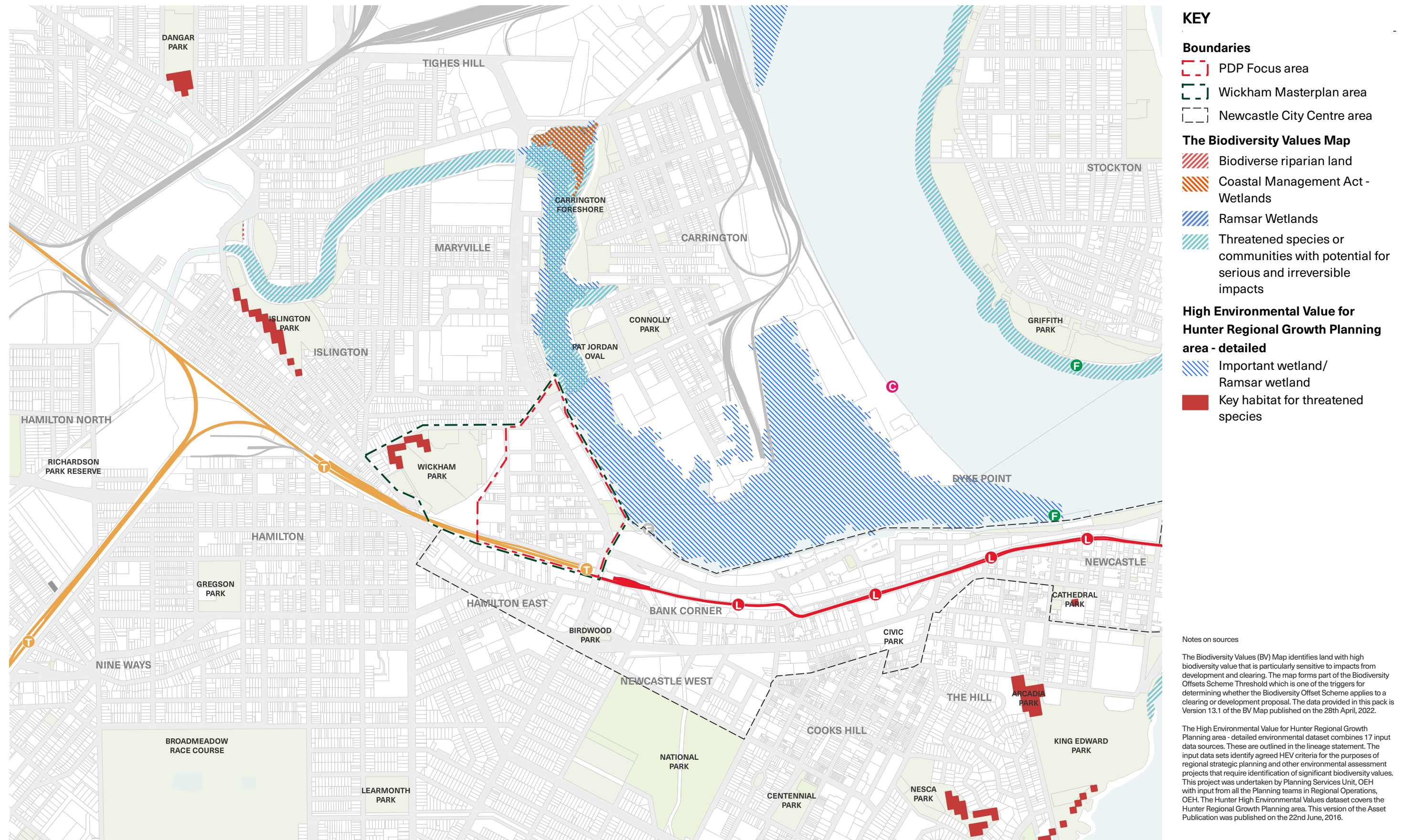
- PDP Focus area
- Wickham Masterplan area
- Newcastle City Centre area

Historic railway

- Australian Agricultural Company's Railway
- Great Northern Railway (NSW Railway)
- Burwood Coal Company Railway

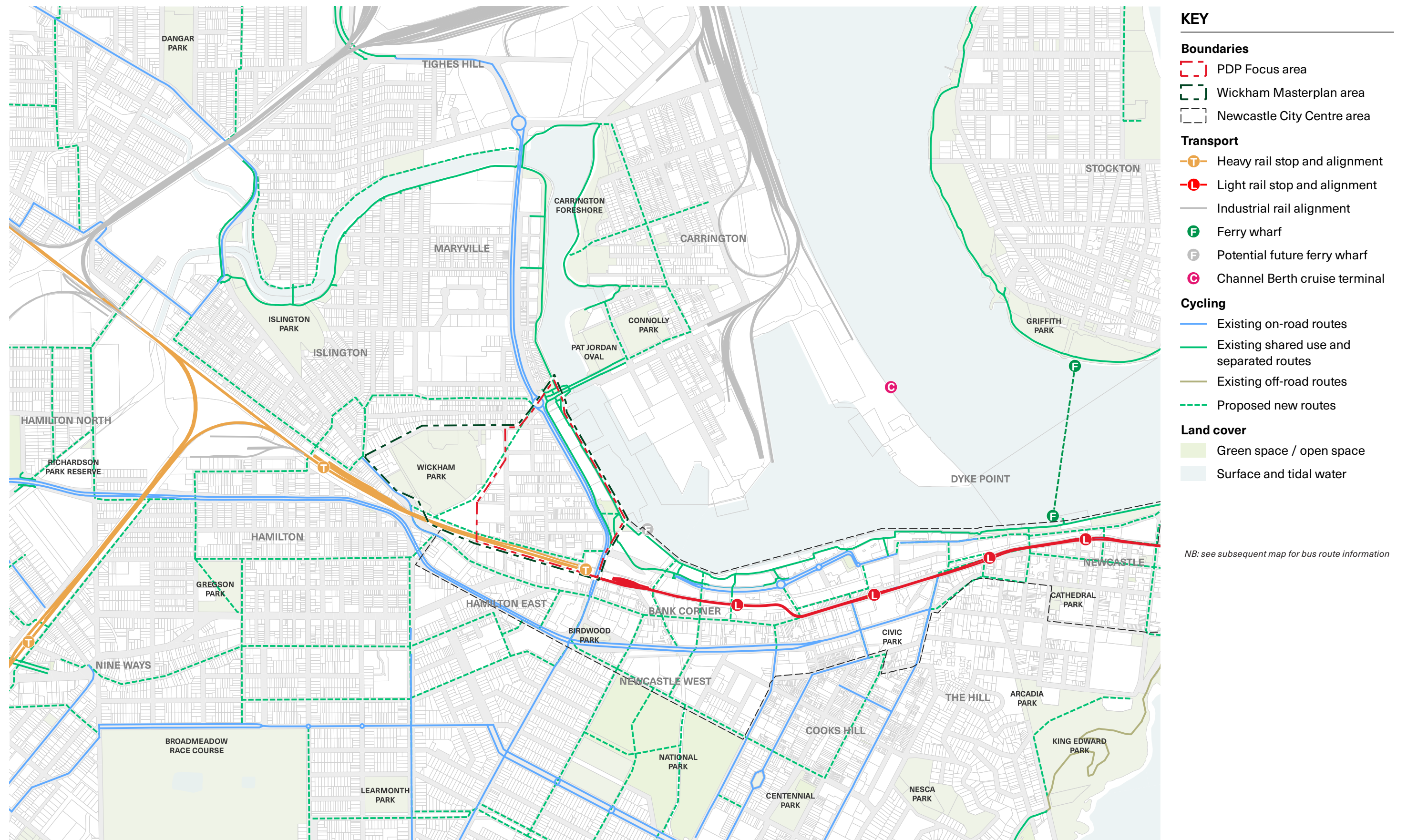
Appendix 1 – Urban Context Analysis

5.6 Ecological communities

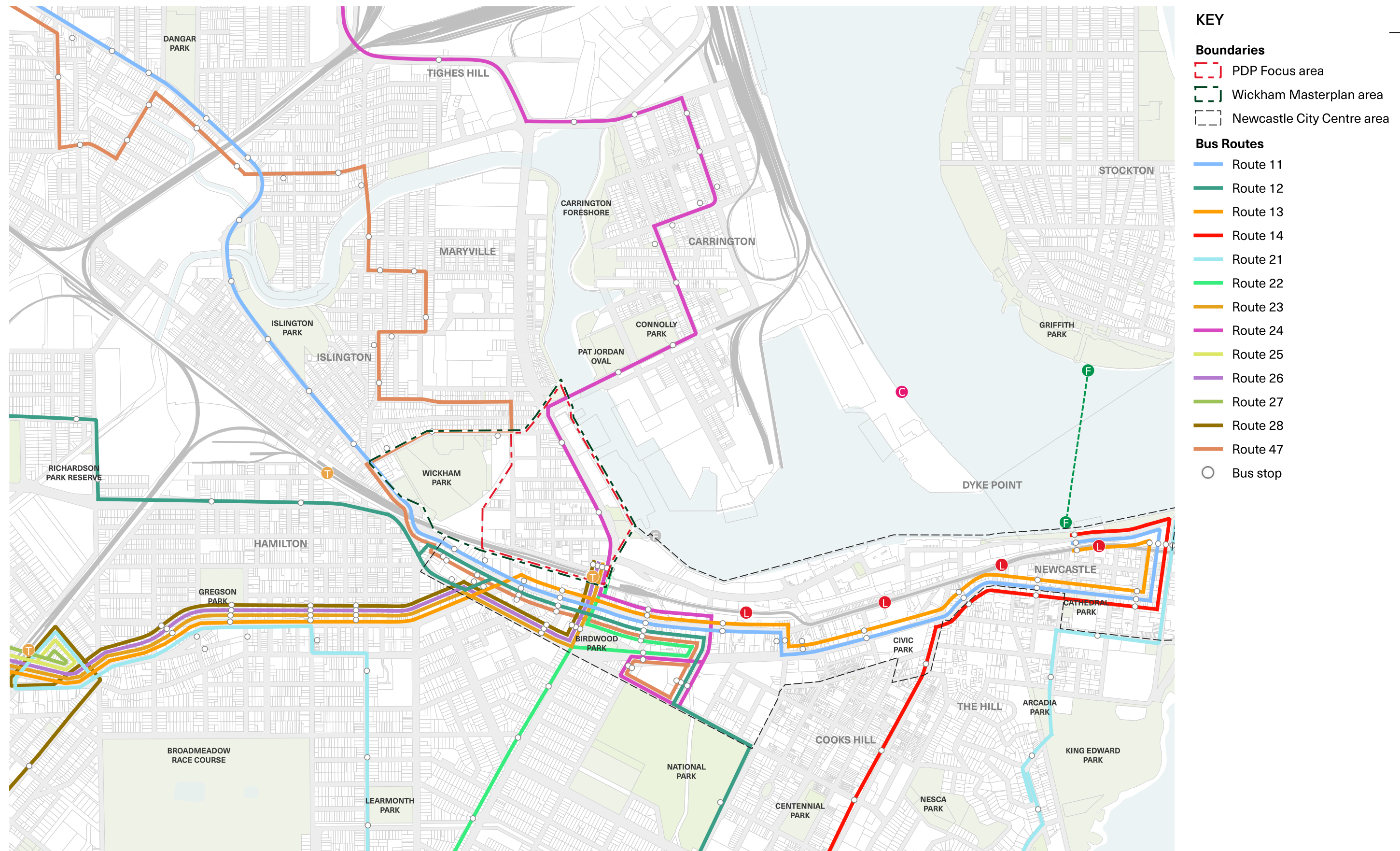


Appendix 1 – Urban Context Analysis

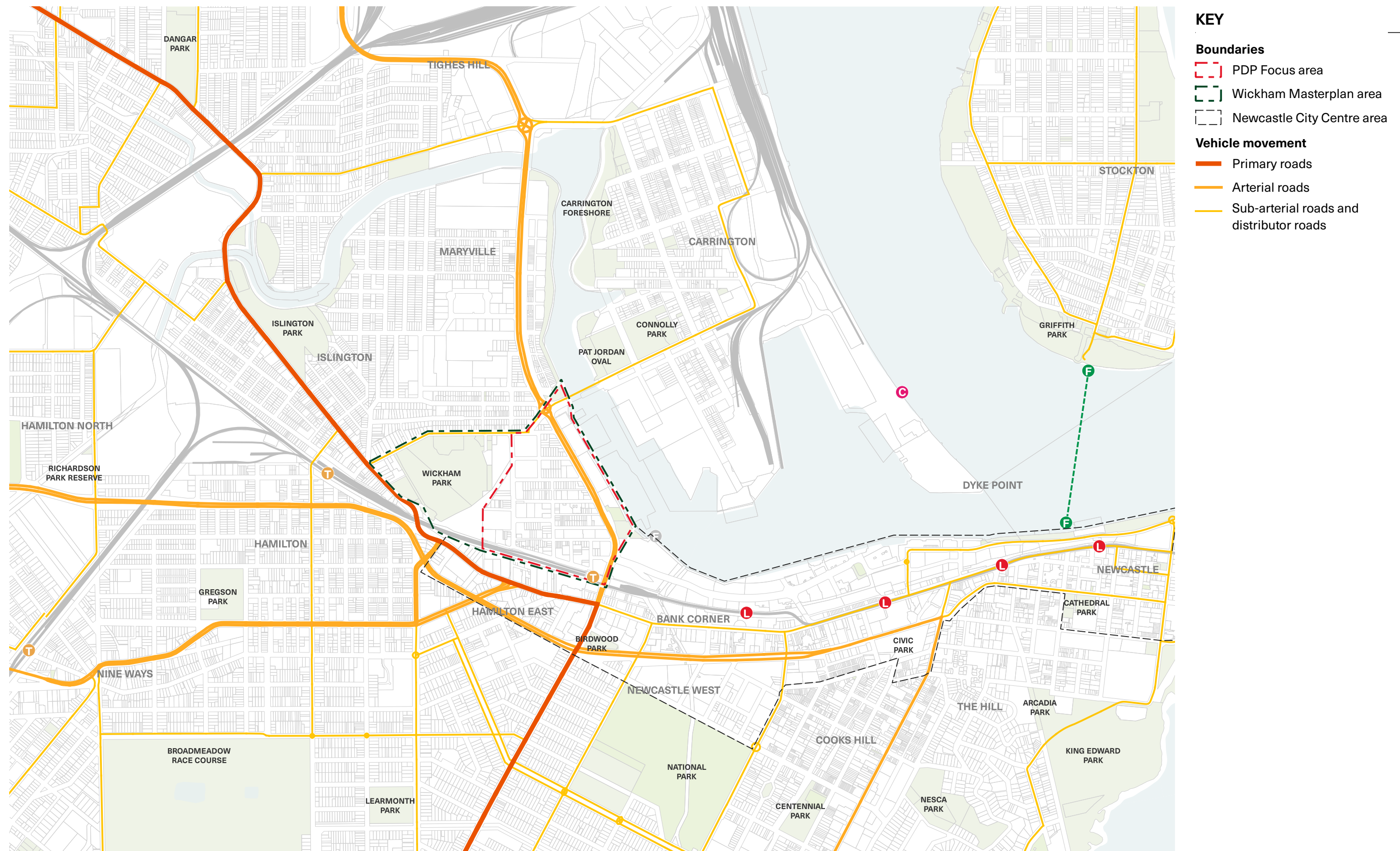
5.7 Active transport



5.8 Public transport

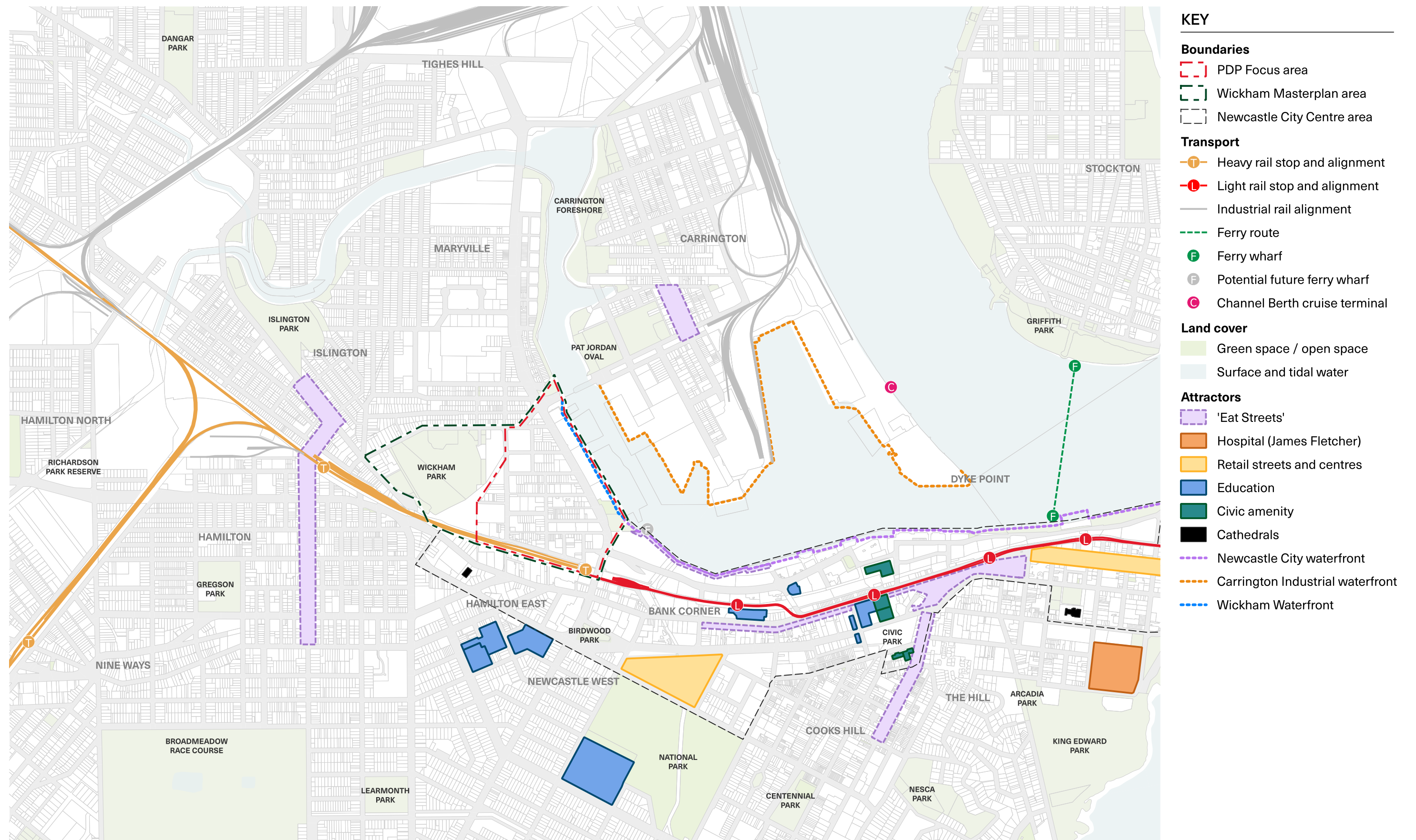


5.9 Vehicle movement

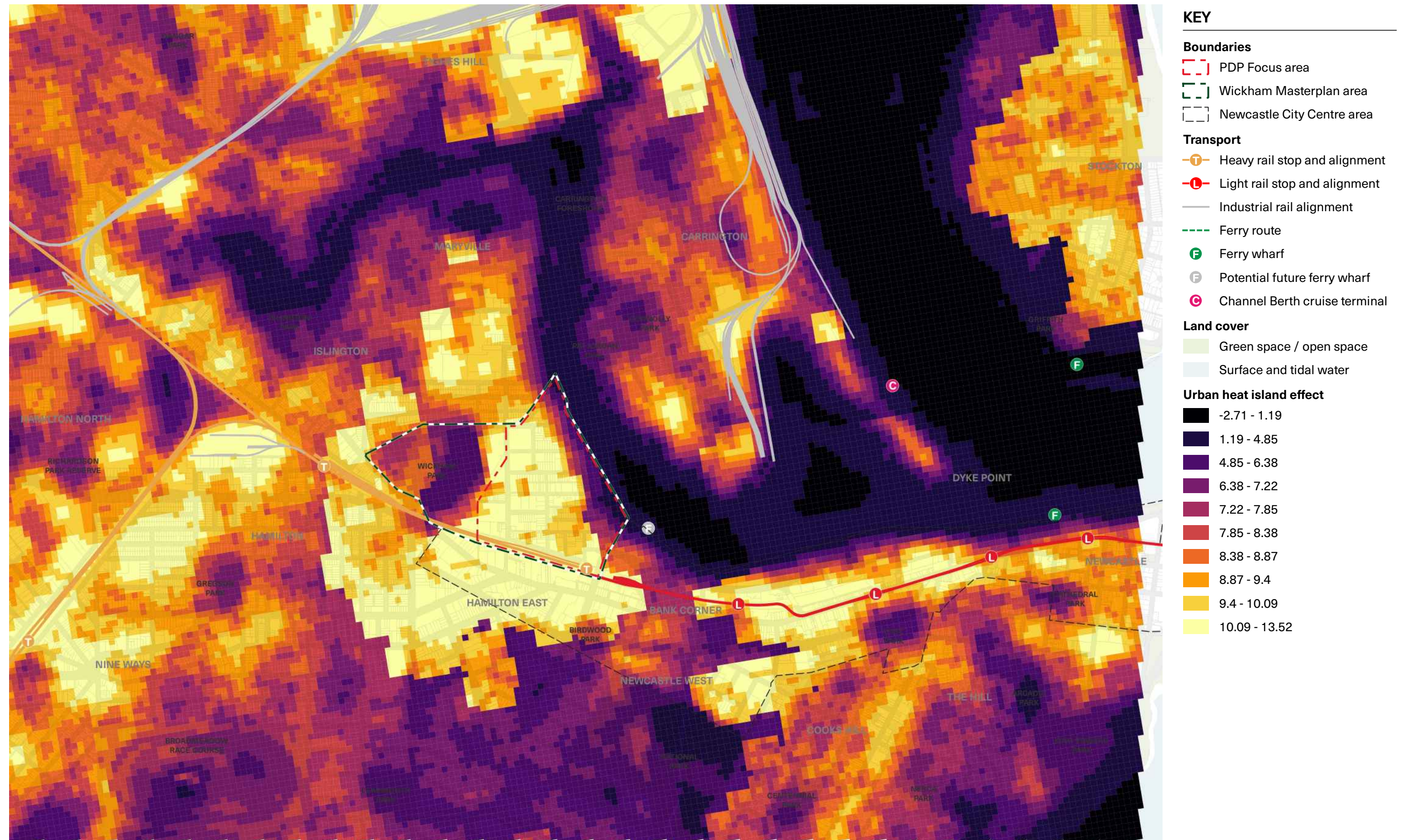


Appendix 1 – Urban Context Analysis

5.10 Activity nodes and attractors

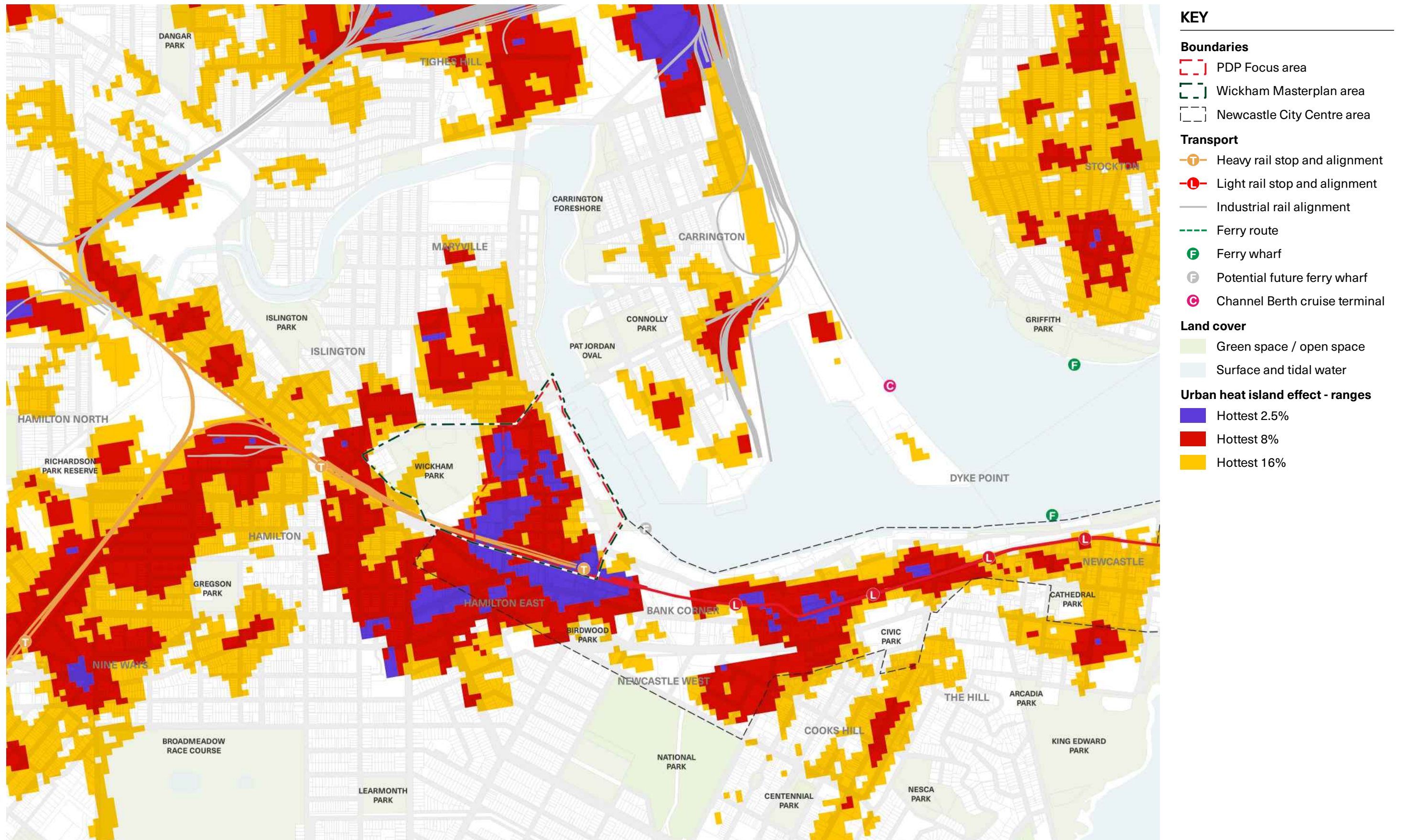


5.11 Urban heat: ranges



Appendix 1 – Urban Context Analysis

5.12 Urban heat: hottest areas

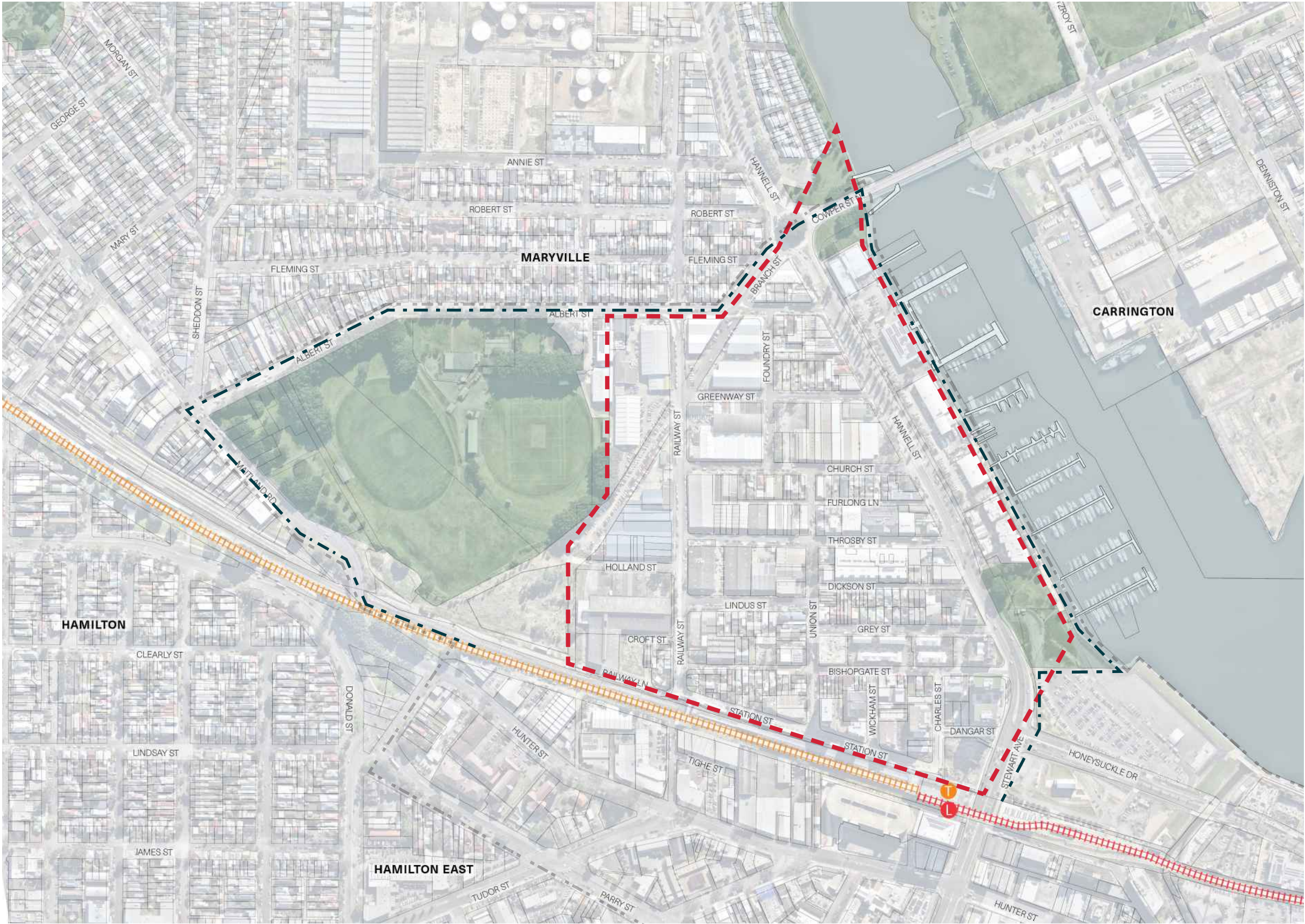


Appendix 2

Precinct Analysis



6.1 Project site



KEY

Transport

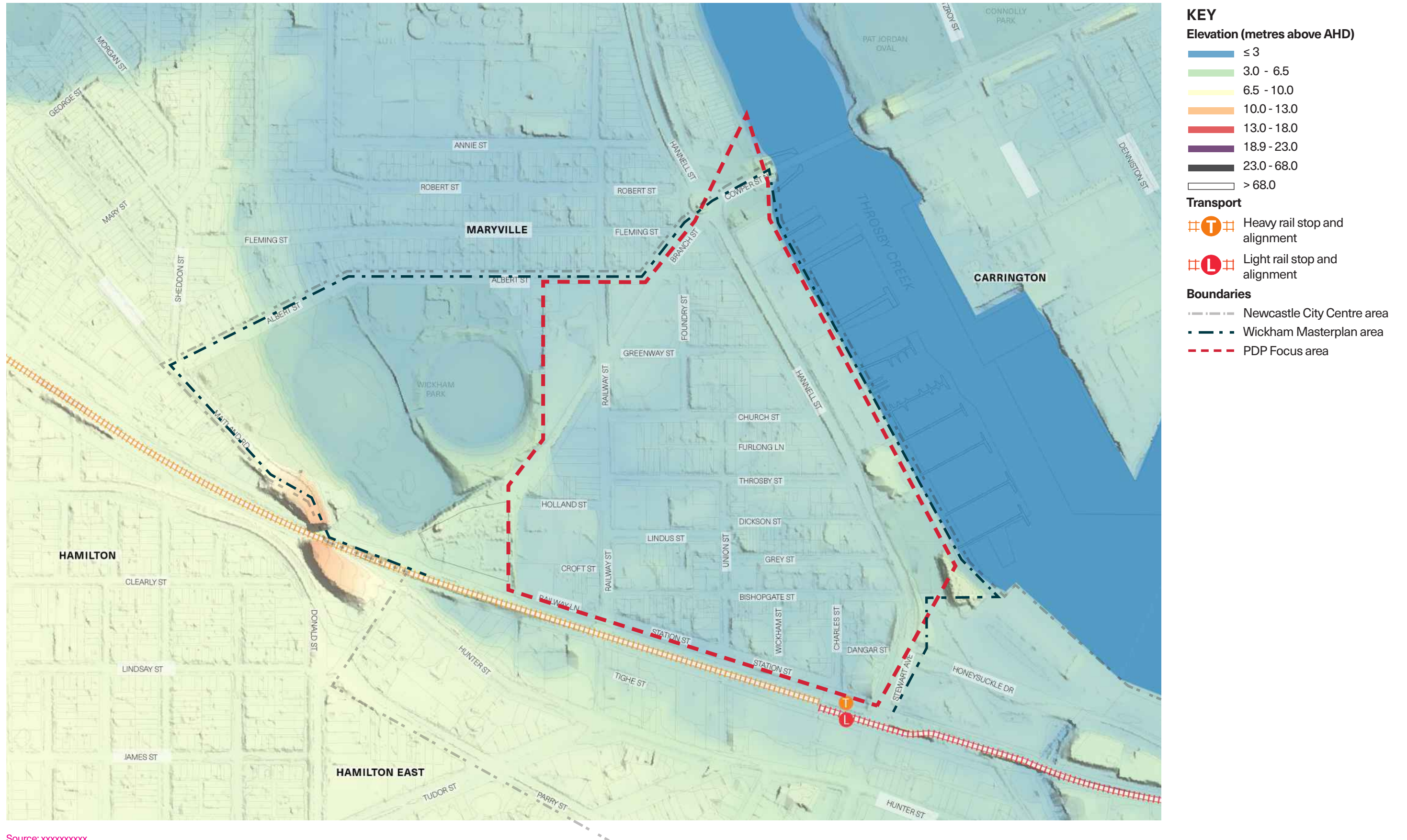
- Heavy rail stop and alignment
- Light rail stop and alignment

Boundaries

- Newcastle City Centre area
- Wickham Masterplan area
- PDP Focus area

Appendix 2 – Precinct Analysis

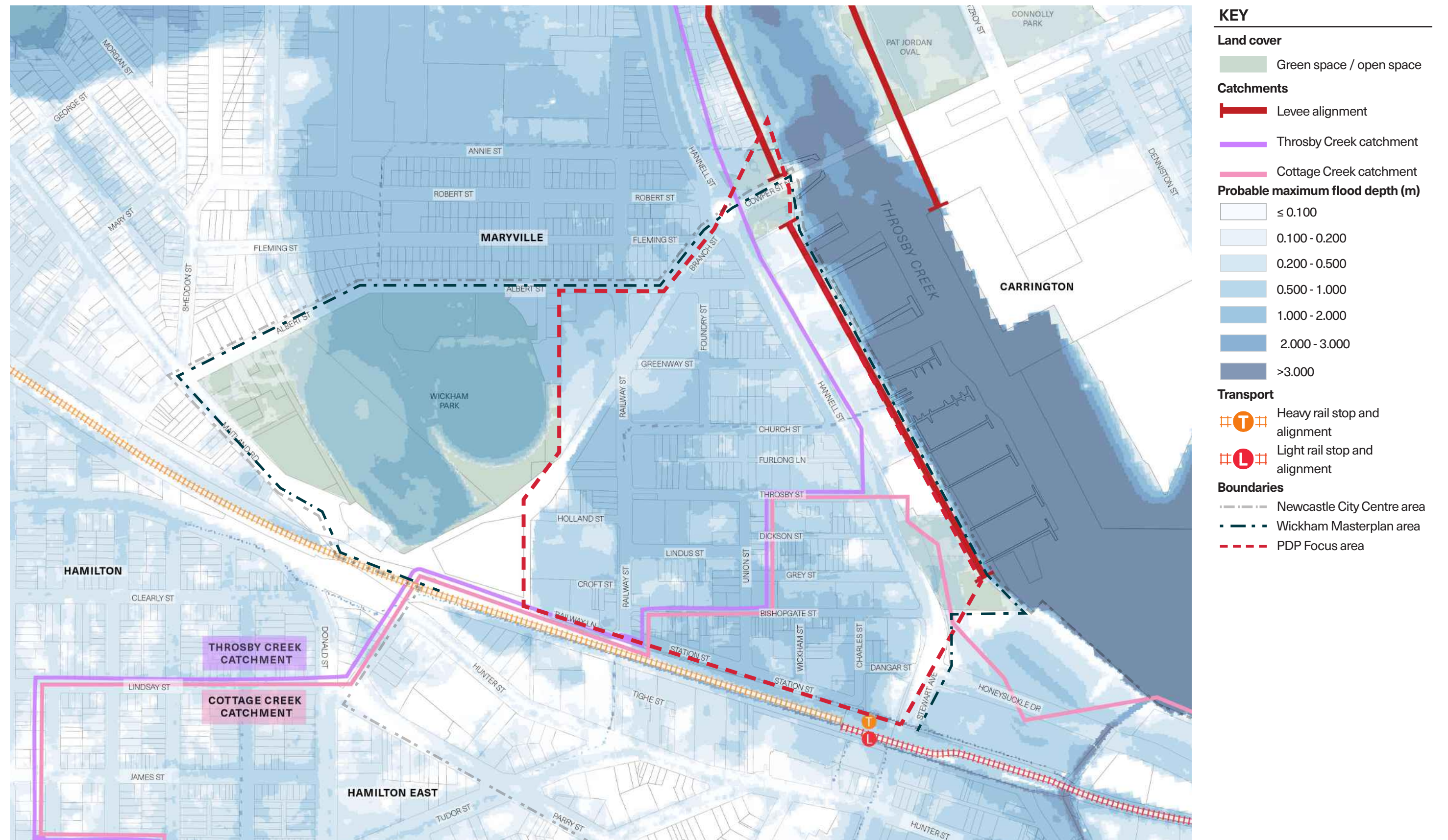
6.2 Topography



Source: xxxxxxxx

Appendix 2 – Precinct Analysis

6.3 Hydrology: waterways and flooding



Source: xxxxxxxx

Appendix 2 – Precinct Analysis

6.4 Streets and open Space



Hannell Street



Station Street



Railway Street



Union Street



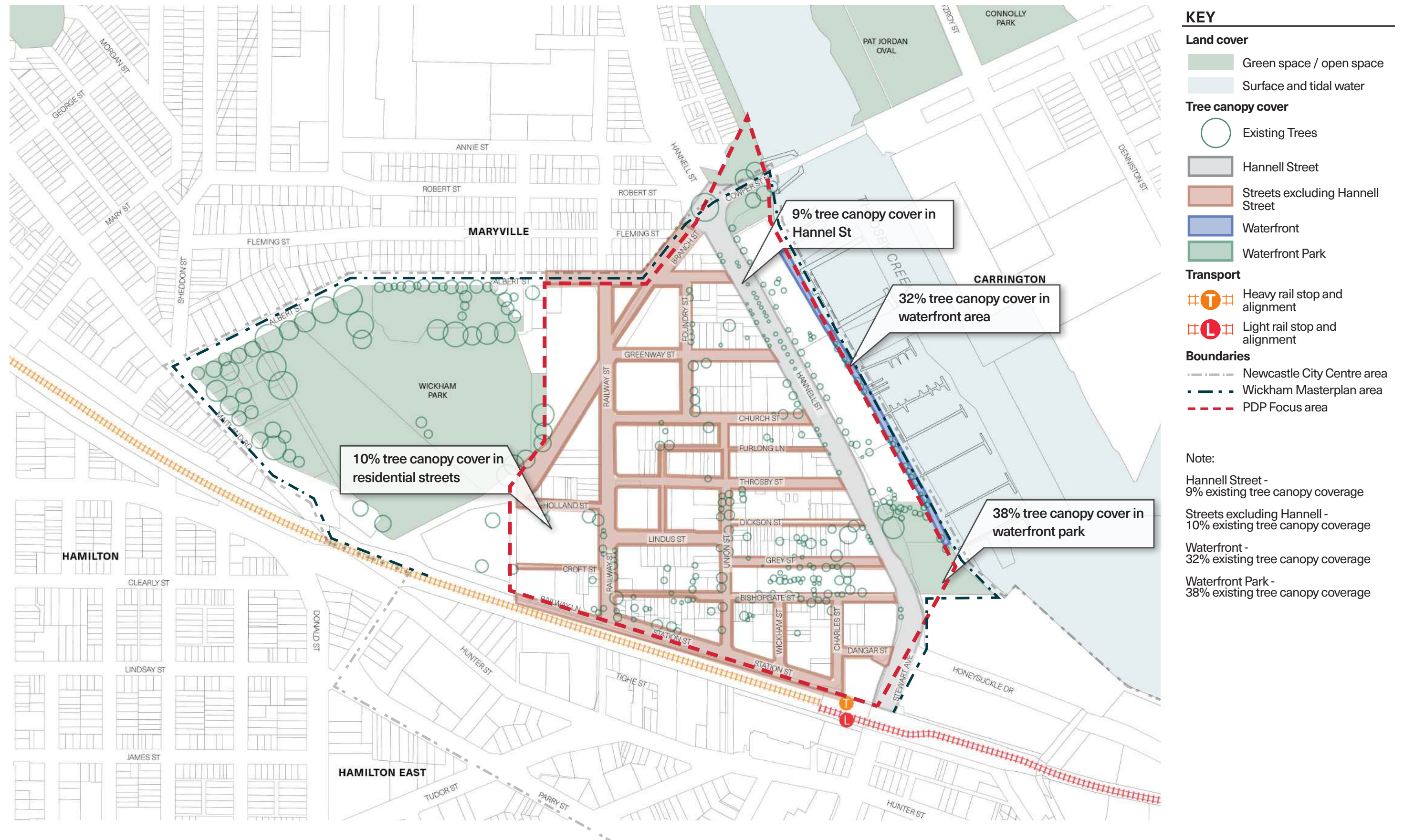
Throsby Street



Furlong Lane

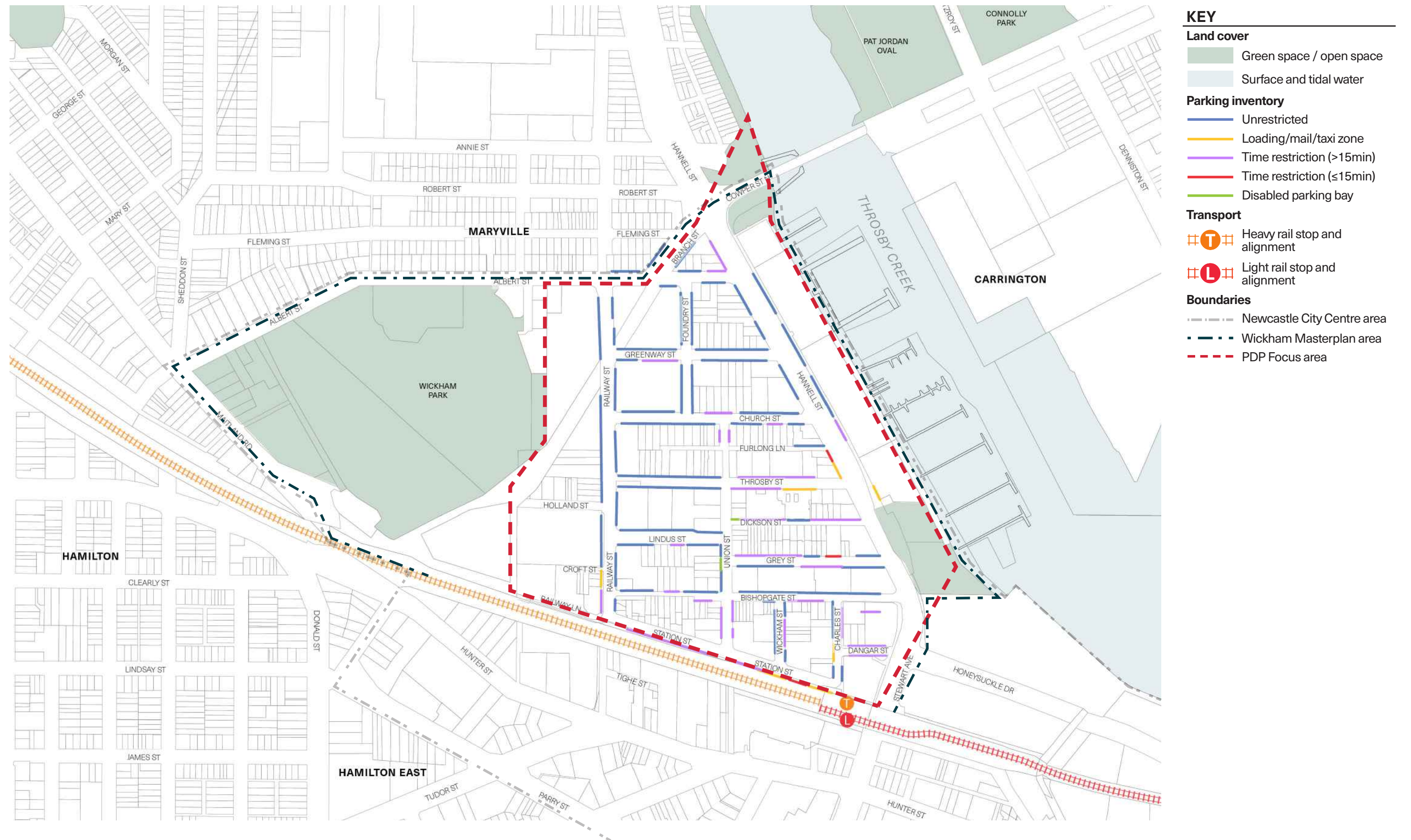
Appendix 2 – Precinct Analysis

6.5 Tree canopy cover



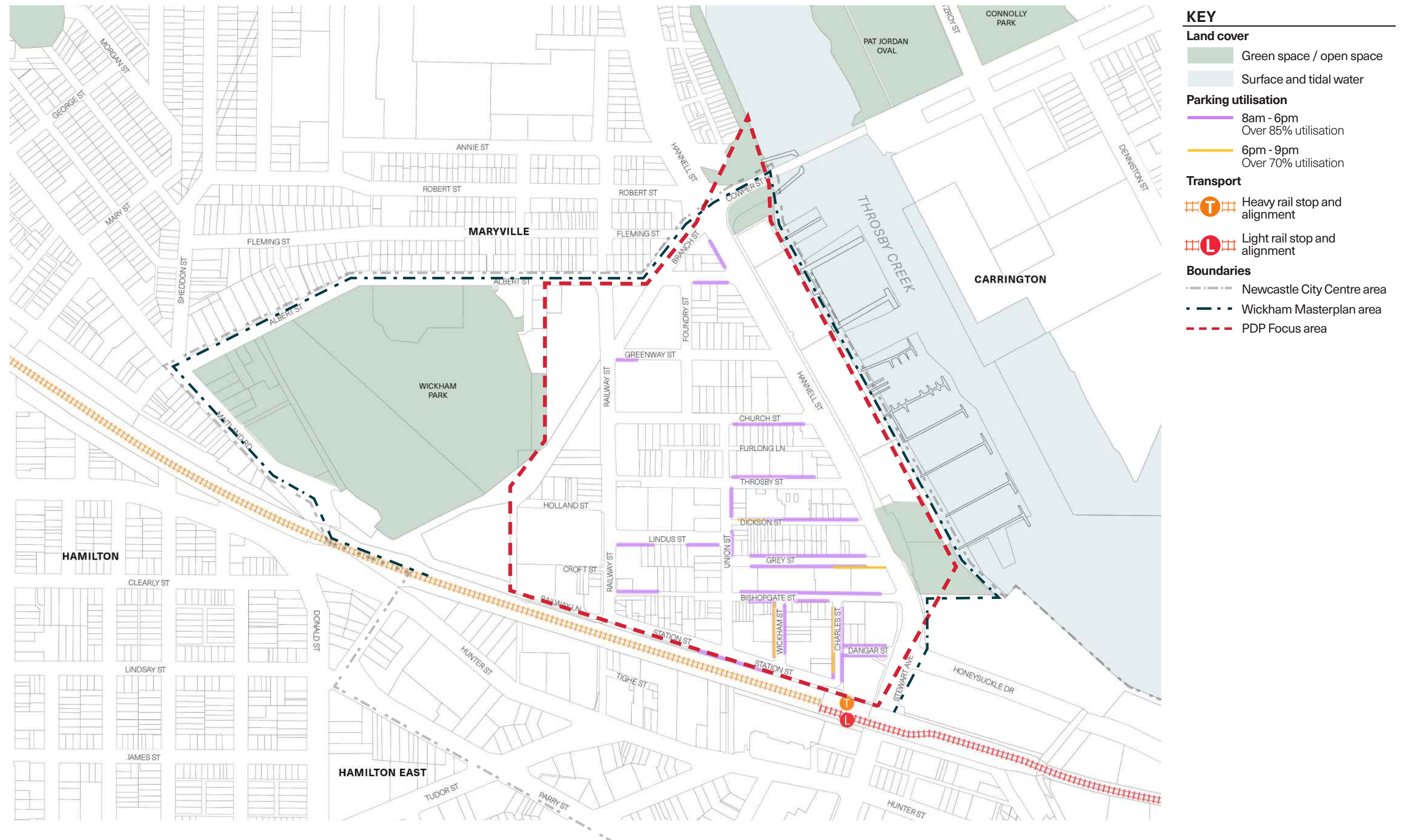
Appendix 2 – Precinct Analysis

6.6 Parking inventory



Appendix 2 – Precinct Analysis

6.7 Parking utilisation



6.8 Non-Aboriginal heritage summary

6.8.1 Historical context

Wickham’s development commenced in the 1840s, with small-scale agriculture and related industries. Residential subdivisions began in 1868 but, by the mid-1870s, coal mining companies had opened shafts at Wickham and on Bullock Island. In 1876, a railway branch line through Wickham to the wharves on Bullock Island was opened and Ritchie Bros completed a large engineering works the following year. By 1900, housing had evolved into clusters of working-class cottages, set amongst these industrial enterprises. A local council and public schools were established but, in the same decade, the collieries all closed and the Bullock Island Branch Line closed in 1908. By the 1920s, the large and heavy industries had relocated out of Wickham, leaving the area as a part light-industrial, part urban-fringe suburb of Newcastle. By the 1950s, Wickham was in decline but the 1980s saw a re-emerging interest in inner city areas and the waterfrontages were redeveloped for residential purposes in the 1990s. In the 21st century, Wickham is a mix of traditional workers housing, public housing, warehouses and small industries, with multi-storey residential buildings on the waterfront.

6.8.2 Heritage items

There are no places of State, National or World Heritage significance. There are twelve recognised Local heritage items within the Wickham Masterplan area (refer Table 1).

6.8.3 Archaeology

The Ritchie & Sons / Hudson Bros Engineering site is identified in the LEP as a potential archaeological site. Other potential archaeological sites include: The Ferndale Colliery; The Wickham/Linwood Colliery; the Water Pumping Station; and the Corroboree Ground.

6.8.4 Pavements

A key issue for the Wickham Masterplan Area is the presence of particular pavements decorated with what are known as ‘Rising Sun’ and ‘Pineapple’ patterns. Sandstone block kerbs and sandstone block gutters also survive within the Masterplan area.

6.8.5 Early twentieth century housing

Wickham’s surviving early twentieth century housing forms a particular character element with the Masterplan area. Most of these are not heritage listed but could be recognised as elements of a dispersed Heritage Conservation Area.

6.8.6 Aboriginal sites and places

There are no Aboriginal Places identified under the NSW Heritage Act located with the Wickham Masterplan Area. There are nine sites registered in the Aboriginal Heritage Information Management System (AHIMS). These are mostly located on the fringes of the railway easements and in the south-east corner of the Wickham Masterplan Area.



Figure 1 – Surviving 19th and early 20th century residential buildings in Wickham



Figures 2 & 3 – Brick footpath pavements (left) and Sandstone kerbs and gutters (right)



Figure 4 – Heritage Items within the Wickham Masterplan Area (blue boundary). General Heritage Items are coloured brown, Landscape items are coloured green and Archaeological sites are coloured yellow. Heritage Conservation Areas are shown with red hatching. (Source: NCC – Heritage Maps 004FA and 004G)

Name	Address	No.
Former Wickham Town Hall and Council Chambers	18A Albert Street	I674
Hawkins Oval	22 Albert Street	I675
Hawkins Oval Memorial	22 Albert Street	I676
Residence	15 Charles Street	I681
Wickham Public School	54 Hannell Street	I685
Former Infants School	64 Hannell Street	I686
Albion Hotel	72 Hannell Street	I687
The Missions to Seamen building	96 Hannell Street	I688
The Salvation Army Men’s Hostel	116–120 Hannell Street	I689
Lass O’Gowrie Hotel	14 Railway Street	I691
Former Wickham and Bullock Island Railway Corridor	Railway Easement	I706

Table 1

Appendix 2 – Precinct Analysis

6.9 Heritage features



Wickham in the late 19th century with Bullock Island Branch railway



'Tree of Knowledge' / Yum Yum Tree



Significant fig trees, Albert Street (adjacent Wickham Park)



Croatian Club, Albert Street



Former Wickham Public School, Hannell Street

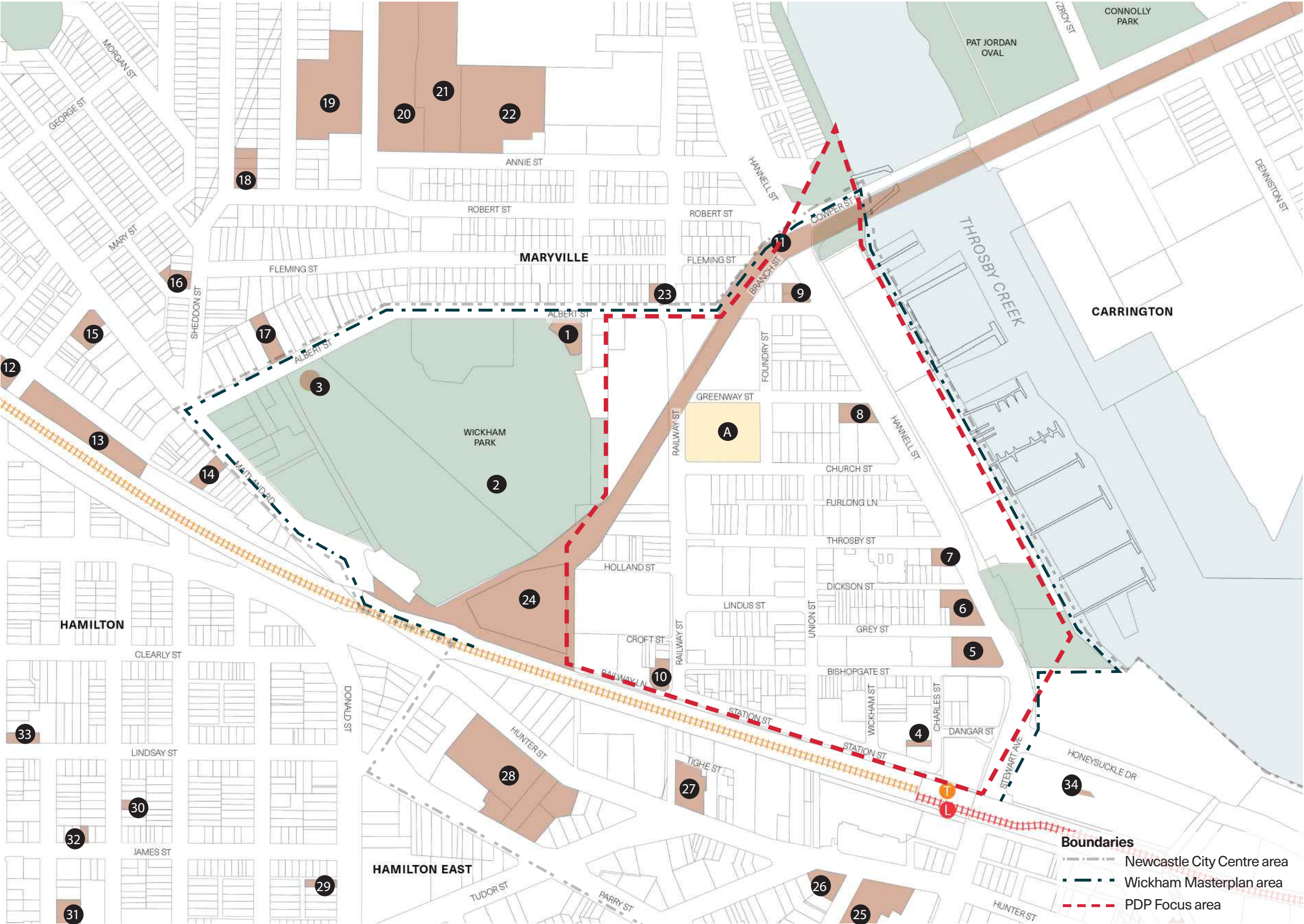


Former Infants School / Awabakal Community Centre, Hannell Street



"Rising Sun" brick pavers

6.10 Heritage



- KEY**
- Land cover**
- Green space / open space
 - Surface and tidal water
- Heritage**
- Local heritage site
 - Local archeological site
- Heritage sites**
- 1 R A Ritchie & Sons / Hudson Bros Engineering (fmr industrial site)
 - 2 Fmr Wickham Town Hall & Council Chambers
 - 3 Hawkins Oval
 - 4 Hawkins Oval Memorial
 - 5 Residence
 - 6 Wickham Public School
 - 7 Former Infants School
 - 8 Albion Hotel
 - 9 Missions to Seamen building
 - 10 Salvation Army Men's Hostel
 - 11 Lass O'Gowrie Hotel
 - 12 Fmr Wickham & Bullock Island Railway Corridor
 - 13 Hamilton Station Hotel
 - 14 Station Buildings & Signal Box
 - 15 Residence
 - 16 Fmr Regent Picture Theatre
 - 17 Kenilworth (Residence)
 - 18 Ventetia (Residence)
 - 19 St Mark's Anglican Church
 - 20 Winchombe Carson Warehouse
 - 21 Elder's Warehouse
 - 22 Dalgety Warehouse
 - 23 Fmrr NZ Loan Co Wool Store
 - 24 Fmr Police Lock-Up
 - 25 Fmr Wickham & Bullock Island Railway Corridor
 - 26 Fmr Castlemaine Brewery
 - 27 Former School of Arts
 - 28 Dairy Farmer's Building
 - 29 St Josephs Convent & Sacred Heart Church & School
 - 30 Anona (residence incl front fence)
 - 31 Terrace (incl Iron Fence & Gate)
 - 32 Scots Kirk Presbyterian Church
 - 33 Fmr Hamilton Volunteer Fire Station
 - 34 Hamilton Post Office
 - 35 Former School of Arts
- Boundaries**
- Newcastle City Centre area
 - Wickham Masterplan area
 - PDP Focus area

Appendix 2 – Precinct Analysis

6.11 Built form



View from Honeysuckle to Wickham



Original timber housing



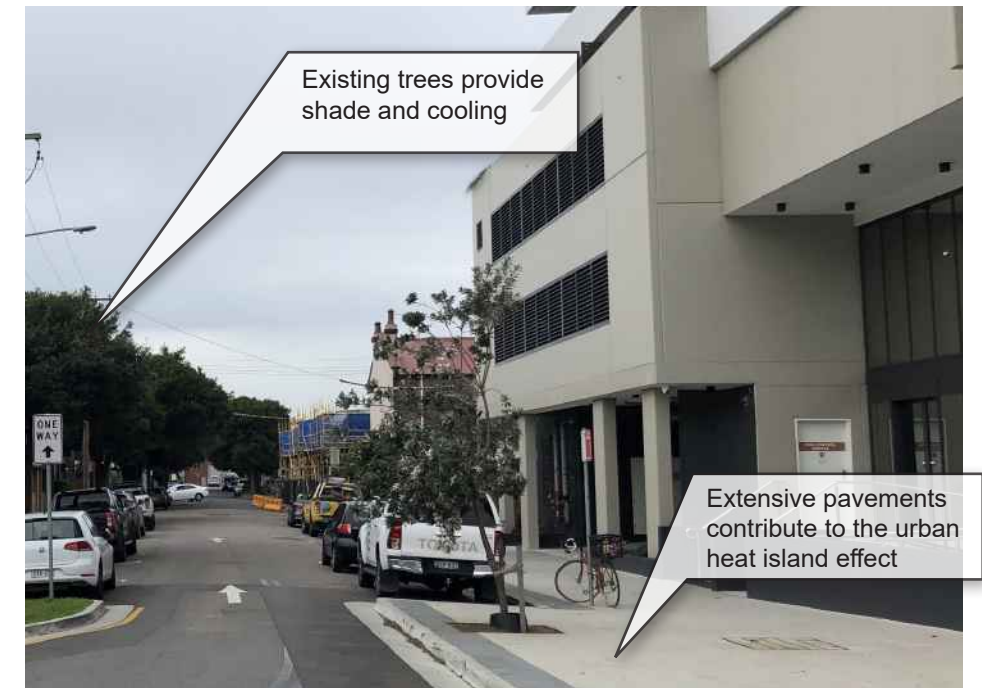
Adaptive reuse of industrial buildings



New housing on Throsby Street / Union Street



Station Street - east



Street interface to Station Street (adjacent Newcastle Interchange)

Extensive pavements contribute to the urban heat island effect

Appendix 2 – Precinct Analysis

6.12 Land ownership



KEY

Land cover

- Green space / open space
- Surface and tidal water

Land Ownership

- Newcastle City Council
- Rail Corporation NSW
- State of NSW
- Private Ownership

Transport

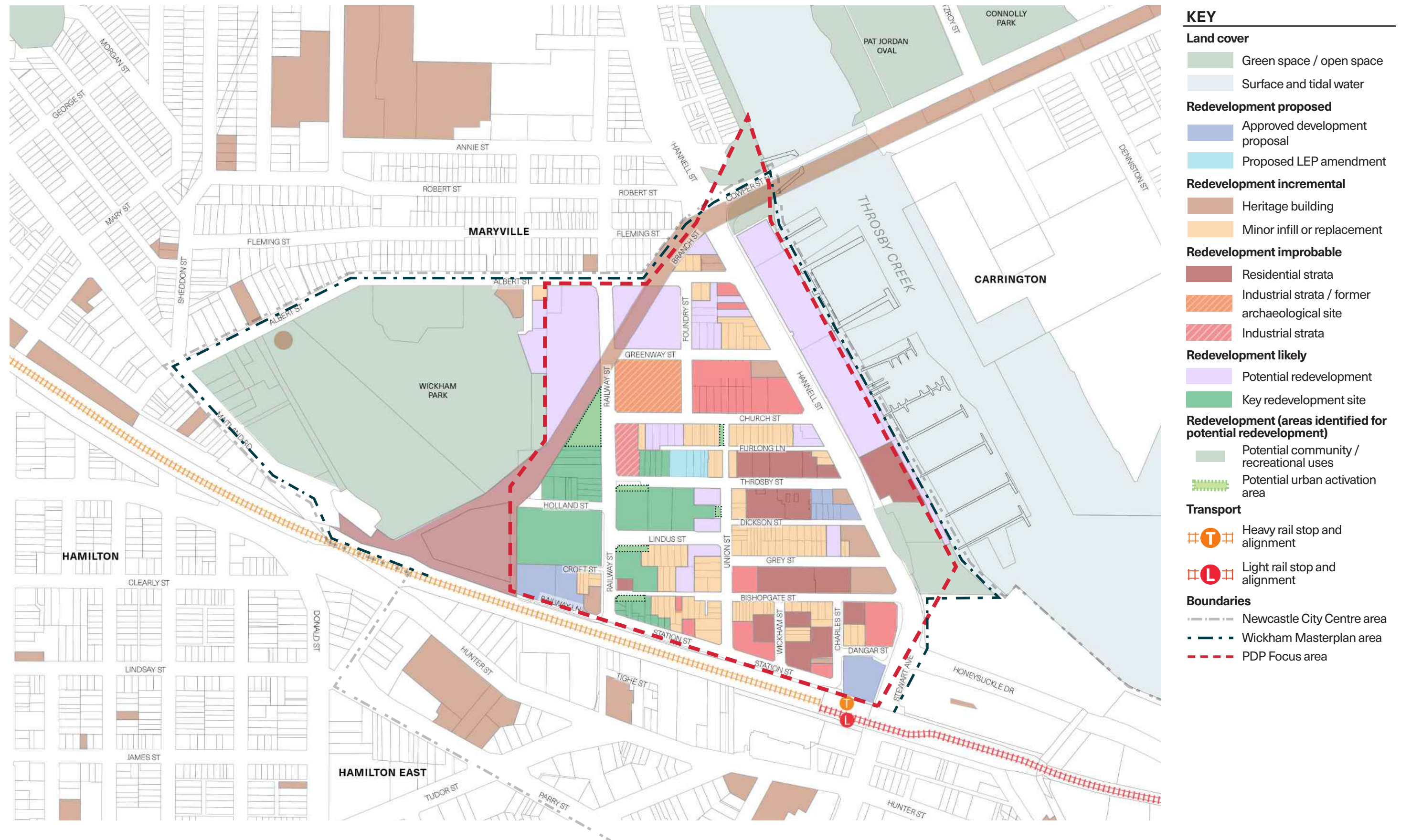
- Heavy rail stop and alignment
- Light rail stop and alignment

Boundaries

- Newcastle City Centre area
- Wickham Masterplan area
- PDP Focus area

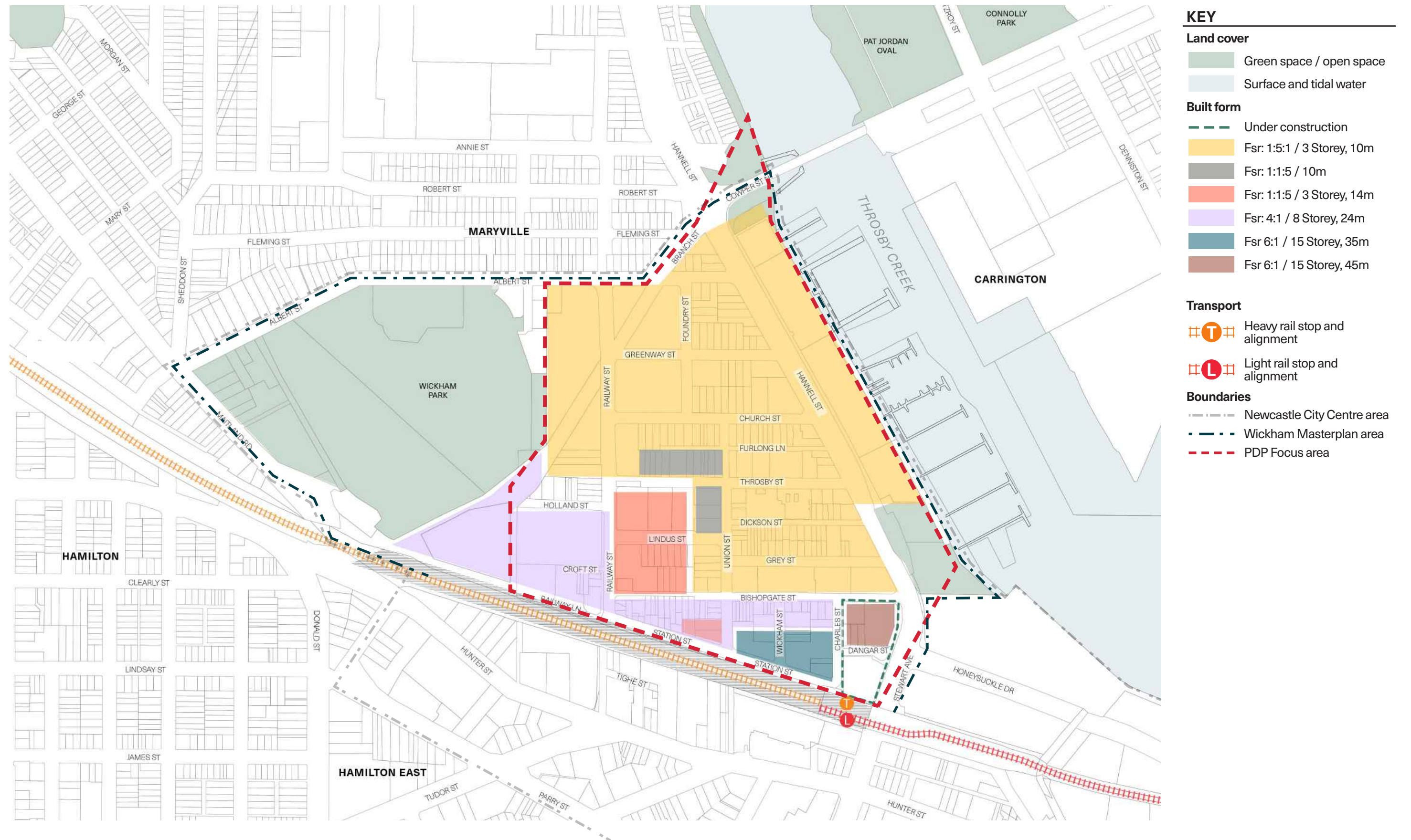
Appendix 2 – Precinct Analysis

6.13 Potential change of land use



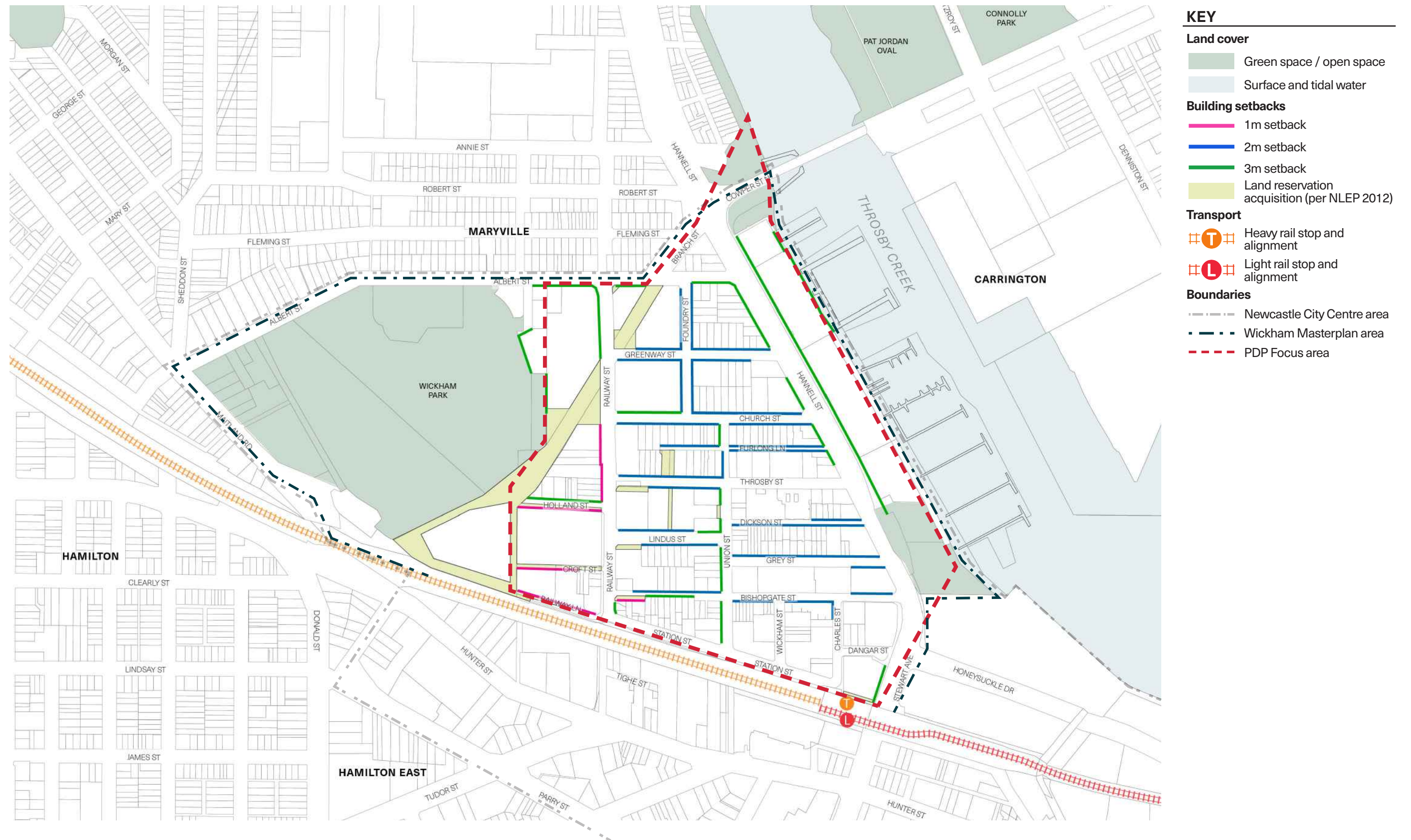
Appendix 2 – Precinct Analysis

6.14 Built form scale



Appendix 2 – Precinct Analysis

6.15 Minimum building setbacks



Appendix 2 – Precinct Analysis

6.16 Activity nodes and attractors: public art

Increase engagement with local Aboriginal community

Enhance Newcastle's public space through public art



Public art murals

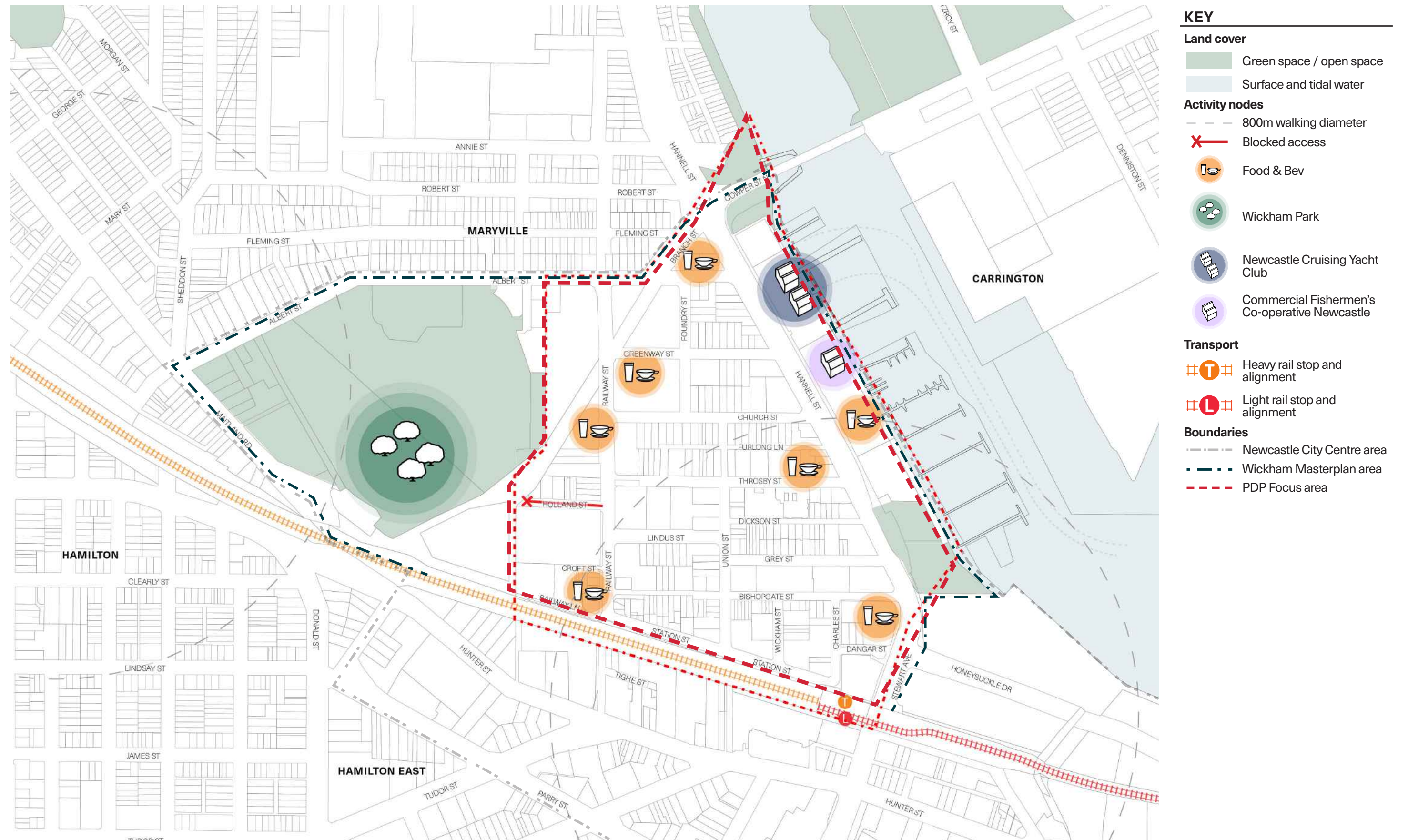
Street artwork

Public art

Celebrate the city and contribute to its identity

Appendix 2 – Precinct Analysis

6.17 Activity nodes and attractors: wider context

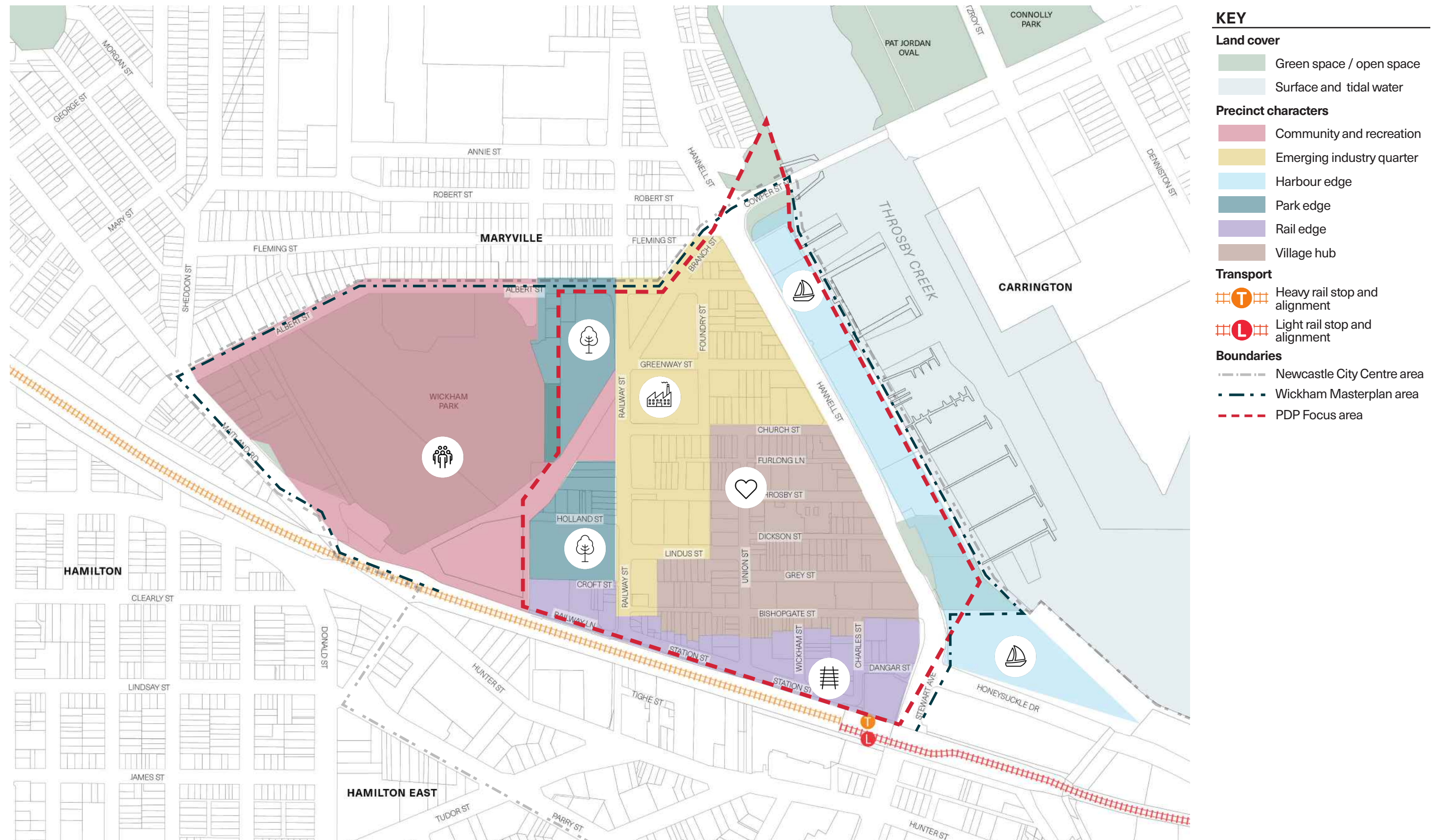


Opportunities and Constraints



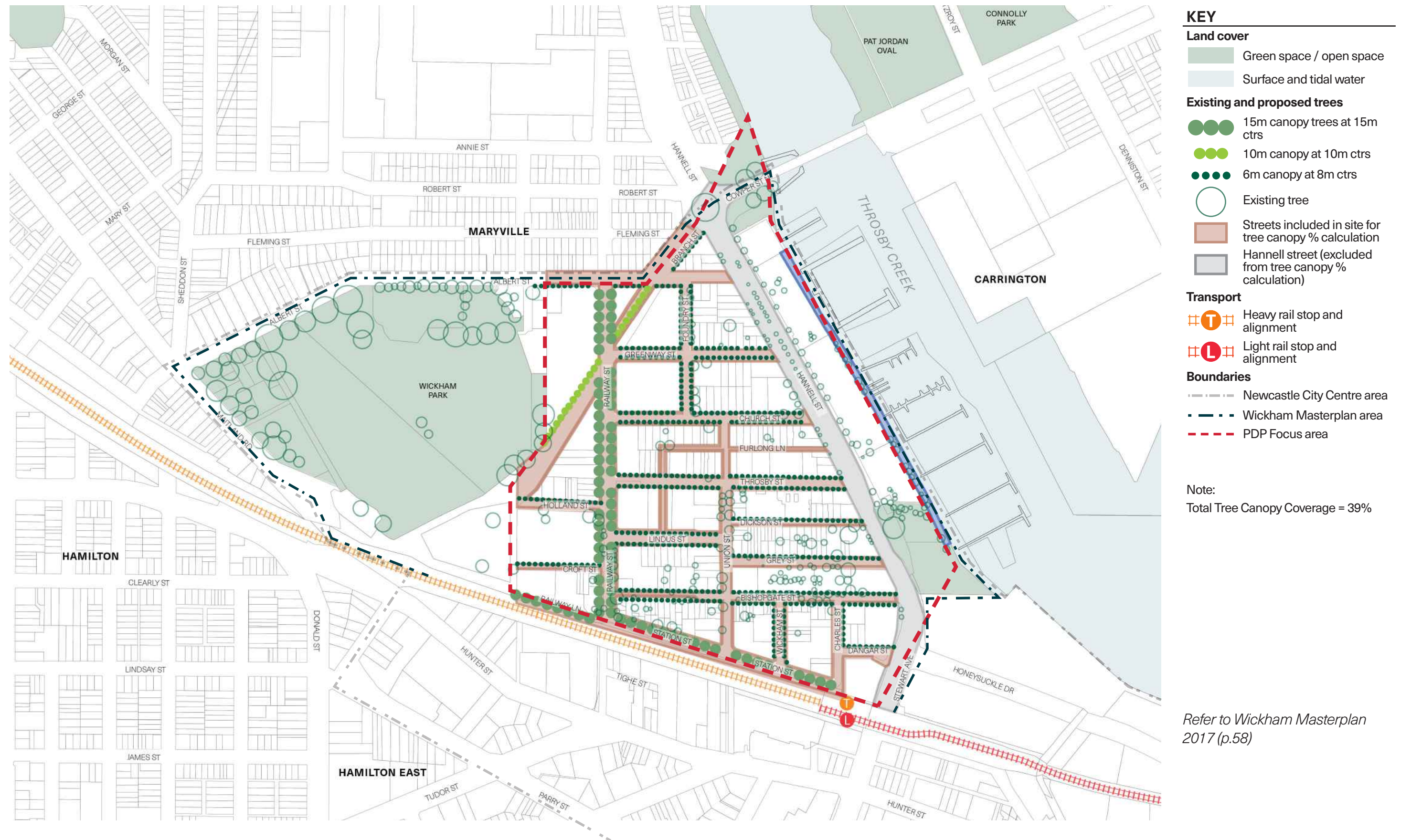
Appendix 3 – Opportunities and Constraints

7.1 Precinct character

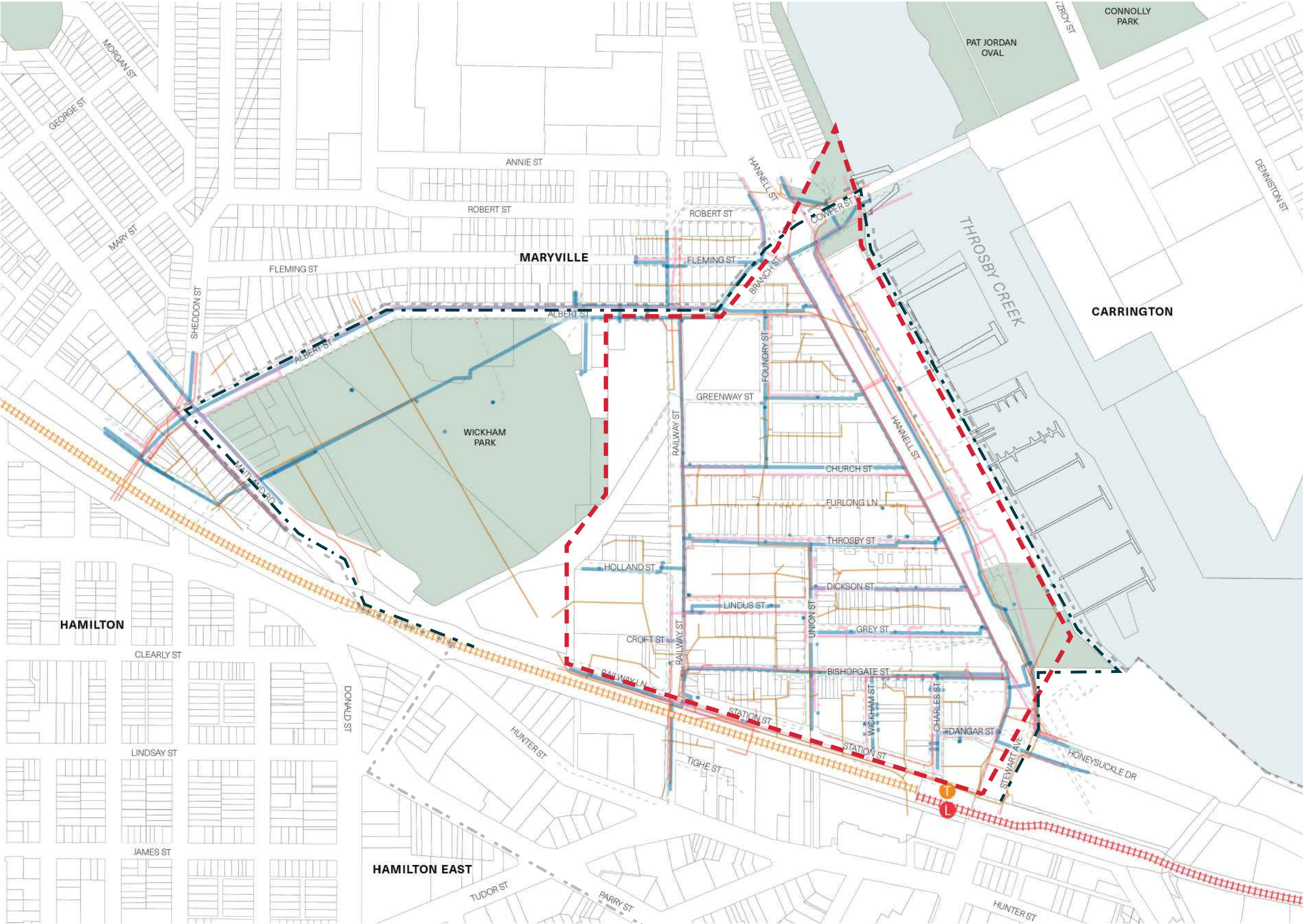


Appendix 3 – Opportunities and Constraints

7.2 Open space and tree canopy



7.3 Existing utilities



KEY

Land cover

- Green space / open space
- Surface and tidal water

Services

- Telecom
- Gas
- Electrical
- Water
- Sewer

Transport

- Heavy rail stop and alignment
- Light rail stop and alignment

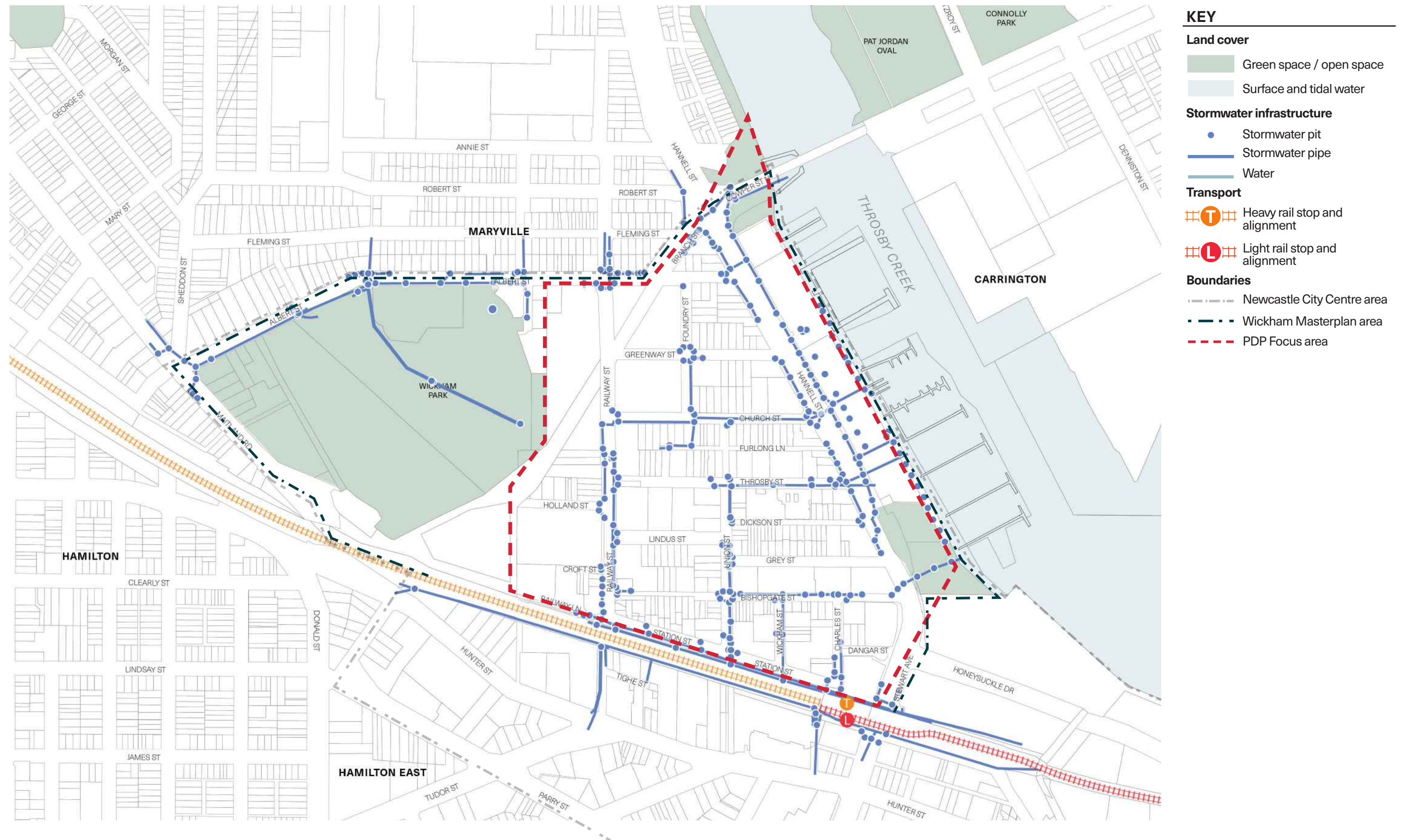
Boundaries

- Newcastle City Centre area
- Wickham Masterplan area
- PDP focus area

- Utility Opportunities & Challenges**
- Opportunity to underground existing Ausgrid overhead powerlines
 - Opportunity to extend telecommunications cabling to any future smart-poles, noting a roll-out is not currently planned within Wickham
 - Opportunity for future Electric Vehicle charges in Wickham Park
 - Congested utilities in footpaths are significant constraint to additional tree planting and may have to be managed through build-outs
 - Significant sewer assets that service the majority of the site run down Union & Station Street and will have to be managed

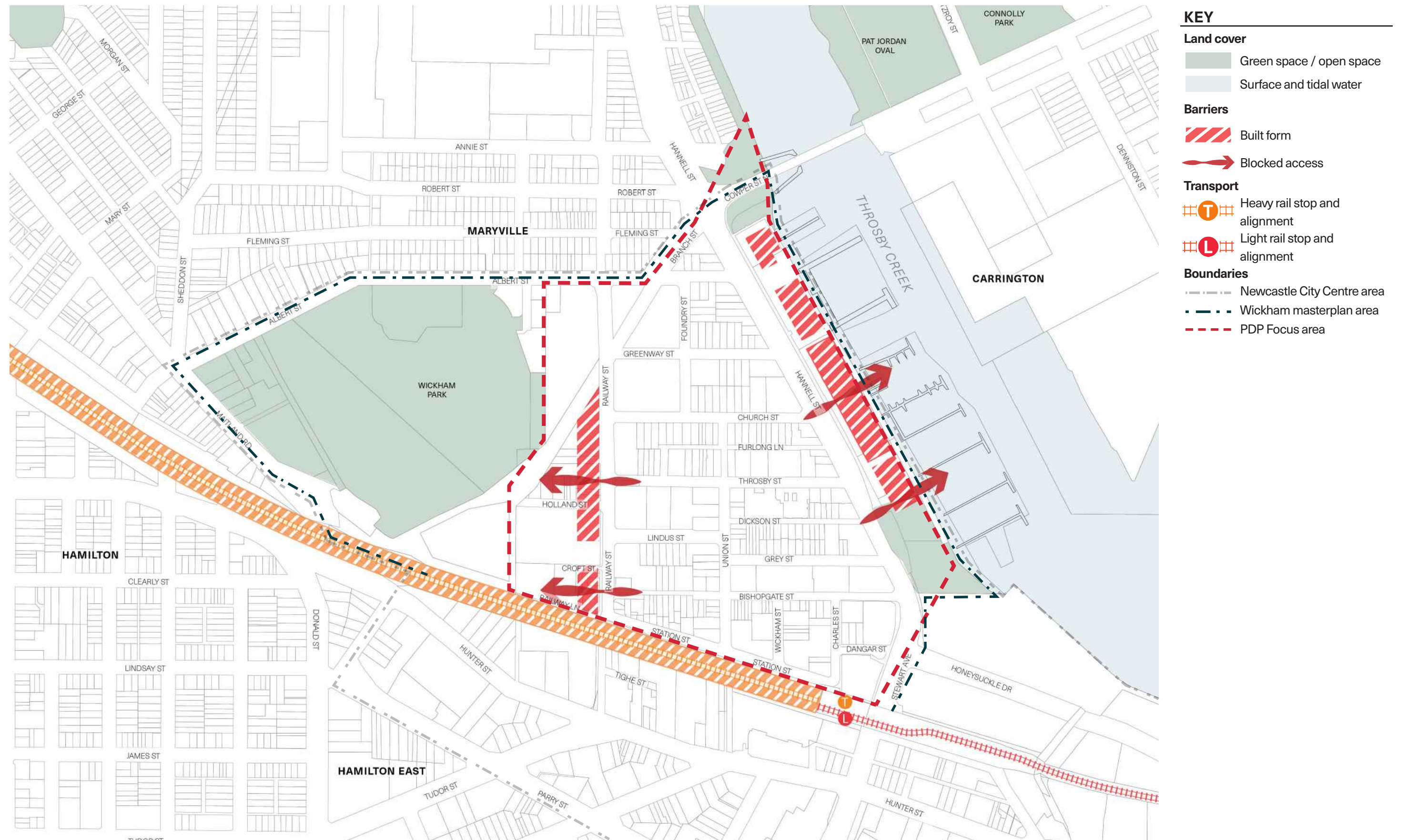
Appendix 3 – Opportunities and Constraints

7.4 Stormwater management

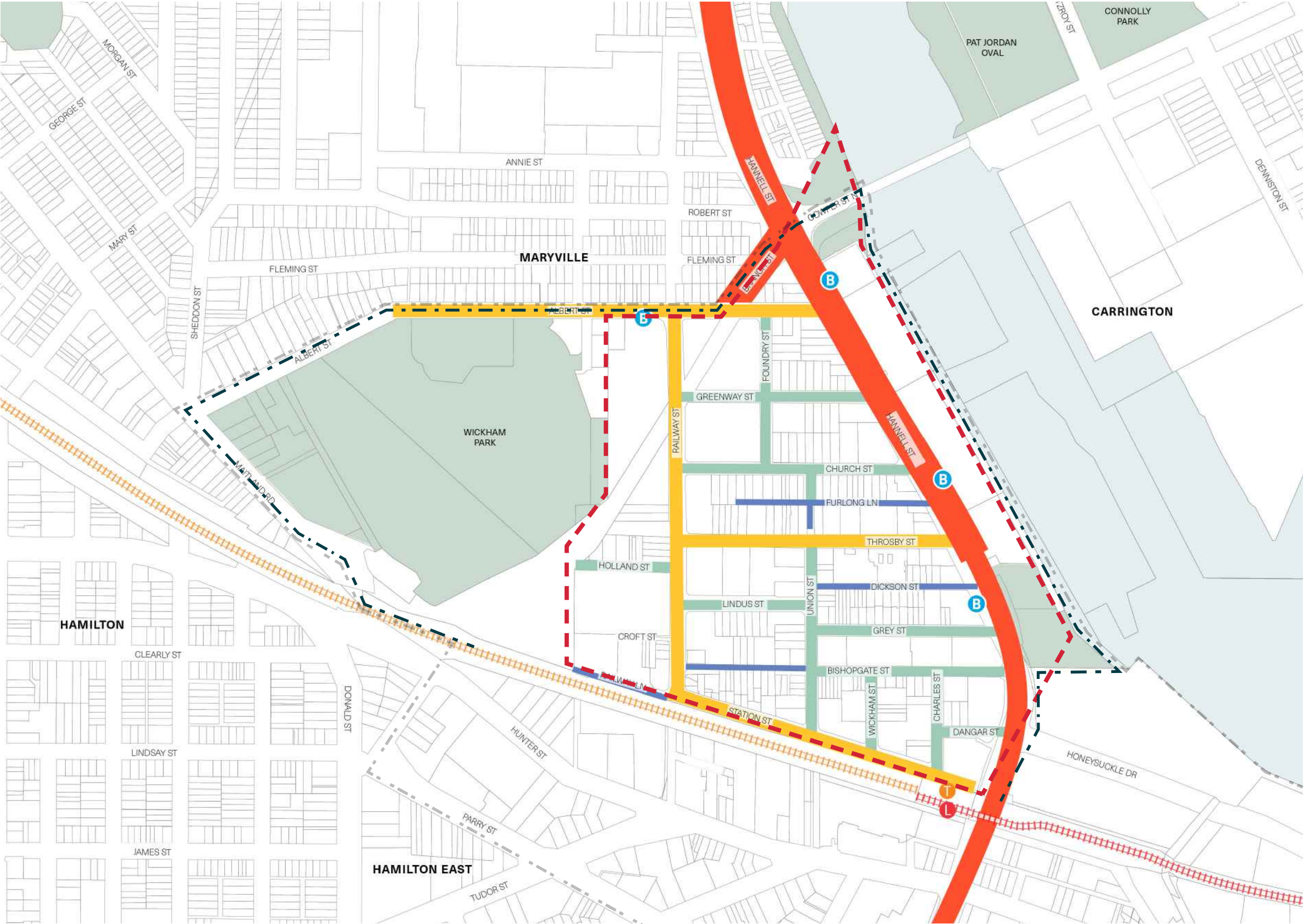


Appendix 3 – Opportunities and Constraints

7.5 Barriers



7.6 Existing road reserve widths and typology



KEY

Land cover

- Green space / open space
- Surface and tidal water

Road typology

- Arterial road (30m+)*
- Collector road (20-30m)
- Local street (10-15m)
- Laneway (6-10m)

Transport

- Heavy rail stop and alignment
- Light rail stop and alignment

Boundaries

- Newcastle City Centre area
- Wickham Masterplan area
- PDP Focus area

• Note that, while an arterial road, Hannell Street-Stewart Avenue narrows south of Throsby Street

Appendix 4

Background documents



Summary of Background Documents

Document	Date/Author	Key findings
Wickham Masterplan (2017)	NCC	<p><i>Vision for Wickham...</i></p> <p>Wickham Urban Village – history</p> <ul style="list-style-type: none">• First subdivided in mid-1800s• By the 1900s it became Newcastle’s popular suburb for workers; current urban structure reflects original pattern of development• 2012 – NSW Government introduced Newcastle Urban Renewal Strategy; Wickham Masterplan was prepared in response to NURS• 2014 – NURS updated and advocates a strategic shift of the commercial core of the Newcastle City Centre from Newcastle East to Newcastle West• Changing demographic with increased demand for inner city residential apartments• Potential uses within Wickham to support technology sector (e.g. shared work spaces, ‘makerspaces’ etc)• Important that redevelopment expectations be managed through density controls to avoid over inflation of unimproved land values• Challenges to address:<ul style="list-style-type: none">— Accessibility and connectivity— Increased demand for car parking— Maintaining land for employment— Safety and amenity— Physical limitations particularly flooding• Opportunities to build upon:<ul style="list-style-type: none">— Location – adjacent to new central business district of Newcastle City centre— Wickham Park – support recreation and social needs of community— Physical attributes – supports walking and cycling, proximity to waterfront and park— Eclectic urban character – varied building types, scale and styles; vibrant examples of community place making and public art; historical remnants; mix of land uses• Vision:<ul style="list-style-type: none">— Railway edge – interface to the emerging commercial core of Newcastle; mixed use development; adjacent interchange and development capacity— Village hub – incorporates redicential subdivision patters established in mid 1800s; fine-grain character and lower scale built form; prioritises pedestrian amenity and safety; Union Street and main pedestrian spine; retail and commercial; and community activity— Harbour Edge – prime waterfront location; maintain three storey scale; support entertainment and tourism activity; pedestrian and cycle links between Newcastle Interchange and foreshore promenade; potential additional ferry stop would provide greater connectivity for Wickham and the city and surrounding suburbs—Emerging industry quarter – larger development sites and wider streets to support larger scale development; support growth in smart technologies, research and development; support ‘smart city’ initiatives; former Bullock Island corridor as open space— Park edge precinct – residential apartment development and clusters of commercial and retail; provides key east-west connections to Wickham Park— Wickham Park – social and recreation needs of local residents and wider Newcastle community; playing fields, playground, community gardens, market and event space, community facilities and amenities (Note – outside of WPDP project site area)• Implementation<ul style="list-style-type: none">— Provide connected and accessible pedestrian and cycling routes through the Wickham Master Plan area— Manage traffic within the Wickham Master Plan area to improve amenity and safety within local streets— Manage car parking demands generated both internal and external to the Wickham Master Plan area— Eclectic urban character – varied building types, scale and styles; vibrant examples of community place making and public art; historical remnants; mix of land uses

Appendix 4 – Background documents

Document	Date/Author	Key findings
Wickham Masterplan (2017)	NCC	<div>— Create safe, attractive and inclusive public spaces:<ul style="list-style-type: none">- Improve streetscapes to meet future capacities and current community expectations- Implement opportunities for activation of the public realm- Acquire land required for implementing the Wickham Master Plan</div> <div>— Ensure built environment is functional, responsive and resilient:<ul style="list-style-type: none">- Redevelopment densities reflect the desired future character of the WMP area and provide opportunities to deliver community benefits- Built form reflects the desired future character of the WMP area</div>
Wickham Masterplan (2021 Update)	NCC	<p>Update reflects construction of Newcastle Interchange, Honeysuckle redevelopment, and key sites in proximity to the site</p> <p>Challenges to realising the long-term Vision for Wickham:</p> <div><ul style="list-style-type: none">• Development controls:<ul style="list-style-type: none">— Street setbacks— Lack of activation along primary street frontages— Not delivering the identified public domain areas— Lack of variety of size or intent of ground floor spaces within larger development sites— Driveway access diminishing pedestrian safety and amenity— Lack of lot amalgamation resulting in residual parcels• Mine subsidence• Opportunities to realising the long-term Vision for Wickham:<ul style="list-style-type: none">- Redevelopment potential – includes proposed, improbable, incremental, likely redevelopment; consider potential• Key areas of interest by stakeholders during engagement:<ul style="list-style-type: none">—Access and parking around Wickham Park—Additional housing types within the area other than apartments—Expansion of the Village Hub—Footpaths, cycleways and shared paths— Mine subsidence risk—Traffic movements and carparking• Wickham 2040<ul style="list-style-type: none">— “Wickham will continue to transform into a dynamic urban neighbourhood that supports a diverse mix of uses, which complement the adjoining commercial core of the Newcastle City Centre located within Newcastle West.— Urban renewal within the area is envisaged to build on the existing urban structure to deliver greater connectivity, improved public domain amenity, and a built form reflecting the envisaged function and character.”</div>
Newcastle 2040 Community Strategic Plan	NCC	<p>Vision...</p> <p>“We will celebrate our cultural heritage and history, protect our natural environment and support our people to thrive and prosper. As an inclusive community, we will strive for equal rights and access for all. We will face challenges with integrity, innovation and creativity. Support business growth, education and employment opportunities. We will be a leading lifestyle city with vibrant public places and spaces, connected transport networks and unique urbanism. This will be achieved within a framework of open and collaborative leadership.”</p>

Appendix 4 – Background documents

Document	Date/Author	Key findings
Newcastle 2040 Community Strategic Plan	NCC	<p><i>Vision...</i></p> <p>2016 – 150,919 people 2036 - population forecast 195,530 Newcastle welcomes 4.1 million visitors each year</p> <p>The population of the LGA will increase and we will play a stronger role as the capital of the region as it's population grows to 1 million in 2036.</p> <ul style="list-style-type: none">Guiding Principles:<ul style="list-style-type: none">— Recognise diverse local community needs and interests— Consider social justice principles of equity, access, participation and rights— Consider long term and cumulative effects of actions on future generations— Consider principles of ecologically sustainable developmentAlign with International Sustainability Goals:<ul style="list-style-type: none">— Integrated and accessible transport— Protected environment— Vibrant, safe and active public places— Inclusive community— Liveable built environment— Smart and innovative— Open and collaborative leadership
On Our Bikes Cycling Plan (2021)		<p>On our bikes cycling plan aligns with the Newcastle Transport Strategy and overarching Newcastle 2030 Community Strategic Plan, and NSW Government's Greate Newcastle future Transport Plan }</p> <p>— Adopts principles of the Movement and Place framework that aims to achieve 'efficient investment, by thinking about the built environment holistically including the social, environmental and economic context'. — Safe System approach – aims to eliminate fatal and serous injury on the road through a holistic view of the road transport system and intractions between elements (users, vehicles, travel speeds) — Reducing speed limits – and aim to separate pedestrians and cyclists when speeds are above 30km/hr</p> <p>Goal: By 2030, City of Newcastle will have a safe, connected cycling network of principal routes and low stress streets. Riding and walking will be the natural choices for short trips, for all members of our community.</p> <p>— Improve safety and comfort — Connect and improve the network — Support people to ride — Facilitate active transport in centres</p> <p>Travel patterns and cycling</p> <p>— approx. 78% of all weekday trips are by private vehicles (average < 8km) — 1.9% of residents ride to work</p>

Appendix 4 – Background documents

Document	Date/Author	Key findings
Cultural Strateg (2016-2019)		<p>The Cultural Strategy 2016-2019 addresses Council's role and participation with the community in supporting and developing a thriving creative culture</p> <p>The Strategy is a four year framework detailing the priorities for Council's investment in arts and culture in Newcastle. It outlines a 10 year vision and 17 key strategies and actions. The following strategies are relevant to the Wickham Public Domain Plan:</p> <ul style="list-style-type: none"> — Strategy 2: Increase connection between Council's cultural institutions for the benefit of moving and building audiences — Strategy 3: Diversify public programs to support the development of new audiences and new programming experiences — Strategy 5: Increase focus on young people (16-30) — Strategy 6: Expose local stories, both historic and contemporary, through cultural programming and build Newcastle's cultural identity — Strategy 7: Grow the city's identity via its collections of art and artefacts, local history and architecture — Strategy 8: Enhance Newcastle's public space through public art — Strategy 9: Ensure Newcastle audiences have access to a diverse range of exhibitions and works of high quality — Strategy 12: Increase engagement with local Aboriginal community in consultation with Council's Guraki Committee — Strategy 13: Build cultural tourism by presenting events that celebrate the city and contribute to it identity — Strategy 14: Increase outdoor programming and programming in non-traditional spaces — Strategy 15: Advocate to Urban Growth for the inclusion of an arts space in their development plans for the city. — Strategy 16: Increase support for, and engagement with, local artists, innovative thinkers, academic creatives and cultural practitioners
City Centre Public Domain Technical Manual (2014)		<p>The City Centre Public Domain Technical Manual sets guidelines, design coordination and material palettes for all public domain, streetscape and maintenance works in the city centre, and provides a guide for designing streetscapes within Wickham Public Domain Plan.</p>
Smart City Strategy (2017-2021)		<p>Newcastle is the second largest non-capital urban centre, and the second oldest city in Australia. The Newcastle Local Government Area (LGA) is home to 152,948 people (2016 census) and welcomes about 4.1 million visitors each year.</p> <p>Newcastle LGA is expected to continue to grow with an additional 32,000 people anticipated by 2036.</p> <p>Newcastle is the cultural and economic capital of the Hunter - Australia's largest regional economy contributing around 8 percent of the economic activity in NSW, with a regional output of approximately \$44.5 billion in 2017.</p> <p>The Strategy expands on Newcastle 2030 and provides a roadmap for delivering Smart City initiatives. These include a series of strategies under these key areas:</p> <ul style="list-style-type: none"> — Smart Mobility — Smart Governance — Smart Living — Smart Environment — Smart People — Smart Economy

Appendix 4 – Background documents

Document	Date/Author	Key findings
Parkland and Recreation Strategy (2014)		<p>The Parkland and Recreation Strategy guides sustainable provision of parkland and recreation facilities for current and future communities.</p> <p>Key initiatives to address the identified challenges and gaps include:</p> <ul style="list-style-type: none">• Planning and development of facilities – quality parks, active and passive infrastructure, improved connections to other open space opportunities, upgrade and adaption of existing recreational facilities, adoption of new technologies to increase capacity and opportunities for commercial use and revenue streams, continued provision of pathways, bicycle lanes, fitness equipment, sport fields, bicycle paths to cater for population changes and community demand• Management of facilities – opportunities to effectively manage facilities, investigate opportunities for greater commercial use, activities and revenue streams• Partnership development – formal partnerships with NSW Government, inclusion of key stakeholders in the process, strategic partnerships and financial investment with sporting associations, clubs and key stakeholders• Promotion of facilities and opportunities – promotion and marketing strategies, continued support and attraction of state, national and international events to the city of Newcastle <p>Challenges</p> <ul style="list-style-type: none">• Financial sustainability• Sustainable facility management and community organisations• Managing community expectations• Biodiversity protection <p>Opportunities</p> <p>Partnerships with education sector</p> <ul style="list-style-type: none">• Improved capacity of facilities• Reduction of service levels• Disposal o excess parkland• Leasing of parkland and recreation facilities• Increase awareness of facilities• Sponsorship• Community trusts

Wickham Public Domain Plan



City of
Newcastle

In February 2023, City of Newcastle (CN) invited the community to have their say on the Wickham Public Domain Plan which provides a framework for the delivery of high-quality streetscapes and public spaces in Wickham.

Key features of the Plan include:

Upgrading Union Street to be an attractive pathway to the heart of the precinct

Improved footpath conditions

Creation of new community spaces with active transport links

New laneways and connecting roads

New tree plantings and landscaping throughout the precinct



Engagement activities

CN sought feedback on the draft concepts for the Public Domain Plan (PDP) from 22 February to 22 March 2023.

The community were able to provide feedback by:

- completing the online survey
- providing a written submission via the online form
- calling or emailing us
- attending a drop-in session and speaking to us in person.

Communication & promotion



Have Your Say
webpage

1,153 visits



Social media

7,471 people
reached



Flyers and on-
site signage

1,200 flyers and
6 corflutes



e-newsletter

nearly 2,500
reached



Media release



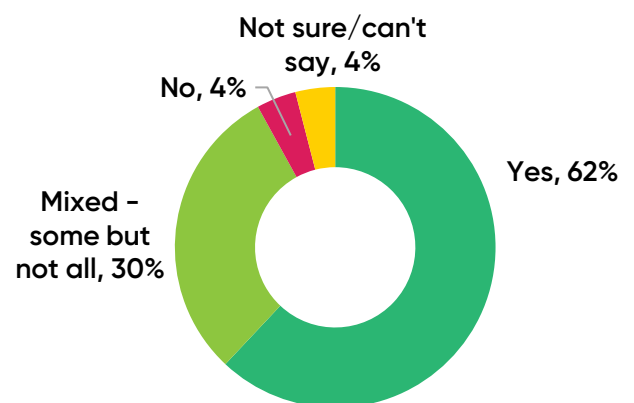
What we heard

73 people responded to the survey.

The overarching design elements of the PDP, and changes to vehicle movement, the pedestrian network and cycleway network were generally well supported.

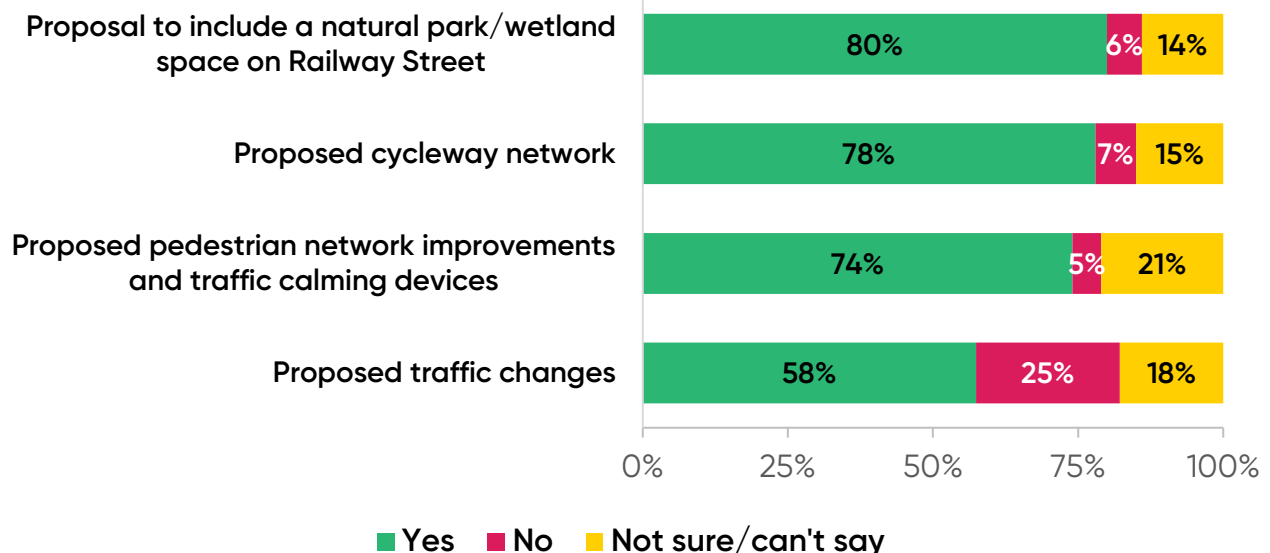
Positive feedback was received on the increase of trees and greenery as well as additional spaces for recreation.

Agreement with the overarching design elements



Sample size: N=73

Support for specific elements



Sample size: N=73

Overarching design elements

Respondents were also asked to provide reasons to explain their level of support towards the overarching design elements. The responses were categorised into recurring themes, with the main themes being shown below.

Main reasons for supporting the overarching design elements (N=25)	Mentions
General support/positive comments	12
Trees/plantings/greenery	10
Pedestrian and cyclist movement	8
Revitalisation of the area/growth area	6
Design/aesthetics/overall look and feel	5
Open spaces	5

Main reasons for having mixed support (N=20)	Mentions
Pedestrian and cyclist movement	12
Trees/plantings/greenery	10
Traffic movement	8
Open spaces	6
Concerns about parking/loss of parking	5
Open spaces	5

Only 5 people did not support the overarching design elements or were unsure, and provided a comment.



Vehicle movement framework

58% of respondents supported the proposed traffic changes or vehicle movement framework, with 25% saying they do not support the proposed changes.



The main themes identified are shown below.

Comments about proposed traffic changes (N=36)	Mentions
General support/positive comments	5
No need for traffic lights at roundabout	5
Furlong Lane works well as a two-way street	4
Prefer to leave Bishopsgate Street as is	3
Concerns about Furlong Lane exiting onto Hannell Street	2

Pedestrian network improvements & traffic calming

74% of respondents supported the proposed pedestrian network improvements and traffic calming devices, with only 5% not supporting the proposed changes.



The main themes identified are shown below.

Comments about proposed pedestrian changes (N=31)	Mentions
General support/positive comments	8
Support traffic calming devices	6
Support accessibility improvements	3
Need to incorporate disability standards	2
Footpaths/pedestrian access down Bishopsgate Street needs improving	2

Proposed cycleway network

78% of respondents supported the proposed cycleway network, with only 7% not supporting the proposed changes.



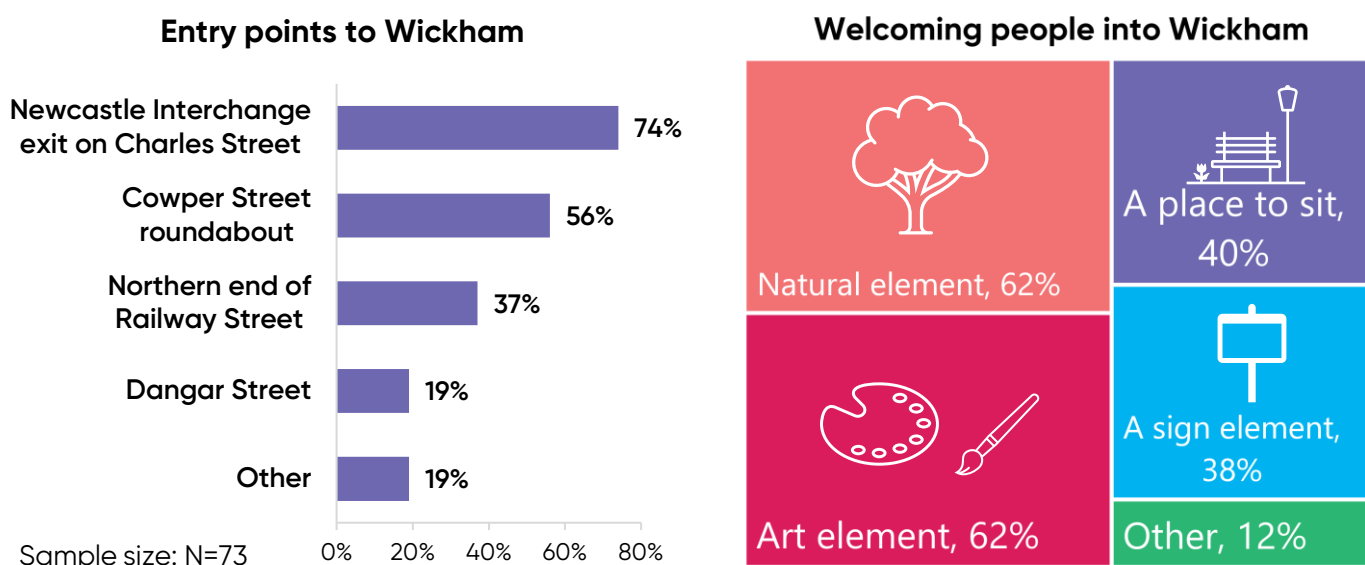
The main themes identified are shown below.

Comments about proposed pedestrian changes (N=30)	Mentions
General support/positive comments	10
Need a safer connection at Hannell Street to connect to main cycleway	3
More consideration for cyclists on smaller roads/connection to cycleways	2
Concerns about loss of parking	2
Include wide footpaths/shared paths where possible rather than on-road	2

Entry points to Wickham

74% of respondents suggested that the Newcastle Interchange exit on Charles Street was a key entry point to Wickham. Other common responses included the Cowper Street roundabout, the northern end of Railway Street and Dangar Street.

The two most popular options for welcoming people into Wickham were a natural element or an art element, followed by a place to sit and a sign.



'Other' suggestions with 2 or more mentions included Throsby Street, Bishopgate Street, Albert Street and Grey Street.

Ideas for urban activation areas

Sample size: N=40

Respondents were asked to provide input into appropriate amenities for each of the new urban activation areas identified in the PDP.

These open spaces/activation zones are:

- Corner of Throsby Street and Railway Street
- Corner of Lindus Street and Railway Street
- Union Street between Dickson and Throsby Streets
- Bullock Island Rail Corridor

Below is a comparison of the top themes across each activation area. For each column, the colours are used to show how each theme ranked within the top 5 categories.

Overall, there were similar mentions across all four zones such as trees/shade/greenery, park/grass area or picnic area and seating.

Suggestions for amenities	Cnr Throsby and Railway	Cnr Lindus and Railway	Union Street	Bullock Island Rail Corridor
Park/grass area/picnic area				
Trees/shade/greenery				
Seating				
Community garden				
Playspace				

Stories, landmarks, art and other elements to be celebrated

Respondents were asked an open-ended question about the stories, landmarks, art, objects or other elements that should be celebrated about Wickham. Responses were categorised to identify recurring themes. The most frequently mentioned themes were:



The history of
Wickham



Aboriginal heritage, culture
and sites of significance
(incl. the Tree of Knowledge)



Heritage
buildings



Local art



Natural
landscape

Sample size: N=35

General comments/feedback

At the end of the survey, respondents were able to provide open-ended comments allowing them to reflect on what was most important to them. The most common themes are shown below.

General comments/feedback (N=38)	Mentions
Pedestrian safety/accessibility	10
General positive comments	9
Cyclist safety/accessibility	9
Recreation and open spaces	6
Trees/plantings/greenery	5
Parking	5
Cleanliness and beautification	4

Sample size: N=38

Other submissions

There were a total of 13 other written submissions (12 via the online form and 1 via email). The submissions were categorised to identify the most common themes which are shown below.



Pedestrian safety/
accessibility (6)



Vehicle
movement (4)



Trees/plantings (4)



Lighting (4)



Parking (4)

*Numbers in brackets represent the number of mentions

Drop-in sessions

Two drop-in sessions were held during the public exhibition to inform community members about the PDP, provide an opportunity to speak to the project team and encourage feedback to be provided via the Have Your Say webpage.

The drop-in sessions were held as follows:

- **Thursday 2 March** from 11am to 1pm – Marketown Shopping Centre, Newcastle West
- **Saturday 18 March** from 11am to 1pm – Tree of Knowledge Park, Wickham

The main questions or feedback that we heard were in relation to:

Scope/key concepts of the PDP

Pedestrian network and traffic calming devices

Cycleway network

Traffic/vehicle movement

Parking

Trees/plantings/greenery

Open spaces/shared spaces

Heritage and character of Wickham

Development

Flooding

Lighting

Maintenance and cleanliness



The feedback that we heard at the drop-in sessions was generally in line with that of the online survey.

Next steps

The project team will review and consider all feedback received during community consultation. Feedback from the community will be used to help inform the final PDP. Once finalised, the PDP will be presented to the Newcastle City Traffic Committee before going back to Council for adoption.



Thanks for having your say!

We'll continue to update the community as the project progresses.

newcastle.nsw.gov.au/yoursay



GENERAL COMMENTS

Proposed traffic changes:

- Bishopsgate Street vehicle movement should remain unchanged
- One-way streets need more consideration as to building access and exit points
- The need for educating motorists about changes and having adequate street signs
- Reduce vehicle speed limits in the precinct to 40km/h due to the number of trucks and poor visibility on streets
- Union Street should be residential traffic only and a loading zone is needed

Proposed pedestrian changes:

- Footpaths should be uniform throughout Wickham
- PDP needs greater definition/detail for overarching pedestrian design elements
- Footpaths should be reviewed by accessibility consultants to conform to disability standards
- Width of footpaths e.g. too much space taken up by verge gardens, not enough space for wheelchair users to pass another pedestrian
- Lighting should be included to improve pedestrian access

Proposed cycleway changes:

- Cycleways should be separated from roads and footpaths where possible
- Planning for cyclists on other local streets e.g. more detail needed around contra-flow, one-way streets make it unsafe for cyclists
- Providing access to bicycle parking and racks
- Potential impact on loss of parking

Entry points/welcome nodes:

- Suggested entry points include: the northern end of Railway Street, Dangar Street, Throsby Street, Bishopsgate Street, Albert Street, Grey Street and Union Street
- The most common suggestions for welcoming people into Wickham were: a natural element or art element, a place to sit, a sign or a green space/trees/wall garden
- Some other suggestions included wayfinding, distinctive footpath treatments, lighting and a historical element

Urban activation areas:

- Recreational fitness was a top suggestion for the Bullock Island Rail Corridor
- Community gardens and play spaces
- Public toilets, public art, seating, heritage, music stages/entertainment, cafes/bars

Stories, landmarks, art, etc:

- The most common suggestions for things that should be celebrated about Wickham were: the history of Wickham, Aboriginal heritage/culture and sites of significance (including the Tree of Knowledge), heritage buildings and local art

Other key themes:

- **Lighting:** how lighting will be used to provide a safe after-dark environment
- Warm lighting is preferred
- Bollard lighting instead of overhead lighting
- Ground uplights, wall lighting and other street lighting to activate areas
- Harsh fluorescent lighting at Newcastle Interchange

- **Connectivity:** interchange needs better connection with bus, train and ferry terminal nearby
- Throsby Street is an emerging area with activation potential and should be included in the connectivity

- **Trees and plantings:** suggestion that if footpaths are too narrow for gardens, then gardens could be moved into the street and also be used to help calm traffic as well as prevent parking across driveways or footpaths
- Concerns about tree heights under powerlines and deciduous trees dropping leaves in gutters and on footpaths
- Potential impact of trees on sight lines and visibility, and the impacts on maintenance and cleanliness e.g. impact on footpaths, gutters and drains

Recreation spaces/open space:

- Concerns about public seating encouraging antisocial behaviour at night. Suggestion to include street trees/gardens in the proposed urban activation spaces instead
- Include facilities for dog owners e.g. off-leash dog park
- Tree of Knowledge Park should be made larger, waterfront parkland with improved facilities – a central place for residents, workers, walkers and cyclists
- Bullock Island Rail Corridor – a great space for a walk or a picnic. Could also include fitness equipment, seating, wayfinding, local history signage
- Provision of public rubbish bins

Design/aesthetics:

- Heritage conservation and architectural character should be an overarching design element
- Developers to incorporate more colours rather than monochromatic colour scheme
- Industrial design element referencing heritage of the area should continue

Attachment C

LEGEND

- Existing two-way link
- Proposed two-way link
- Existing one-way link
- Proposed one-way link

M

M

MOTT
MACDONALD

COMMUNITY ENGAGEMENT REPORT

Project No. : 703425806

Sketch No. : SK01

Date: 10/05/2023

Some commented that the proposed lights at the roundabout would assist pedestrians with crossing the roads, especially during peak hours. Others mentioned that the lights were not needed. Suggestion to include a pedestrian fence along the median down Branch Street to better direct pedestrians. Concerns about cyclist safety at the roundabout

Railway Street is wide enough to have a physically separated cycleway on the west side, similar to what is planned for Station Street

Extend cycleway network e.g. via Holland Street and Wickham Park, connect proposed cycleways to existing cycleway at Tree of Knowledge Park/harbour foreshore, Maryville, Beaumont Street

Pedestrian safety crossing the corner of Railway Street and Greenway Street due to poor visibility

Furlong Lane to remain two-way to limit traffic exiting onto Hannell Street

Provide pedestrian crossing at Hannell Street to enable access to the harbour

Union Street one-way from Furlong Lane to Church Street will force more traffic onto Throsby Street

Vehicle access on the southern side of Wickham Park should be restricted to cyclists and pedestrians only. One-way from Maitland Road to Holland Street is not needed and would make Wickham Park less pedestrian friendly

Restricted vision for drivers on Union Street turning right onto Throsby Street

Pedestrian safety on Dickson Street e.g. need traffic calming devices, better signage or to make the street a cul-de-sac

Croft Street and Railway Lane should not be extended

Move the road closure on Union Street past Grey Street

Wickham Street is too narrow to support two-way traffic and parking on both sides of the road. Suggestion to remove parking on one side

Wickham Street should be one-way north

Station Street can be made one-way west until Union St to enable access to Hannell Street

Vehicle access to/from Newcastle Interchange and suggestion to include a drop-off zone

Wayfinding needed to connect Station Street to Union Street. Suggestion to also include features such as a welcome sign, distinctive pavement and street greening to invite people to walk to the village heart.

Station Street should be made one way to allow space for cycling infrastructure



ATTACHMENTS DISTRIBUTED UNDER SEPARATE COVER

CCL 28/11/23 – BROADMEADOW TO HAMILTON EAST CYCLEWAY

- | | | |
|-----|----------------------|--|
| 8.8 | Attachment A: | Broadmeadow to Hamilton East
Cycleway - Consultation Flyer |
| 8.8 | Attachment B: | Broadmeadow to Hamilton East
Cycleway - Summary of submissions |
| 8.8 | Attachment C: | Broadmeadow to Hamilton East
Cycleway - Amended design concepts |

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Active Transport Project

What is proposed

We are seeking feedback on a proposal to deliver new infrastructure that will improve pedestrian and cyclist safety between Chatham Street, Broadmeadow and Gordon Avenue, Hamilton East. This project is part of the cycleway connection from Adamstown to Newcastle West.

Proposed works include:

- A refuge for cyclists to safely cross Chatham Street at Koree Road
- A shared path on the south side of Dumaresq Street from Chatham Street to Steel Street
- Establishing a cycling 'quiet way' on sections of Steel Street and Everton Street by installing:
 - Kerb extensions at various intersections along Everton Street, including Steel Street, Lawson Street and Gordon Avenue
 - Cycling refuges at the intersections of Everton Street and Beaumont Street & Everton Street and Gordon Avenue to improve crossing safety and calm traffic along Everton Street
- Landscaping improvements, including rain gardens
- Street lighting upgrades

Visit the **Have Your Say** webpage for more information and to view the concept plans.

How we use feedback

We are seeking community feedback to make an informed decision on this project.

To have your say, visit www.newcastle.nsw.gov.au/your say and complete the online form.

Alternatively, you can email a written submission to engage@ncc.nsw.gov.au.

Please include 'Submission – Broadmeadow to Hamilton East Cycleway Improvements' in the subject line.

Postal submissions can be sent to:

Chief Executive Officer
City of Newcastle

Attention: Planning, Transport and Regulation

PO Box 489
Newcastle NSW 2300

Subject: Submission – Broadmeadow to Hamilton East Cycleway Improvements

The public exhibition period closes
5pm, Monday 11 September 2023.



Cycling improvements

Broadmeadow to Hamilton East Cycleway



This project proposes to deliver new infrastructure for a safe cycling route extending from Chatham Street, Broadmeadow to Gordon Avenue, Hamilton East.

Chatham Street, Broadmeadow to Gordon Avenue, Hamilton East

- Shared path
- Quietway

Refuge on
Chatham St
at Koree Rd

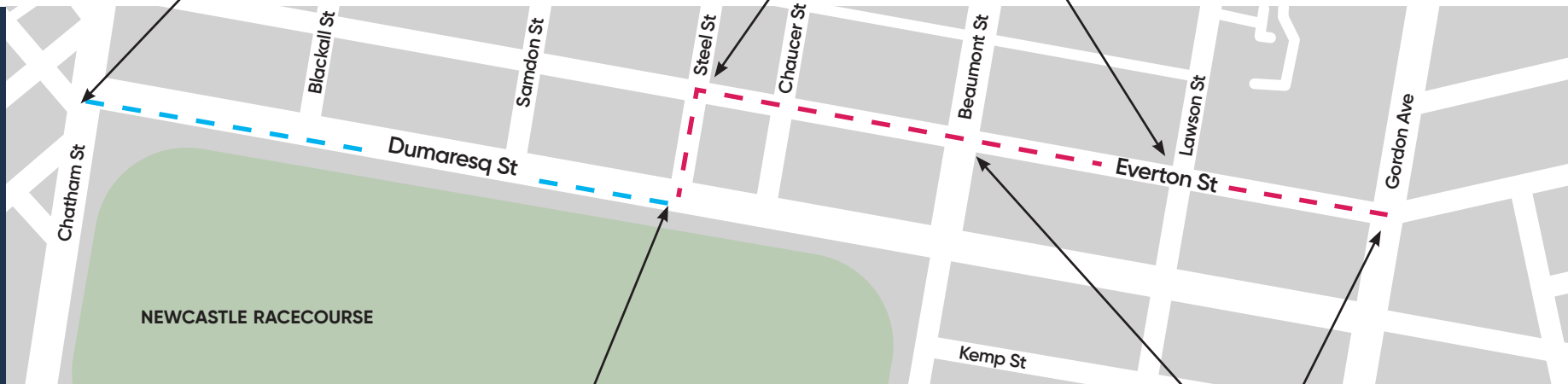
Kerb extensions at Everton St
and Steel St intersection –
similar treatment at Lawson St
intersection

Concept plans for all intersections
and crossing points can be found
on the Have Your Say Page

Background

Cycling between Adamstown, Broadmeadow, Hamilton and Newcastle West is very popular. It connects some key cycleways and destinations, including schools, parks and local centres. However, the existing route which runs along Dumaresq Street is not designed for all ages and abilities, and there are some safety concerns which need to be addressed.

This work is part of a broader project which will provide a safe cycling connection between Newcastle West and Adamstown Station, which was identified in CN's 'On Our Bikes – Cycling Plan 2021-2030'.



Transition between
shared path on
Dumaresq St and
quietway at Steel St

Proposed cycling refuge
at Everton St and Gordon
Ave intersection – similar
treatment for Beaumont St
intersection*

*Alternative options can be found
on the Have Your Say page



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SUMMARY OF SUBMISSIONS – BROADMEADOW TO HAMILTON EAST CYCLEWAY IMPROVEMENTS

No.	Document Date	Suburb	Support	Summary of Submission	CN Response
1	Sep 11, 2023	Sydney	Yes	Supports quietway, with kerb extensions being preferred to cycling refuge noting that if limiting right turn and thru movements is an issue, a hybrid of the options should be considered.	It will be difficult to ban movements without introducing a physical barrier, so a hybrid solution would not be possible while still achieving the desired outcomes
2	Sep 11, 2023	The Hill	Yes	Recommends intersection at Chaucer St is improved for pedestrians with kerb extensions, kerb ramps, and protected refuges on Chaucer for pedestrians.	Pedestrian improvements at Chaucer St can be considered in detailed design
3	Sep 11, 2023	Hamilton	Yes	Recommends kerb extensions at Samdon Street/Denison Street intersection and Samdon Street/Everton Street intersection.	This is outside the scope of this project. It could be considered in future LATM works
4	Sep 11, 2023	Hamilton	No	Only supports refuge on Chatham Rd, noting issues with concrete on Dumaresq and safety and parking on Everton. Concerned that the cycleway lacks connections and destinations.	There will be minimal parking loss. Appreciating the issues with impermeable surfaces, the shared path will have multiple accessibility benefits for both pedestrians and cyclists. This project needs a defined scope and so cannot complete the whole network. However, the connections to the east will be considered as planned in the cycling plan
5	Sep 11, 2023	Broadmeadow	Yes, with changes	Supports cycling improvements in general, but believes the cycleway should be on Dumaresq St and that the quietway will pose issues for younger riders with preference for an offroad path.	Both Everton St and Dumaresq St are on the cycling plan. Dumaresq St will have improvements made in a future project. Appreciating quietways are a newer treatment, their design is meant to cater for cyclists of all ages and abilities. After evaluations, further traffic calming may be considered to ensure children feel comfortable to ride along the quietway.

No.	Document Date	Suburb	Support	Summary of Submission	CN Response
6	Sep 11, 2023	Lambton	Yes, with changes	Notes there are issues with narrowing roads where cyclists ride	Noting that kerb extensions can cause hazards for cyclists on busier roads, on streets which are quiet, the cyclist shouldn't feel the need to ride to the left of the lane and therefore be disturbed by kerb extensions.
9	Sep 11, 2023	Hamilton	Yes, with changes	Recommends extending the shared path to Beaumont St. Emphasises the importance of left-in left-out at Gordon Avenue and Beaumont Street. Recommends kerb extensions at Samdon st/Denison st intersection and Samdon st/Everton st intersection.	A footpath between Steel St and Beaumont St is being planned as part of our Pedestrian Access & Mobility Program. Works in Samdon St are outside the scope of this project. It could be considered in future LATM works.
10	Sep 10, 2023	Hamilton	No	Does not support diversion to Everton Street. Believes road narrowing creates pinchpoints and reduces cycling safety.	Both Everton St and Dumaresq St are on the cycling plan. Dumaresq St will have improvements made in a future project. Kerb extensions support making streets quieter, where the cyclist shouldn't feel the need to ride to the left of the lane and therefore be disturbed by kerb extensions.
11	Sep 10, 2023	Hamilton	Yes	Supports refuge islands	Noted
12	Sep 10, 2023	Hamilton East	No	Doesn't support refuge on Chatham Road as it doesn't allow for onroad cyclists to join cycleway. Doesn't support road narrowing and believes the route is less desirable than a direct route	Noted. This will be considered in detailed design along with a thorough design for cyclists approaching and leaving this cycleway to ride on the existing onroad network. Everton St is the direct route from Adamstown to Newcastle West and the Transport interchange.
13	Sep 10, 2023	Hamilton	Yes, with changes	Concern that flipping the priority to Everton St at Lawson St could create a worse rat run if left-in/left-out options aren't achieved for adjacent intersections. Believes cycling refuges will	Speeds and volumes along Everton Street will be monitored after the project to ensure no adverse effects. If no turning restrictions are made as part

No.	Document Date	Suburb	Support	Summary of Submission	CN Response
				provide a good option for school students travelling to SFX.	of this project, the alignment of the Stop priorities will be reviewed under detailed design
14	Sep 10, 2023	Hamilton	Yes, with changes	Doesn't support kerb extensions, particularly at Steel Street where there are drainage issues.	Drainage issues will be considered in detailed design.
15	Sep 10, 2023	Hamilton	Yes	Need to consider traffic calming on Samdon Street including kerb extension at Denison Street	This is outside the scope of this project. It could be considered in future LATM works.
16	Sep 10, 2023	The Hill	Yes, with changes	Need to consider pedestrian accessibility and safety throughout the plan - including crossing Dumaresq St at Chatham Road and at Steel St - need for kerb ramps and refuge. Consider improvements on Dumaresq St at the same time, especially at Gordon Avenue. Ensure pedestrian accessibility on both sides of Everton St at Gordon Ave and Beaumont St.	Pedestrian improvements at these intersections will be considered in detailed design
17	Sep 09, 2023	Mayfield	No	Concern over safety of small sections of bike lane, hook turns, sight distances, and conflicts with cars turning left. Advice to look at Dutch planning guides and raise the crossing points	Sightlines will be checked with appropriate traffic engineering standards, hook turns are an existing treatment in Newcastle, noting this is a different application of them. These short cycling plans are designed to give cyclists the opportunity to wait in a more central position, noting they can go straight while cars can only turn left. On other streets, when a car is turning left and cyclists going right, this is the natural way they will approach the intersection.
18	Sep 08, 2023	Islington	Yes	Supports cycleway noting that it shouldn't be too adverse for parking or driving.	Noted

No.	Document Date	Suburb	Support	Summary of Submission	CN Response
19	Sep 08, 2023	Hamilton South	Yes	Concerns the kerb extensions will reduce parking where there is no off street parking at Gordon Ave, and that bringing cyclists off the road will pose an issue for elderly pedestrians.	These issues with pedestrian/cyclist conflicts are being considered and will be factored into the decision of which option we choose. There will be minimal parking losses.
20	Sep 07, 2023	The Hill	Yes, with changes	Need to consider pedestrian accessibility and safety throughout the plan - including crossing Koree Road (need for improved refuge), Dumaresq St at Chatham Road and at Steel St - need for kerb ramps and refuge. Ensure pedestrian accessibility on both sides of Everton St at Gordon Ave and Beaumont St with refuges and kerb ramps. Include a footpath along Dumaresq to Steel St.	Pedestrian improvements at these intersections will be considered in detailed design
21	Sep 07, 2023	Adamstown	Yes, with changes	Need for more off-road cycleways	Noted - and provided where space allows.
22	Sep 07, 2023	Broadmeadow	No	Need for physical barriers, concerns for safety of design	While separation of cyclists and motorised traffic is ideal, where there are issues with driveways and we have an existing quiet street, a quietway has been seen as the more viable option in residential areas.
23	Sep 07, 2023	Highfields	Yes, with changes	Concerns for the safety and effectiveness of the design for cycling user experience, with the need for separation	While separation of cyclists and motorised traffic is ideal, where there are issues with driveways and we have an existing quiet street a quietway has been seen as the most suitable option for residential streets.
25	Sep 07, 2023	The Hill	No	Concerns this isn't efficient for cyclists. Recommends improvements on Dumaresq St and to put a refuge island on Brunner Road	Both Everton St and Dumaresq St are on the cycling plan. Dumaresq St will have improvements made in a future project.

No.	Document Date	Suburb	Support	Summary of Submission	CN Response
26	Sep 07, 2023	Merewether	No	Recommends improvements on Dumaresq St	Both Everton St and Dumaresq St are on the cycling plan. Dumaresq St will have improvements made in a future project.
27	Sep 07, 2023	New Lambton	Yes	Supports the project	Noted
28	Sep 07, 2023	Merewether	Yes, with changes	Doesn't support the shared path on Dumaresq St, noting safety issues between commuting cyclists and pedestrians	The pedestrian and cycling interaction is an important consideration for shared paths and will be monitored after this project. The shared path will be 3m with no pinch points or restrictions on visibility of users.
30	Sep 07, 2023	Mayfield	Yes, with changes	Issues with intersection design - recommends stopping area for cyclists in front of cars	Consideration of bicycle lanes and left turning cars will be considered in detailed design.
32	Sep 06, 2023	Broadmeadow	No	Concerns of loss of parking of Koree Road, and at other locations with kerb extensions	There will be minimal parking loss, noting that the kerb extensions will be where existing No stopping is near intersections. Some parking loss is necessary (at Koree Rd) to make the road safer for pedestrians and cyclists.
33	Sep 06, 2023	Hamilton	Yes, with changes	Recommends separation of cyclists and pedestrians on Dumaresq St, with a cycle lane and footpath required to avoid conflicts, noting high rate of dog walking and cycle commuting along this route	An on road separated cycling facility is not feasible while maintaining the current high recreational/training cycling use around the race course. The pedestrian and cycling interaction is an important consideration for shared paths and will be monitored after this project. The shared path will be 3m which is wider than many existing shared paths across the city.
34	Sep 06, 2023	Hamilton	Yes	Supports moving the cycleway to Everton St, but doesn't support the treatment that will create a conflict near SW corner at Driveway on Everton St	These issues are being considered and will be factored into detailed design to address all property access points.

No.	Document Date	Suburb	Support	Summary of Submission	CN Response
35	Sep 06, 2023	New Lambton	Yes, with changes	Recommends making the whole side of Dumaresq St (between racecourse and trees) an active transport zone, with consideration of a similar treatment on Parkway Avenue where there aren't residents and exploring connection options between and along to Bar Beach. Consider using Dutch style roundabouts where possible and avoid diverting cyclists to Everton St	This would involve consideration of where traffic would divert, but it may be an option for the future as part of our cycling plan. The connection to Bar Beach is outside the scope of this project.
38	Sep 06, 2023	Broadmeadow	Yes	Doesn't support the left-in/left-out restrictions at intersections, with the potential to increase traffic on Dumaresq St	Currently there are very low volumes of traffic on Everton St so any diversion to other streets will not adversely affect other streets.
39	Sep 06, 2023	Lambton	Yes	Checking no parking affected between Chaucer St and Steel St	There will be no parking affected at this location
40	Sep 05, 2023	Belmont North	Yes, with changes	Recommends direct connection at Chatham to Everton St. Consider tandem bikes, and trailers in designing the cycling refuges	Everton Street is hilly west of Steel St so we have opted to facilitate the flatter route. Different bike types will be considered in designing the cycling refuges.
41	Sep 05, 2023	Hamilton East	Yes	Recommends extending no stopping areas near Gordon Ave, and reducing speeds on Gordon Ave	No stopping areas will be checked against traffic engineering standards. LATM projects can be developed to support future cycleway works on Gordon Ave with the potential to reduce speeds.
42	Sep 04, 2023	Adamstown	Yes, with changes	Recommends off/on ramps from Shared path on Dumaresq St/Chatham Rd to provide more movement options for cyclists. Recommends extending the path to Beaumont St. Need to focus on wayfinding east of Gordon Ave and also consider making Dumaresq St a quietway	Noted. Entry/exit to the shared path will be considered in detailed design along with a thorough design for cyclists approaching and leaving this cycleway to ride on the existing onroad network. Path will be extended as part of the PAMP program. Wayfinding east of Gordon Avenue will be considered as part of our wayfinding works.

No.	Document Date	Suburb	Support	Summary of Submission	CN Response
43	Sep 04, 2023	Kotara	Yes, with changes	Need to consider cyclists going from Pokolbin St to the shared path, consider long bikes in refuge design, consider flipping priority at Gordon Avenue.	Entry/exit to the shared path will be considered in detailed design along with a thorough design for cyclists approaching and leaving this cycleway to ride on the existing onroad network. Refuges will be designed to cater for a range of bikes
44	Sep 04, 2023	Adamstown	Yes, with changes	Consider focus on Dumaresq St where Strava data indicates high cycle use. Recommends separated cycleways	Both Everton St and Dumaresq St are on the cycling plan. Dumaresq St will have improvements made in a future project. While separation of cyclists and motorised traffic is ideal, where there are issues with driveways and we have an existing quiet street a quietway has been seen as the most suitable option for residential streets.
45	Sep 03, 2023	Hamilton South	Yes	Ensure kerb ramps are included at all crossing points, including refuges	Kerb ramps will be provided at all crossing points.
46	Sep 03, 2023	Broadmeadow	Yes, with changes	Notes issues with cars from Chatham Road entering wrong side of Koree Rd - for consideration in plans, and placement of Giveaway signs	Improvements to the refuge and kerb blisters on Koree Rd will be considered as part of detailed design. Placement of signs and lines will also be considered in the final stages of the project.
48	Sep 03, 2023	Hamilton	Yes	Encourages project to proceed promptly	Noted
50	Sep 01, 2023	Kotara	Yes	Supports project	Noted
51	Aug 31, 2023	Hamilton South	Yes, with changes	Issues with Dumaresq St shared path being busy with pedestrians, need to consider lighting	Lighting will be considered in detailed design
54	Aug 31, 2023	Rankin Park	No	Doesn't agree with quiet way design and diversion to Everton St, doesn't believe the cycling refuges benefit outweigh the inconveniences for traffic flow	Appreciating quietways are a newer treatment, their design is meant to cater for cyclists of all ages and abilities. The cycling refuges have been supported by the majority of respondents, and

No.	Document Date	Suburb	Support	Summary of Submission	CN Response
					have been determined to provide a suitable calming treatment while prioritising cycling
55	Aug 31, 2023	Highfields	Yes	Supports improvements, with benefits for children. Notes the importance of long term planning for a dedicated cycleway	Noted
56	Aug 31, 2023	Thornton	Yes, with changes	Recommends Chatham Rd refuge is moved towards Pokolbin St and caters for cyclists from Pokolbin St	Moving the refuge slightly will be considered as part of the detailed design.
57	Aug 30, 2023	New Lambton	Yes, with changes	Recommends reducing speeds to 30km/h	Speed reductions may be advocated for at the completion of the project.
60	Aug 30, 2023	Adamstown	No	Notes need for more dedicated cycleways	Noted
61	Aug 30, 2023	Hamilton	Yes	Recommends reducing speeds to 30km/h	Speed reductions may be advocated for at the completion of the project.
62	Aug 30, 2023	Hamilton	Yes	Need for speed reductions on Dumaresq, Beaumont St and Gordon Ave to ensure safety of crossing locations	LATM projects can be developed to support this cycleway with the aim to reduce speeds.
64	Aug 30, 2023	Adamstown	Yes, with changes	Consider neighbours, and the need to improve crossing Brunker Road	Noted. Crossing points on Brunker Rd will be investigated as part of the cycleways and LATM programs.
66	Aug 30, 2023	Hamilton East	Yes	Supports refuges but notes the need to consider how additional traffic on Dumaresq St will affect intersections, including crossing Gordon Ave	We will monitor the road network after the completion of the project to ensure surrounding streets are not adversely impacted.
67	Aug 30, 2023	Islington	Yes, with changes	Recommends reducing speeds to 30km/h and giving priority to cycleway	Speed reductions may be advocated for at the completion of the project. The Stop priority has been swapped at Lawson Street to maintain cycling continuity, while continuing to manage through traffic volumes
68	Aug 29, 2023	Hamilton	Yes, with changes	Need for speed reductions on Dumaresq	LATM projects can be developed to support this cycleway with the aim to reduce speeds.

No.	Document Date	Suburb	Support	Summary of Submission	CN Response
69	Aug 28, 2023	Hamilton	Yes, with changes	Supports refuges but notes flipping priority could encourage speeding along Everton St (Changing the location of the Stop hold)	Speeds and volumes along Everton Street will be monitored after the project to ensure no adverse effects. Given the support of the central refuges, the change to the Stop priority at Lawson Street will assist cycling continuity.
70	Aug 27, 2023	Birmingham Gardens	No	Doesn't support project, concerns over driver's sight lines	Noted. Sightlines will be checked with appropriate traffic engineering standards.
71	Aug 26, 2023	New Lambton Heights	Yes, with changes	Recommends cycleways are separated from motor vehicles	While separation of cyclists and motorised traffic is ideal, where there are issues with driveways and we have an existing quiet street a quietway has been seen as the most suitable option for residential streets.
72	Aug 24, 2023	Hamilton	Yes, with changes	Issues with diversion to Everton St and restriction left-in/left-out, recommends extending cycleway to Merewether High School	Both Everton St and Dumaresq St are nominated routes within the Cycling Plan. Dumaresq St will have improvements made in a future project. The left-in/left-out should have minimal impact on locals, noting alternative streets and roundabouts. The cycleways will be extended along Pokolbin St to Merewether High School as part of the cycling plan.
73	Aug 23, 2023	Hamilton	Yes, with changes	Doesn't support diversion to Everton St and recommends improvements to cycling along Beaumont St	Both Everton St and Dumaresq St are nominated routes within the Cycling Plan. Dumaresq St and Beaumont St will have improvements made in a future project as part of the cycleways program.
74	Aug 22, 2023	Broadmeadow	Yes	Issues with shared path on Dumaresq St with pedestrians and cyclists mixing, recommends cycleway along the whole length of Everton St	Everton Street is hilly west of Steel St so we have opted to facilitate the flatter route. The pedestrian and cycling interaction is an important consideration for shared paths and will be monitored after this project. The shared

No.	Document Date	Suburb	Support	Summary of Submission	CN Response
					path will be 3m which is wider than many existing shared paths across the city.
75	Aug 22, 2023	Hamilton	Yes, with changes	Queries treatments on Steel St with the assumption cyclists will share the road with cars	Cyclists will share the road with cars on Steel St, with marked cycle lanes.
76	Aug 21, 2023	Hamilton	No	Recommends a roundabout at intersection of Gordon Avenue and Dumaresq St or extension to moving cycling refuge to the location. Recommends extending path to Beaumont St. Doesn't support diversion to Everton St and kerb extensions	Both Everton St and Dumaresq St are nominated routes within the Cycling Plan. Dumaresq St will have improvements made in a future project, including improvements at Gordon Ave. Path will be extended in the Pedestrian Accessibility & Mobility Program.
77	Aug 20, 2023	Hamilton South	Yes	Consider shrubs in the kerb extensions	Planting will be considered in kerb extensions, noting that sightlines for children will be considered in plantings.
78	Aug 20, 2023	WallSEND	Yes	Would prefer cycle path towards edge of road with potential barrier	While separation of cyclists and motorised traffic is ideal, where there are issues with driveways and we have an existing quiet street a quietway has been seen as the most suitable option for residential streets.
79	Aug 20, 2023	Hamilton East	Yes, with changes	Concerns that some options don't support children to ride, with the need to ride towards centre of the road near the intersection	Appreciating quietways are a newer treatment, their design is meant to cater for cyclists of all ages and abilities. After evaluations, further traffic calming may be considered to ensure children feel comfortable to ride along the quietway. Children will have the option to use the footpath and cross at pedestrian refuges.

No.	Document Date	Suburb	Support	Summary of Submission	CN Response
80	Aug 20, 2023	Hamilton East	Yes	Need to support children to ride (doesn't support cycling refuges), and reduce traffic movements	Appreciating quietways are a newer treatment, their design is meant to cater for cyclists of all ages and abilities. After evaluations, further traffic calming may be considered to ensure children feel comfortable to ride along the quietway. Children will have the option to use the footpath and cross at pedestrian refuges.
81	Aug 18, 2023	Hamilton	No	Questions plans east of Gordon Avenue	This project needs a defined scope and so cannot complete the whole network. However, the connections to the east will be considered as planned in the Cycling Plan.
82	Aug 17, 2023	Hamilton	Yes	Supports plans, noting minimal parking loss	Noted
83	Aug 16, 2023	Hamilton	Yes	Concerns of maintenance of landscaping at kerb extensions,	Engagement with Asset maintenance will determine the details of the kerb extensions and how they will be maintained.
84	Aug 16, 2023	Hamilton East	No	- <i>No comment provided</i>	
85	Aug 16, 2023	Hamilton East	Yes	Recommends separated cycling facilities, noting no issues between Lawson St and Gordon Ave and use of angle parking to reduce parking loss	Quietway treatment more feasible within residential areas to minimise conflicts. Minimal parking loss with no need for angled parking.
86	Sept 7, 2023	Hamilton East	Yes	Supports option 1 at Gordon Ave, noting the potential use for school students. Recommends kerb ramps and refuge to the south of the intersection, no queuing across intersection, speed reductions on Gordon Avenue, and shrub plantings or rain gardens	Noted. A pedestrian crossing point to the south of the intersection can be considered in detailed design. There are limitations as to where queuing across intersections can be limited, but this can be investigated. Shrubs or rain gardens will be part of the planting.

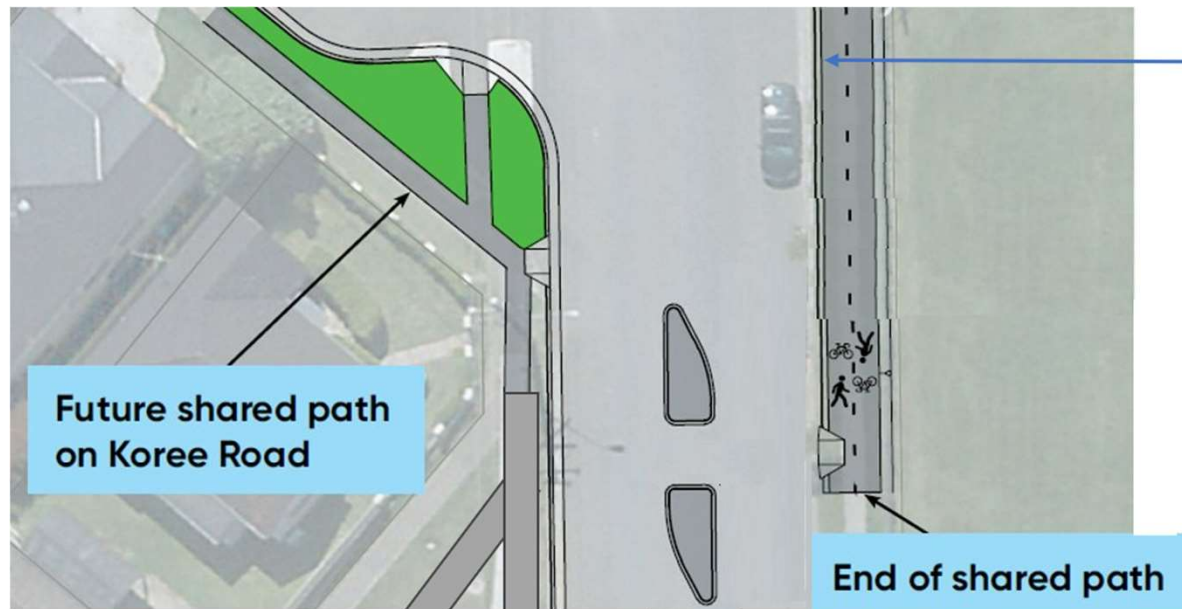
No.	Document Date	Suburb	Support	Summary of Submission	CN Response
87	Sept 9, 2023	Hamilton	Yes	Supports shared path but not the quietway, noting issues with the turning restrictions. Recommends extending no stopping on Chatham Rd to create a safer environment for cyclists. Consider positioning lighting so trees can shield residences from the light	Extending the no stopping on Chatham Rd can be considered in detailed design. The lighting design will consider how trees can shield residents from light. Turning restrictions should limit impacts to residents noting parallel roads which one can use to access Stewart Avenue.



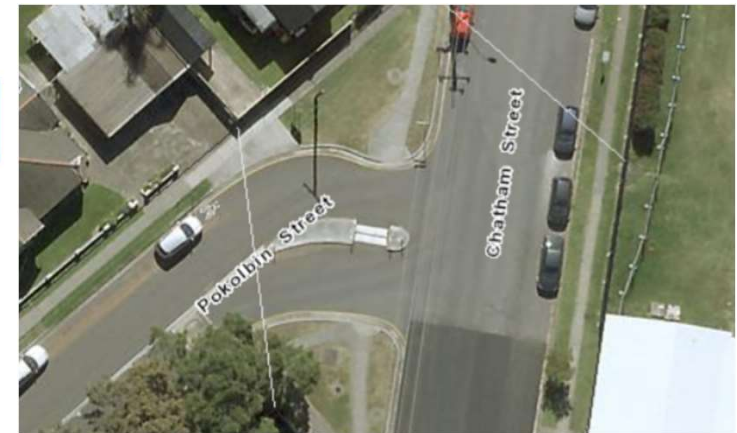
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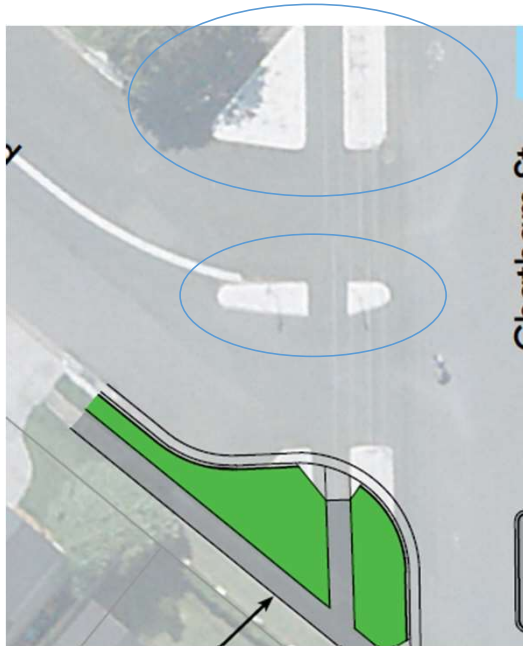
AMENDED DESIGN CONCEPTS: Chatham Rd refuge and connections



Develop a connection through to Pokolbin St



Chatham at Koree Rd– Upgrade refuge



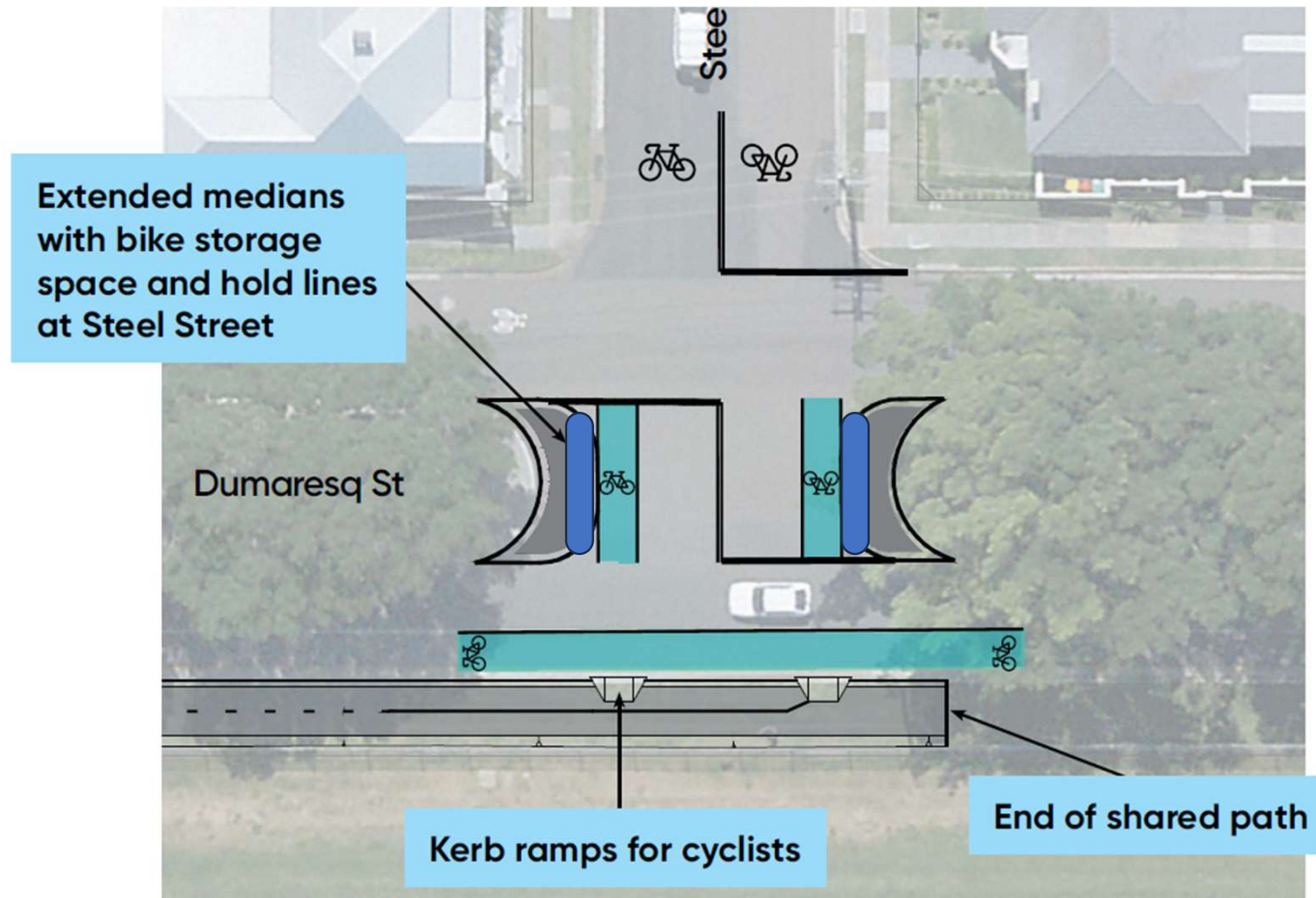
Existing kerb islands and central refuge to be revised and updated to current standards to improve pedestrian safety and public domain outcomes.

Turning movements for buses to be maintained.

Chatham St at Dumaresq St



Dumaresq St at Steel St



Small islands or kerb extensions for pedestrians to be installed on at least on side of Steel St to provide protected central median.

Kerb ramps to be provided to ensure accessibility for pedestrian access.

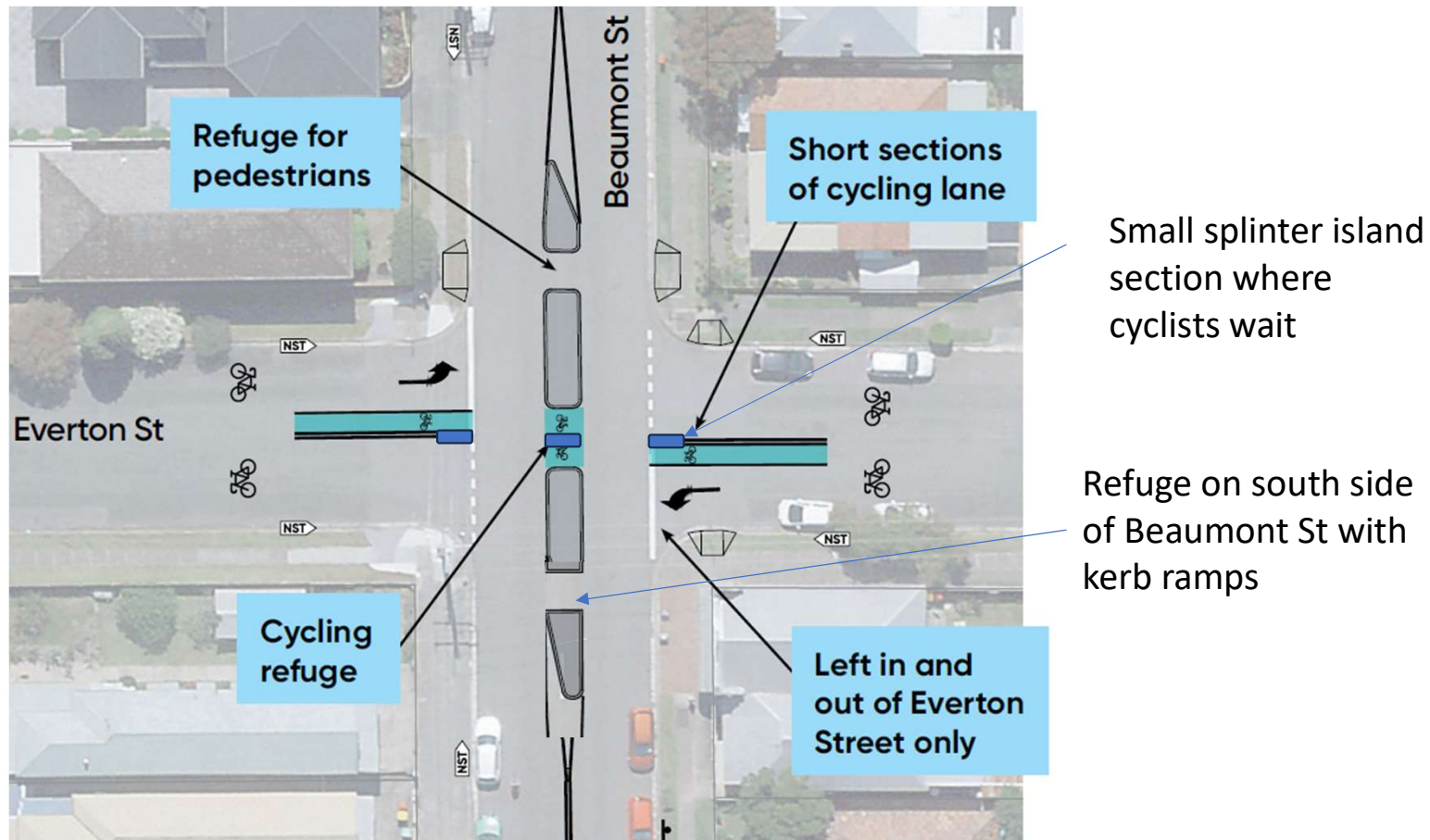
Everton St at Chaucer St



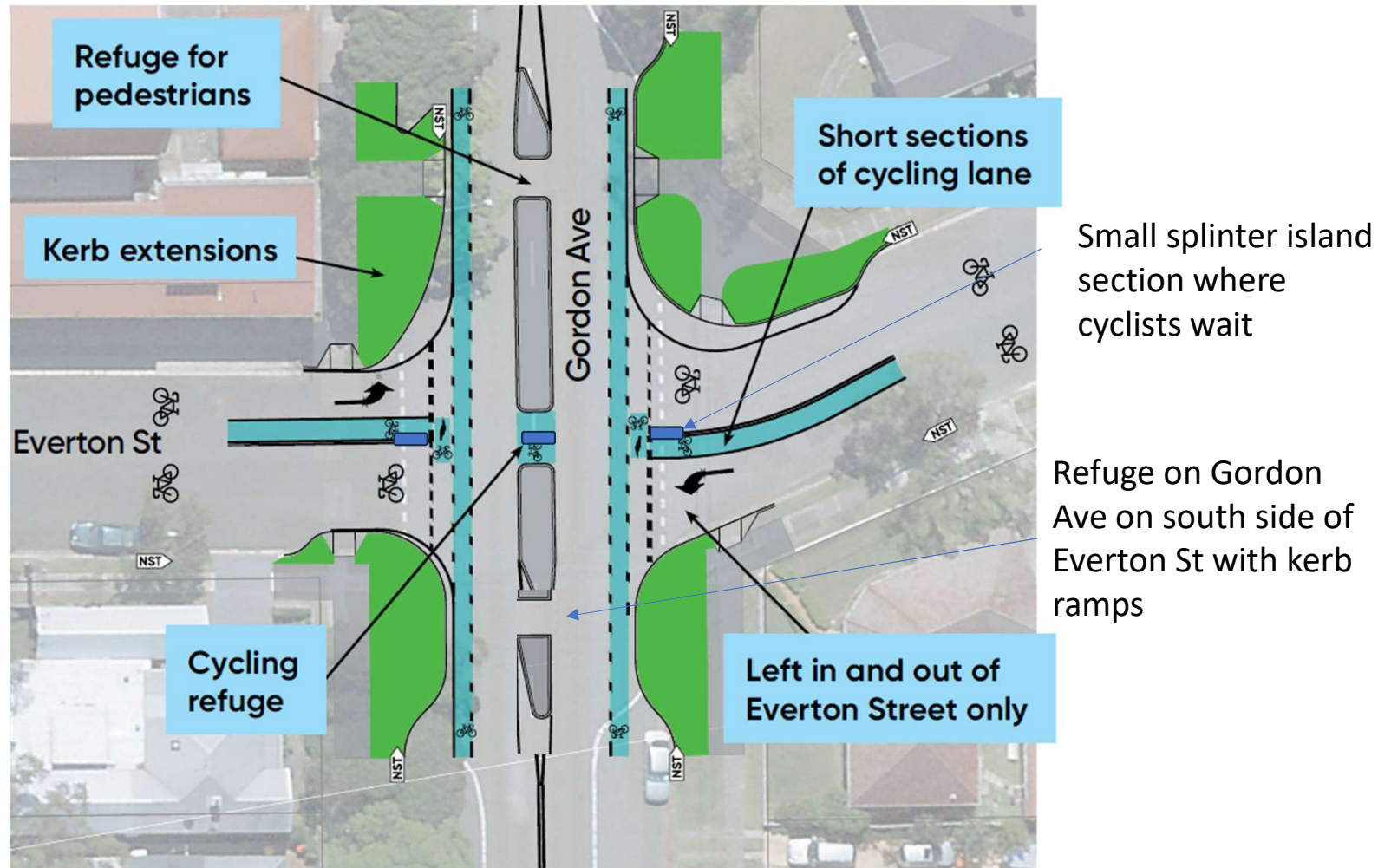
Small islands or kerb extensions for pedestrians to be installed on at least on side of Everton St to provide protected central median.

Kerb ramps to be provided to ensure accessibility for pedestrian access.

Everton St at Beaumont St



Everton St at Gordon Ave



ATTACHMENTS DISTRIBUTED UNDER SEPARATE COVER

CCL 28/11/23 – JESMOND CYCLEWAY IMPROVEMENTS

- | | | |
|-----|----------------------|--|
| 8.9 | Attachment A: | Jesmond Cycleway Improvements - Consultation Flyer |
| 8.9 | Attachment B: | Jesmond Cycleway Improvements - Summary of submissions |
| 8.9 | Attachment C: | Jesmond Cycleway Improvements - Amended concept design |

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Active Transport Project

Cycling improvements

William Street and Blue Gum Road, Jesmond



How we use feedback

We are seeking community feedback to make an informed decision on this project.

To provide feedback and find out more information visit www.newcastle.nsw.gov.au/yoursay and complete the online form.

Alternatively, you can email a written submission to engage@ncc.nsw.gov.au.

Please include 'Submission – Jesmond Cycleway Improvements' in the subject line.

Postal submissions can be sent to:

Chief Executive Officer
City of Newcastle

Attention: Planning, Transport and Regulation

PO Box 489
Newcastle NSW 2300

Subject: Submission – Jesmond
Cycleway Improvements

The public exhibition period closes
5pm, Friday 25 August 2023.



Background

Jesmond is a key area for cycling, with a number of major routes passing through it connecting nearby destinations including the University, Wallsend, John Hunter Hospital, as well as Jesmond Park, and Jesmond local centre.

The construction of the Inner-City Bypass has temporarily interrupted the popular R5 cycleway which is the main route between Wallsend and the city centre.

To improve connectivity at this time, the development of an alternate path through Jesmond is being provided along a largely off-road shared path, which predominantly runs next to Dark Creek passing through Heaton Park and MacClure Reserve.

Due to the extent of this project, it is being progressed as two stages. We are currently seeking feedback on Stage 1 of the works, involving the connection along William Street and Blue Gum Road.

For more information call **4974 2000**

This project proposes to deliver new infrastructure for a safe cycling route on William Street and Blue Gum Road, Jesmond.

What is proposed


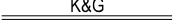


We are seeking feedback on a proposal to deliver new infrastructure for a safe cycling route on William Street and Blue Gum Road, Jesmond. This proposal is part of a broader project to improve cycling safety and connectivity from the existing overpass above the Inner-City Bypass to the shared path at the end of Mordue Parade, Jesmond.

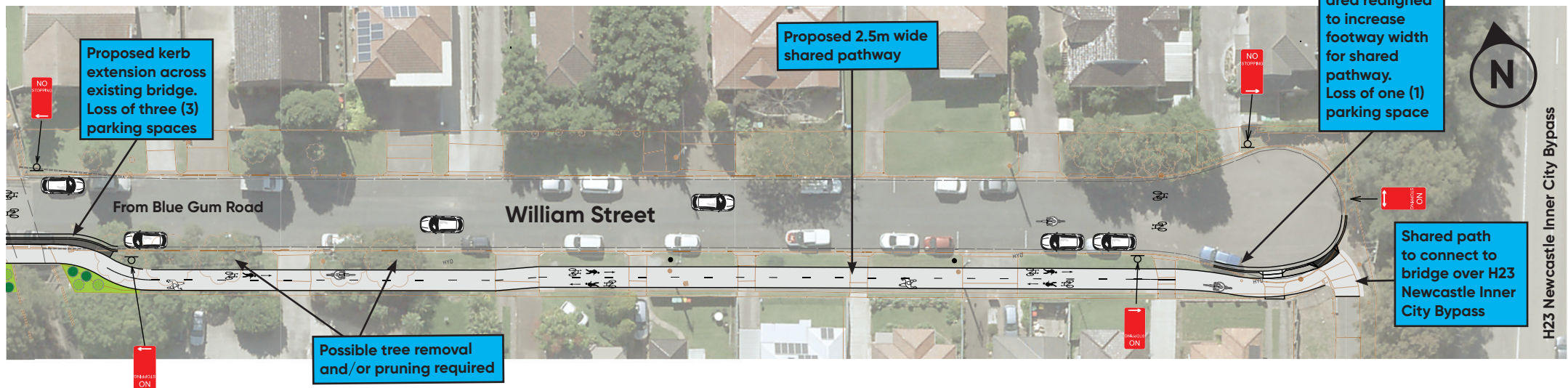
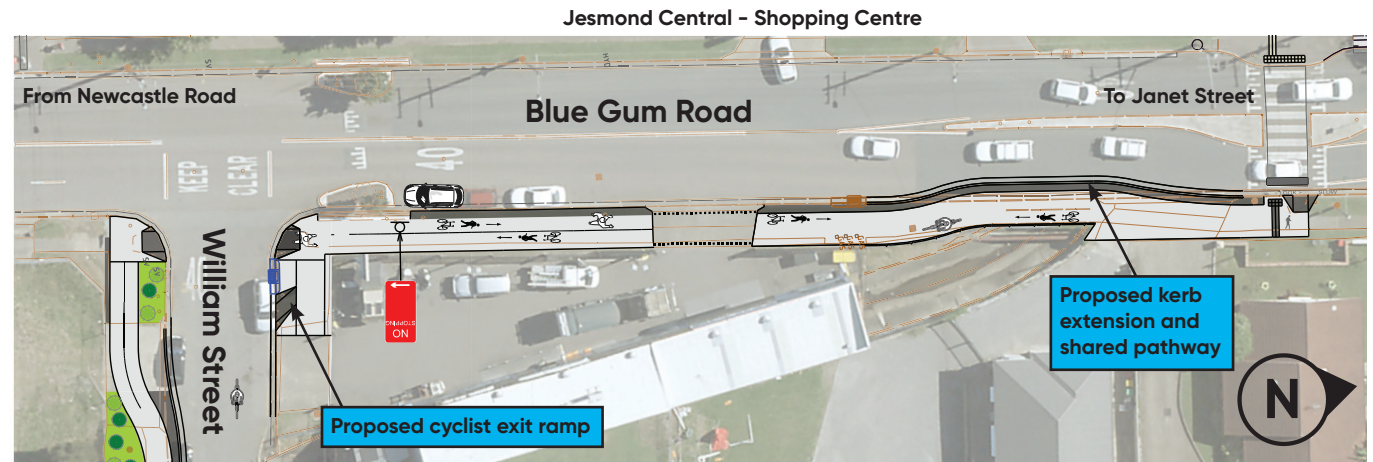
The proposed works include:

- Shared paths on the southern side of William Street, and on the eastern side of Blue Gum Road through to the existing pedestrian crossing
- New kerb ramps at the entry to William Street and an off ramp for cyclists wishing to ride on-road along William Street
- Kerb extension near the entrance to William Street
- Kerb extension on Blue Gum Road to accommodate the new shared path

William Street and Blue Gum Road, Jesmond

Legend

Proposed shared pathway	
Proposed kerb and gutter	
Proposed planting bed	
Proposed kerb ramp	





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SUMMARY OF SUBMISSIONS – JESMOND CYCLEWAY IMPROVEMENTS - WILLIAM STREET & BLUE GUM ROAD

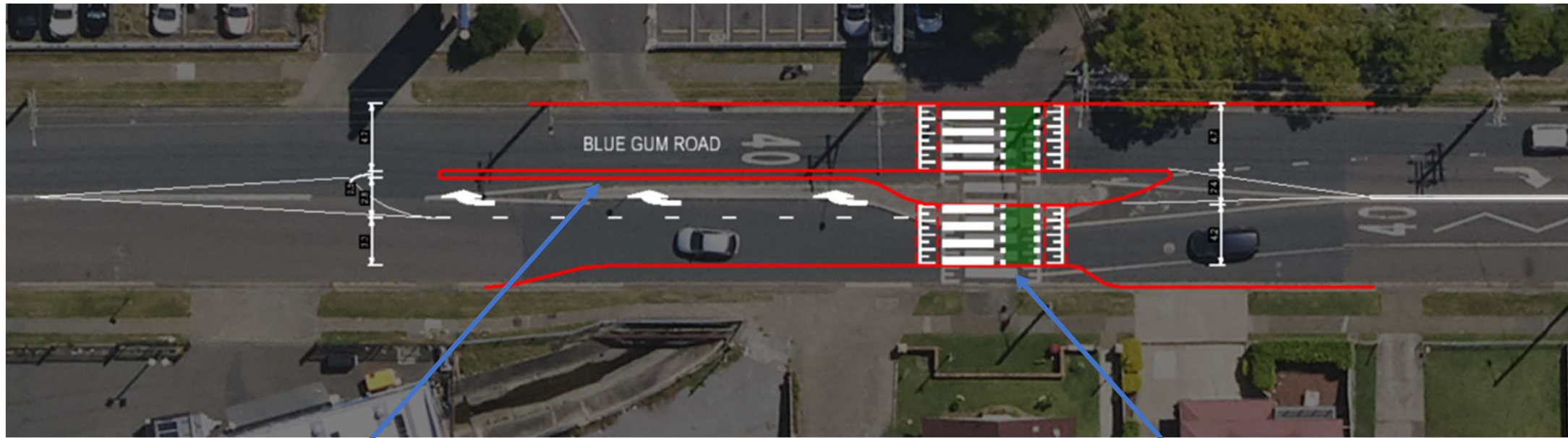
No.	Document Date	Suburb	Support	Summary of Submission	CN Response
1	25-Aug-23	Jesmond	Yes, with changes	Requests traffic calming on William Street	As a dead end street with no through traffic, there is minimal requirement for traffic calming in William Street.
2	24-Aug-23	Birmingham Gardens	Yes	Requests improvements to Illoura Street and access to Jesmond Park. Requests bollards at Tillie Street	Works to improve the accessibility at Illoura Street are being undertaken as part of another project. Tillie Street is outside the scope of this project, and it is being considered under separate investigations.
3	22-Aug-23	Jesmond	Yes, with changes	Supports project and would like to see traffic calming on William Street	As a dead end street with no through traffic, there is minimal requirement for traffic calming in William Street.
4	22-Aug-23	Islington	Yes, with changes	Requests speed limit of 30km/h	30km/h speed limits is not considered suitable for Blue Gum Road, and William St is a dead end street with local traffic only.
5	25-Aug-23		Yes	Supports project and like to see a priority crossing at William Street near Blue Gum Rd, ensure ramps are gentle in grade, potential kerb extensions on corners, splay paths to make it easier for manoeuvring bikes, convert Blue Gum Rd crossing to a combined crossing with kerb extensions, emphasise pedestrian/cycling priority on driveways, install mirrors where needed, reduce speed limits to 30km/h, paint bike symbols in centre of lanes on William Street.	Cycling, pedestrian and motor vehicle activity will be monitored on William Street after this stage of the project is complete and crossing options will be considered if the need arises. A combined crossing on Blue Gum Road will be considered as part of Stage 2 of the project. Painted symbols on William St, and splay paths to improved corners will be considered under detailed design.
6	25-Aug-23		No	Concerns that the shared path will cause congestion near their potential future drive-thru restaurant. Concerns over the width of	There is no approval for this development to date. However, the project manager has been working with the landowner to ensure that the driveway requirements are achieved and has amended the

No.	Document Date	Suburb	Support	Summary of Submission	CN Response
				their driveway entrance being reduced to less than 8 metres.	design. Congestion related to the drive-thru restaurant should be factored into their development application.

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Amended Concept Design: Blue Gum Road



Central median adjusted to provide right turn lane into shopping centre and maintain through traffic

Median island at crossing adjusted to provide consistent traffic paths. Potential to provide width for future shared crossing inclusion

ATTACHMENTS DISTRIBUTED UNDER SEPARATE COVER

**CCL 28/11/2023 – LAMAN STREET, COOKS HILL - PEDESTRIAN
IMPROVEMENTS AND TRAFFIC CALMING**

- | | | |
|-------------|----------------------|--|
| 8.10 | Attachment A: | Laman Street, Cooks Hill - Consultation
Flyer |
| 8.10 | Attachment B: | Laman Street, Cooks Hill - Summary of
submissions |

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Active Transport Project

How we use feedback

We are seeking community feedback to make an informed decision on this project.

To provide feedback and find out more information visit www.newcastle.nsw.gov.au/your say and complete the online form.

Alternatively, you can email a written submission to engage@ncc.nsw.gov.au.

Please include 'Submission – Cooks Hill Pedestrian Improvements' in the subject line.

Postal submissions can be sent to:

Chief Executive Officer
City of Newcastle

Attention: Planning, Transport and Regulation

PO Box 489
Newcastle NSW 2300

Subject: Submission – Cooks Hill
Pedestrian Improvements

The public exhibition period closes
5pm, Friday, 22 September 2023.



Have
your
say

What is proposed

We are seeking feedback on concept designs to improve pedestrian safety and reduce vehicle speeds.

The proposed works include:

- the installation of kerb extensions on Auckland Street to improve pedestrian connectivity
- the installation of landscaped kerb extensions on Laman Street to encourage lower vehicle speeds and support a safer cycling environment

Pedestrian improvements

Laman Street, Cooks Hill



We are seeking feedback on concept designs for kerb extensions on Laman Street and Auckland Street, Cooks Hill to improve the safety of pedestrians and road users.

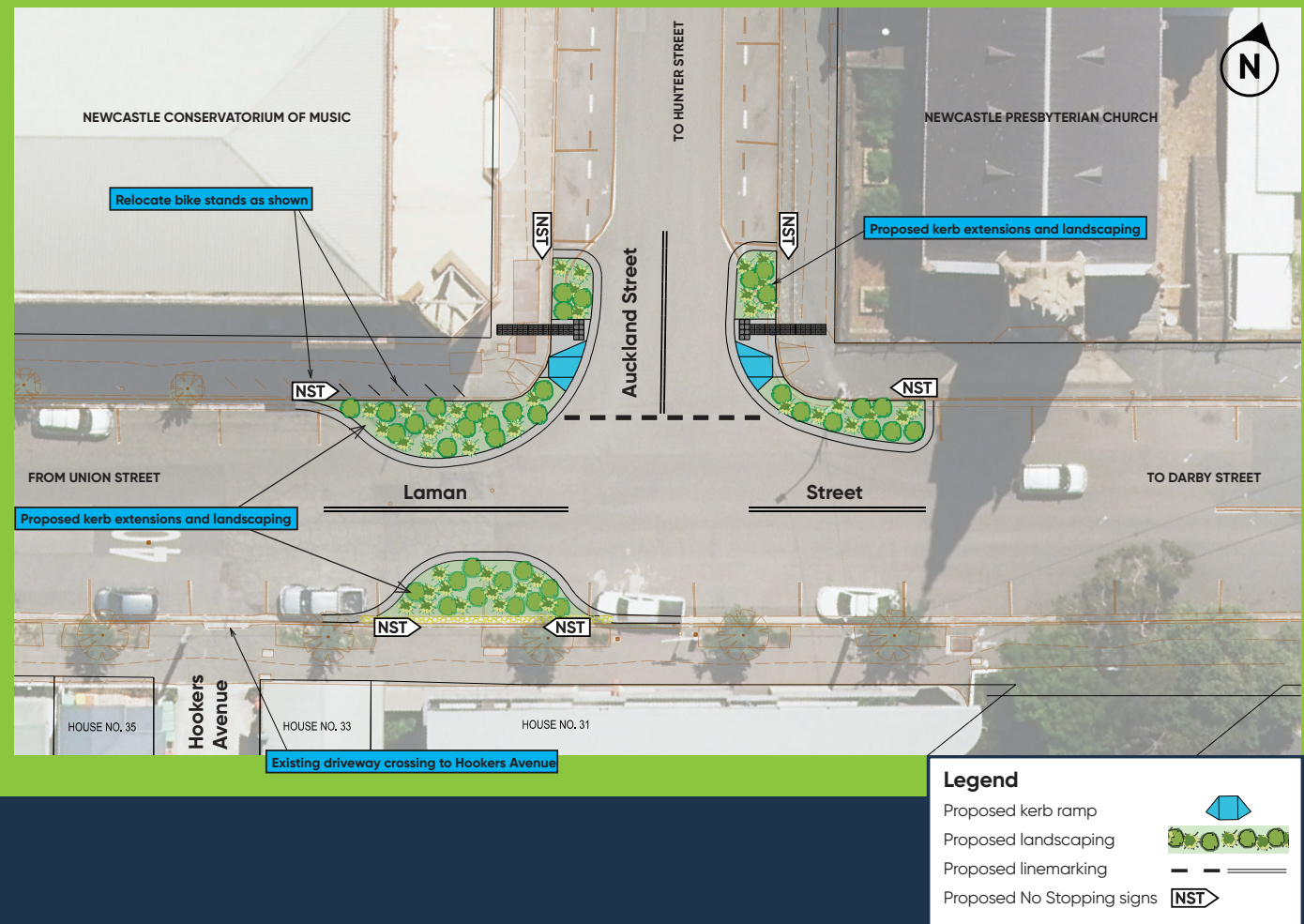
Laman Street, Cooks Hill

Background

Walking and cycling along Laman Street is very popular. It connects residential areas of Cooks Hill with key destinations including the City Library, Civic Park, Newcastle Community Pre-school, and the University of Newcastle City Campus.

We're seeking to improve pedestrian and cyclist safety by introducing kerb extensions to reduce crossing distances and calm traffic.

Laman Street is also part of the R1 Regional Cycle Route in CN's 'On Our Bikes – Cycling Plan 2021–2030'. The proposed works will support a cycling quiet way along Laman Street. Quiet ways aim to create a low-stress on-road experience for cyclists, with low speeds and volumes of motorised traffic.





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SUMMARY OF SUBMISSIONS - LAMAN STREET, COOKS HILL - PEDESTRIAN IMPROVEMENTS & TRAFFIC CALMING

No.	Date Submitted	Suburb:	Support	Summary of Submission	CN Response
1	24-Sep-23	Merewether	Yes	Supports the pedestrian improvements and traffic calming.	Noted
2	22-Sep-23	Cooks Hill	Yes	Supports the pedestrian improvements and traffic calming. Consider provision of a crossing facility on Laman Street.	Pedestrian improvements at the proposed kerb extensions on Laman Street will be considered in detailed design.
3	22-Sep-23	Cooks Hill	Yes	Supports the pedestrian improvements and traffic calming, noting the intersection is difficult to cross, particularly for children. Consider provision of a marked crossing on Auckland Street. Consider provision of kerb extensions at other nearby intersections.	The kerb extensions on Auckland Street will be designed to accommodate future installation of a marked (zebra) crossing. Pedestrian counts will be completed post-construction to inform consideration of a marked crossing. The other locations noted are out of scope for this project. Improvements will be considered as part of the upcoming review of the Cooks Hill LATM plan and ongoing development of the PPN and Walking and Mobility Plan.
4	22-Sep-23	Cooks Hill	No	Concerns regarding reduction of on-street parking.	In 2021, Council adopted a Parking Plan which set a clear goal to manage parking to improve the amenity of our streets, support accessibility of our centres and to encourage mode shift to active and public transport. The proposed kerb extensions have been placed close to the intersection to minimise parking loss. Two parking spaces on the southern side of Laman Street will be repurposed to provide a narrowing to encourage slower vehicle speeds and support a safer cycling environment in the street.

5	22-Sep-23	Waratah West	Yes, with changes	Supports the pedestrian improvements and traffic calming. Consider provision of a crossing facility on Laman Street.	Pedestrian improvements at the proposed kerb extensions on Laman Street will be considered in detailed design.
6	22-Sep-23	Cooks Hill	No	Notes the need for pedestrian improvements at the intersection. Questions whether other crossing treatments, such as a marked crossing or pedestrian refuge can be used. Concerns regarding reduction of on-street parking.	The provision of a refuge island on Auckland Street would require the removal of additional parking spaces to achieve the required sight distance. The provision of a marked (zebra) crossing could not be achieved without the inclusion of kerb extensions, as per the Australian Standard. The proposed kerb extensions have been placed close to the intersection to minimise parking loss. Only two parking spaces will be repurposed to achieve a narrow entry to Laman Street, to support a safer cycling environment and encourage lower vehicle speeds.
7	21-Sep-23	Cooks Hill	No	Consider provision of a raised crossing to reduce vehicle speeds. Concerns regarding reduction of on-street parking.	The provision of a raised (wombat) crossing on Auckland Street or Laman Street could not be achieved without the inclusion of kerb extensions, as per the Australian Standard. The proposed kerb extensions have been placed close to the intersection to minimise parking loss. Only two parking spaces will be repurposed to achieve a narrow entry to Laman Street, to support a safer cycling and pedestrian environment, and encourage lower vehicle speeds.
8	21-Sep-23	Belmont North	Yes	Supports the pedestrian improvements and traffic calming.	Noted
9	21-Sep-23	Cooks Hill	Yes, with changes	Supports the pedestrian improvements and traffic calming. Notes recent changes to provide	There are no proposed changes to disability parking spaces as part of this project.

				accessible on-street parking. Concerns regarding further reduction of on-street parking.	
10	21-Sep-23	The Hill	Yes	Supports the pedestrian improvements and traffic calming.	Noted
11	21-Sep-23	The Hill	Yes	Supports the pedestrian improvements and traffic calming, noting safety for children.	Noted
12	20-Sep-23	The Hill	Yes	Supports the pedestrian improvements and traffic calming. Consider provision of a marked crossing on Auckland Street. Consider further pedestrian improvements along Laman Street.	The kerb extensions on Auckland Street will be designed to accommodate future installation of a marked (zebra) crossing. Pedestrian counts will be completed post-construction to inform consideration of a marked crossing. Pedestrian improvements at the proposed kerb extensions on Laman Street will be considered in detailed design.
13	19-Sep-23	Waratah	Yes, with changes	Supports the pedestrian improvements and traffic calming. Consider provision of a crossing facility on Laman Street.	Pedestrian improvements at the proposed kerb extensions on Laman Street will be considered in detailed design.
14	17-Sep-23	Lambton	Yes	- <i>No comment provided</i>	
15	17-Sep-23	Islington	Yes, with changes	Supports the pedestrian improvements and traffic calming. Concerns regarding recent CN works in Dent Street, Islington.	Feedback has been passed onto the CN Asset Services team.
16	16-Sep-23	The Hill	Yes, with changes	Supports the pedestrian improvements and traffic calming. Consider tree planting along Laman Street.	Opportunities for tree planting will be considered during detailed design.
17	16-Sep-23	Hamilton	No	Concerns regarding reduction of on-street parking.	In 2021, Council adopted a Parking Plan which set a clear goal to manage parking to improve the amenity of our streets, support accessibility of our centres and to encourage mode shift to active and public transport. The proposed kerb extensions have been placed close to the intersection to minimise parking

					loss. Two parking spaces on the southern side of Laman Street will be repurposed to provide a narrowing to encourage slower vehicle speeds and support a safer cycling environment in the street.
18	15-Sep-23	Newcastle	Yes, with changes	Supports the pedestrian improvements and traffic calming, noting safety for children and older pedestrians. Consider provision of a marked crossing on Auckland Street.	The kerb extensions on Auckland Street will be designed to accommodate future installation of a marked (zebra) crossing. Pedestrian counts will be completed post-construction to inform consideration of a marked crossing.
19	15-Sep-23	Lambton	Yes	- <i>No comment provided</i>	
20	15-Sep-23	Islington	Yes, with changes	Supports the pedestrian improvements and traffic calming. Consider provision of a crossing facility on Laman Street, and reducing the speed limit to 30 km/h.	Pedestrian improvements at the proposed kerb extensions on Laman Street will be considered in detailed design. A self-explaining road is a traffic environment which elicits safe and consistent behaviour among road users simply by its design. Research has found that design speeds have a greater effect on driver speed choice than sign posted speed limits. The proposed narrowing is a traffic calming device that will encourage a lower speed environment in Laman Street.
21	15-Sep-23	Newcastle East	Yes	Supports the pedestrian improvements and traffic calming. Questions if there is a dedicated cycleway between the University of Newcastle Honeysuckle buildings and the Conservatorium.	The CN Cycle Plan identifies proposed cycleway upgrades along Auckland Street, Civic Lane and Worth Place. These upgrades are being investigated as part of the Civic Public Domain Plan and in conjunction with the proposed future stages of the University of Newcastle City Campus development.
22	14-Sep-23	New Lambton	No	Concerns regarding reduction of on-street parking. Consider making Laman Street a one-way street with 45 degree angled parking.	The proposed kerb extensions have been placed close to the intersection to minimise parking loss. Two parking spaces on the

					southern side of Laman Street will be repurposed to provide a narrowing to encourage slower vehicle speeds and support a safer cycling environment in the street.
23	14-Sep-23	Red House Farm - Newcastle upon Tyne	Yes	Supports the pedestrian improvements and traffic calming. Supports introduction of low-traffic neighbourhoods.	Noted
24	14-Sep-23	Cooks Hill	No	Concerns that kerb extensions create a hazard for cyclists, and concerns about cyclists riding on footpaths. Concerns regarding reduction of on-street parking.	The proposed kerb extension widths are the same width as the existing kerb extensions at the raised crossing on Laman Street between John Street and Hunnifords Lane. Cycling along Laman Street is very popular, and CN has not received complaints about the existing kerb extensions posing a hazard to cyclists. The proposed narrowing will support a cycling quiet way along Laman Street. Quiet ways aim to create a low-stress on-road experience for cyclists, with low speeds and volumes of motorised traffic. CN is currently progressing detailed design of revised traffic calming and gateway entry treatments to the Darby Street 30 km/h high pedestrian activity area to address community concerns.
25	14-Sep-23	Adamstown	Yes, with changes	Supports the pedestrian improvements and traffic calming, however, concerned Police are not enforcing road rules for cyclists.	Support for the project is noted. Concerns about enforcement of the road rules is a matter for NSW Police.
26	14-Sep-23	Maryland	No	Questions if CN is considering restoring two-way traffic in Laman Street east of Dawson Street.	Changes to the existing one-way westbound traffic in Laman Street between Darby Street and Dawson Street is not proposed as part of this project. Changing the traffic to one-way eastbound will be reviewed as part of the

					upcoming Civic Public Domain Plan and Cooks Hill LATM review. Community consultation will be undertaken as part of any proposed traffic changes.
27	14-Sep-23	Merewether	Yes	- <i>No comment provided</i>	
28	14-Sep-23	The Hill	No	Concerned Laman Street has become a rat-run for westbound traffic from Darby Street. Concerned the proposal is difficult to navigate for vehicles, cyclists and pedestrians. Consider a raised pedestrian crossing on Laman Street and reduce speed limit to 30 km/h.	During feasibility investigations it was determined that a road narrowing is the preferred option to minimise potential noise and vibration impacts on the adjacent Newcastle Conservatorium of Music. A self-explaining road is a traffic environment which elicits safe and consistent behaviour among road users simply by its design. Research has found that design speeds have a greater effect on driver speed choice than sign posted speed limits. The proposed narrowing is a traffic calming device that will encourage a lower speed environment in Laman Street.
29	14-Sep-23	The Hill	Yes, with changes	Consider resurfacing footpaths.	Feedback has been passed onto the CN Asset Services team.
30	14-Sep-23	Newcastle East	Yes	Supports the pedestrian improvements and traffic calming. Consider planting out the kerb extensions, not using turf.	Opportunities for tree planting and landscaping will be considered during detailed design.
31	14-Sep-23	Cooks Hill	Yes	Supports the pedestrian improvements and traffic calming. Consider improvements to intersection of Laman Street and Dawson Street to highlight vehicle priority.	Improvements to the intersection of Laman Street and Dawson Street are out of scope for this project. Improvements will be considered as part of the upcoming review of the Cooks Hill LATM plan.
32	14-Sep-23	Wickham	Yes	- <i>No comment provided</i>	

33	14-Sep-23	Newcastle West	No	Concerns regarding reduction of on-street parking.	In 2021, Council adopted a Parking Plan which set a clear goal to manage parking to improve the amenity of our streets, support accessibility of our centres and to encourage mode shift to active and public transport. The proposed kerb extensions have been placed close to the intersection to minimise parking loss. Two parking spaces on the southern side of Laman Street will be repurposed to provide a narrowing to encourage slower vehicle speeds and support a safer cycling environment in the street.
34	14-Sep-23	Mayfield	No	Concerns the intersection is too wide and the proposal may increase in vehicle, cyclist and pedestrian traffic. Consider planting more trees.	The proposed kerb extensions will improve pedestrian connectivity by narrowing the crossing distance, while maintaining access for vehicles and turning movements. Width is consistent with other treatments on Laman Street.
35	14-Sep-23	Newcastle	No	Concerns regarding cyclists, micromobility users, and skateboarders using footpaths. Concerned recent Darby Street traffic calming changes has increased vehicle volumes on Laman Street.	NSW Road Rule no. 250 - <i>Riding on a footpath or shared path</i> - outlines who may ride a bicycle on a footpath, and this rule apply across the entire state of NSW. CN conducted a six-month Streets as Shared Spaces (SaSS) trial in Darby Street from 29 September 2022 to 28 February 2023. The trial which was funded by the NSW Government, aimed to improve the outdoor dining experience as well as safety and accessibility for pedestrians and cyclists. On 28 March 2023, the elected Council reviewed the outcomes of the trial and approved the retention of infrastructure that was installed as part of the trial, including the outdoor dining deck and associated traffic calming devices.

					CN is currently progressing detailed design of revised traffic calming and gateway entry treatments to the Darby Street 30 km/h high pedestrian activity area to address community concerns.
36	14-Sep-23	Cooks Hill	No	Concerns proposal will not effectively reduce vehicle speeds, and regarding reduction of on- street parking.	The proposed kerb extensions have been placed close to the intersection to minimise parking loss. Two parking spaces on the southern side of Laman Street will be repurposed to provide a narrowing to encourage slower vehicle speeds and support a safer cycling environment in the street.
37	14-Sep-23	Mayfield	Yes	- <i>No comment provided</i>	
38	13-Sep-23	Adamstown Heights	Yes, with changes	Supports the pedestrian improvements and traffic calming. Consider provision of a crossing facility on Laman Street, noting it is a popular place to cross.	Pedestrian improvements at the proposed kerb extensions on Laman Street will be considered in detailed design. The kerb extensions on Auckland Street will be designed to accommodate future installation of a marked (zebra) crossing. Pedestrian counts will be completed post-construction to inform consideration of a marked crossing.
39	9-Sep-23	Cooks Hill	Yes	Supports the pedestrian improvements and traffic calming.	Noted
40	7-Sep-23	Cooks Hill	Yes	Supports the pedestrian improvements and traffic calming.	Noted
41	6-Sep-23	Merewether	Yes, with changes	Supports the pedestrian improvements and traffic calming. Consider changing vehicle priority at the intersection. Concerned the proposal will not effectively reduce vehicle speeds.	The proposed kerb extensions will improve visibility at the intersection. The installation of give way line marking and signage will be considered as part of detailed design.
42	6-Sep-23	Cooks Hill	Yes	- <i>No comment provided</i>	

43	6-Sep-23	Adamstown	Yes, with changes	Supports the pedestrian improvements and traffic calming. Consider provision of a crossing facility on Laman Street. Consider a raised crossing on Auckland Street, noting the need to prioritise people over vehicles on local streets and in civic spaces.	Pedestrian improvements at the proposed kerb extensions on Laman Street will be considered in detailed design. The kerb extensions on Auckland Street will be designed to accommodate future installation of a marked (zebra) crossing. Pedestrian counts will be completed post-construction to inform consideration of a marked crossing.
44	6-Sep-23	Cooks Hill	No	Notes vehicles speed along Laman Street and support for measures to calm traffic. Concerned recent Darby Street traffic calming changes has increased vehicle volumes on Laman Street, and that the proposed changes will displace rat-runners to other local streets unless they also receive traffic calming treatments. Concerns regarding reduction of on-street parking.	CN will monitor the effectiveness of the kerb extension treatments and any changes to traffic conditions in the local area post implementation. Further traffic calming improvements will be considered as part of the upcoming review of the Cooks Hill LATM plan and ongoing development of the PPN and Walking and Mobility Plan. The proposed kerb extensions have been placed close to the intersection to minimise parking loss. Two parking spaces on the southern side of Laman Street will be repurposed to provide a narrowing to encourage slower vehicle speeds and support a safer cycling environment in the street.
45	5-Sep-23	Cooks Hill	Yes, with changes	Supports the pedestrian improvements and traffic calming, noting the current conditions are unsafe for pedestrians. Consider provision of a marked crossing on both Auckland Street and Laman Street. Notes on-street parking will be reduced.	Pedestrian improvements at the proposed kerb extensions on Laman Street will be considered in detailed design. The kerb extensions on Auckland Street will be designed to accommodate future installation of a marked (zebra) crossing. Pedestrian counts will be completed post-construction to inform consideration of a marked crossing. The proposed kerb extensions have been placed close to the intersection to minimise

					parking loss. Two parking spaces on the southern side of Laman Street will be repurposed to provide a narrowing to encourage slower vehicle speeds and support a safer cycling environment in the street.
46	5-Sep-23	Cooks Hill	No	Supports pedestrian improvements and traffic calming, however, is concerned the proposal will reduce on-street parking. Notes footpaths and gardens in the area require maintenance work.	The provision of a marked (zebra) crossing could not be achieved without the inclusion of kerb extensions, as per the Australian Standard. The proposed kerb extensions have been placed close to the intersection to minimise parking loss. Only two parking spaces will be repurposed to achieve a narrow entry to Laman Street, to support a safer cycling environment and encourage lower vehicle speeds.
47	5-Sep-23	Cooks Hill	Yes	Consider additional landscaped kerb extensions along Laman Street.	Further traffic calming improvements will be considered as part of the upcoming review of the Cooks Hill LATM plan and ongoing development of the PPN and Walking and Mobility Plan.
48	4-Sep-23	Cooks Hill	Yes, with changes	Questions the location of the cycleway. Consider additional traffic calming measures to further reduce vehicle speeds, noting Laman Street is a rat run.	The proposed kerb extension widths are the same width as the existing kerb extensions at the raised crossing on Laman Street between John Street and Hunnifords Lane. The proposed narrowing will support a cycling quiet way along Laman Street. Quiet ways aim to create a low-stress on-road experience for cyclists, with low speeds and volumes of motorised traffic. Further traffic calming improvements will be considered as part of the upcoming review of the Cooks Hill LATM plan and ongoing development of the PPN and Walking and Mobility Plan.

49	4-Sep-23	Cooks Hill	Yes, with changes	Consider closing Laman Street to vehicles between Darby Street and Dawson Street, noting it has become a rat run to avoid the traffic signals on King Street. Consider improving the existing raised crossing to more effectively reduce vehicle speeds. Concerned the landscaped islands at the existing raised crossing require maintenance to improve visibility for pedestrians.	Changes to Laman Street between Darby Street and Dawson Street is not proposed as part of this project. This section of Laman Street will be reviewed as part of the upcoming Civic Public Domain Plan and Cooks Hill LATM review. Community consultation will be undertaken as part of any proposed traffic changes.
50	4-Sep-23	Cooks Hill	No	Supports pedestrian improvements and traffic calming at Auckland Street, however, is concerned the kerb extension on the southern side of Laman Street will reduce on-street parking.	The proposed kerb extensions have been placed close to the intersection to minimise parking loss. Two parking spaces on the southern side of Laman Street will be repurposed to provide a narrowing to encourage slower vehicle speeds and support a safer cycling environment in the street.
51	4-Sep-23	Cooks Hill	Yes, with changes	Concerns that kerb extensions create a hazard for cyclists. Consider reducing speed limit to 30 km/h.	<p>The proposed kerb extension widths are the same width as the existing kerb extensions at the raised crossing on Laman Street between John Street and Hunnifords Lane. Cycling along Laman Street is very popular, and CN has not received complaints about the existing kerb extensions posing a hazard to cyclists.</p> <p>The proposed narrowing will support a cycling quiet way along Laman Street. Quiet ways aim to create a low-stress on-road experience for cyclists, with low speeds and volumes of motorised traffic. A self-explaining road is a traffic environment which elicits safe and consistent behaviour among road users simply by its design. Research has found that design speeds have a greater effect on driver</p>

					speed choice than sign posted speed limits. The proposed narrowing is a traffic calming device that will encourage a lower speed environment in Laman Street.
52	31-Aug-23	Cooks Hill	No	Concerns regarding reduction of on-street parking.	The proposed kerb extensions have been placed close to the intersection to minimise parking loss. Two parking spaces on the southern side of Laman Street will be repurposed to provide a narrowing to encourage slower vehicle speeds and support a safer cycling environment in the street.
53	31-Aug-23	Fletcher	Yes	- <i>No comment provided</i>	
54	30-Aug-23	New Lambton	Yes, with changes	Consider introducing additional traffic calming measures to reduce vehicle speeds to 30 km/h in Laman Street.	The proposed narrowing will support a cycling quiet way along Laman Street. Quiet ways aim to create a low-stress on-road experience for cyclists, with low speeds and volumes of motorised traffic. A self-explaining road is a traffic environment which elicits safe and consistent behaviour among road users simply by its design. Research has found that design speeds have a greater effect on driver speed choice than sign posted speed limits. The proposed narrowing is a traffic calming device that will encourage a lower speed environment in Laman Street.
55	30-Aug-23	Mayfield	Yes	Questions if a give way sign will be installed at Auckland Street.	The installation of give way line marking and signage will be considered as part of detailed design.
56	27-Aug-23	Birmingham Gardens	No	Concerned about the expense of the project.	This project has been identified and developed based on community needs and it is working towards achieving strategic outcomes as outlined in our Transport Strategy

57	24-Sep-23	Not Supplied		Supports pedestrian improvements and traffic calming, however, concerns regarding reduction of on-street parking.	The proposed kerb extensions have been placed close to the intersection to minimise parking loss. Two parking spaces on the southern side of Laman Street will be repurposed to provide a narrowing to encourage slower vehicle speeds and support a safer cycling environment in the street.
58	31-Aug-23	Cooks Hill		Supports the pedestrian improvements and traffic calming, noting the current conditions are unsafe for pedestrians, noting young and older pedestrians cross here. Concerned recent Darby Street traffic calming changes has increased vehicle volumes on Laman Street. Consider provision of a marked crossing on both Auckland Street and Laman Street, additional traffic calming devices and reduce the speed limit to 30 km/h.	Pedestrian improvements at the proposed kerb extensions on Laman Street will be considered in detailed design. The kerb extensions on Auckland Street will be designed to accommodate future installation of a marked (zebra) crossing. Pedestrian counts will be completed post-construction to inform consideration of a marked crossing. A self-explaining road is a traffic environment which elicits safe and consistent behaviour among road users simply by its design. Research has found that design speeds have a greater effect on driver speed choice than sign posted speed limits. The proposed narrowing is a traffic calming device that will encourage a lower speed environment in Laman Street.
59	12-Sep-23	Not Supplied		Concerned some existing landscaped LATM devices in Laman Street require maintenance. Concerns regarding reduction of on-street parking and that the proposal will not effectively reduce vehicle speeds. Questions how the proposal will improve safety for cyclists. Consider providing on-street disability parking in Laman Street.	Opportunities for tree planting and landscaping will be considered during detailed design. CN will monitor the effectiveness of the kerb extension treatments and any changes to traffic conditions in the local area post implementation. Further traffic calming improvements will be considered as part of the upcoming review of the Cooks Hill LATM

					plan and ongoing development of the PPN and Walking and Mobility Plan. There are no proposed changes to disability parking spaces as part of this project.
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ATTACHMENTS DISTRIBUTED UNDER SEPARATE COVER

CCL 28/11/2023 - EXECUTIVE MONTHLY PERFORMANCE REPORT

8.12 Attachment A: EXECUTIVE MONTHLY
 PERFORMANCE REPORT -
 OCTOBER 2023

DISTRIBUTED UNDER SEPARATE COVER

Monthly Performance Report

October 2023






Income Statement

Result for the financial period ending 31 October 2023

Full Year Budget	YTD Budget	YTD Actual Result	Variance (\$)	Variance (%)
\$'000	\$'000	\$'000	\$'000	\$'000
Income from continuing operations				
219,236 Rates & annual charges	73,079	73,079	-	0%
117,640 User charges & fees	37,821	38,945	1,124	3%
9,341 Other revenues	3,020	3,525	506	17%
21,220 Grants & contributions - operating	1,942	1,911	(31)	-2%
38,626 Grants & contributions - capital	4,528	4,528	-	0%
9,315 Interest & investment revenue	3,105	4,718	1,613	52%
7,089 Other income	2,495	403	(2,092)	-84%
Total income from continuing operations				
422,467	125,990	127,109	1,119	1%
Expenses from continuing operations				
131,648 Employee benefits & on-costs	42,401	41,670	(731)	-2%
112,483 Materials & services	32,308	29,995	(2,313)	-7%
4,039 Borrowing costs	1,351	1,363	12	1%
69,601 Depreciation & amortisation	23,774	23,654	(120)	-1%
52,147 Other expenses	16,538	16,896	358	2%
7,002 Net loss from the disposal of assets	1,897	1,475	(422)	-22%
Total expenses from continuing operations				
376,920	118,269	115,054	(3,215)	-3%
Operating result from continuing operations				
45,547	7,721	12,055	4,334	56%
Net operating result before grants & contributions - capital				
6,921	3,193	7,526	4,334	136%

Operating Analysis as at 31 October 2023







Over budget by more than 5%	
Over budget by 5% or less	
Result within budget	

Financial Statement Line Item	Indicator	Var (\$'000)	Var(%)	Issue	Explanation
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Operating Revenue

Rates & annual charges		-	0%		
User charges & fees		1,124	3%		\$0.6m increase in revenue related to additional Commercial Tonnes from Summerhill Waste Management Centre \$0.4m increase in regulatory fees predominantly related to one-off revenue from Occupation and Construction certificates relating to timing under Accounting Standards \$0.3m increase in revenue related to Civic shows
Other revenues		506	17%		\$0.4m proceeds from Insurance Claims \$0.2m increase in Food & Beverage sales from Newcastle Venues
Grants & contributions - operating		(31)	-2%		Related to timing of grant payments
Grants & contributions - capital		-	0%		
Interest & investment revenue		1,613	52%		Increased return from investments
Other income		(2,092)	-84%		\$2.1m Unfavourable Fair Value adjustment in TCorp Long Term Growth Fund

Operating Expenses

Employee benefits & on-costs		(731)	-2%		
Materials & services		(2,313)	-7%		\$3.0m saving related to the timing of delivery within the Works Program \$0.3m additional spend related to timing of City Events \$0.1m increase in electricity costs related to Street Lighting
Borrowing costs		12	1%		
Depreciation & Amortisation		(120)	-1%		
Other expenses		358	2%		\$0.5m increase in State Waste Levy related to additional tonnes at Summerhill Waste Management Centre partially offset by timing of donations expenditure
Net loss from the disposal of assets		(422)	-22%		Due to timing of budgeted disposals

Capital Statement

Result for the financial period ending 31 October 2023

Full Year Budget \$'000	YTD Budget \$'000	YTD Actual Result \$'000	Variance (\$) \$'000	Variance (%) \$'000
Capital funding				
70,107 General fund contribution to capital	23,817	27,730	3,912	16%
2,410 Stormwater Management Service Charge	803	803	-	0%
32,127 Capital Grants & Contributions	4,528	4,528	-	0%
719 Proceeds from the sale of assets	12	12	-	0%
(3,339) Net Loans Borrowings / (Repayments)	(1,113)	(1,113)	-	0%
Funding available for capital				
102,024 expenditure	28,048	31,960	3,912	14%
Capital Expenditure				
42,956 Asset Renewal	10,630	8,282	(2,348)	-22%
41,764 New / Upgrade	14,533	10,714	(3,819)	-26%
84,720 Total capital expenditure	25,163	18,996	(6,167)	-25%
17,304 Transfer to or (Draw down on) reserves	2,885	12,964	10,079	349%

Debtors Report as at 31 October 2023

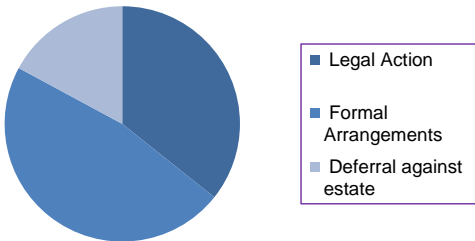
Outstanding Rates

Debt Recovery Action	No. of Properties	\$ Amount
Legal Action	123	997,604
Formal Arrangements	452	1,317,827
Deferral against estate	17	478,308
Total	592	2,793,739

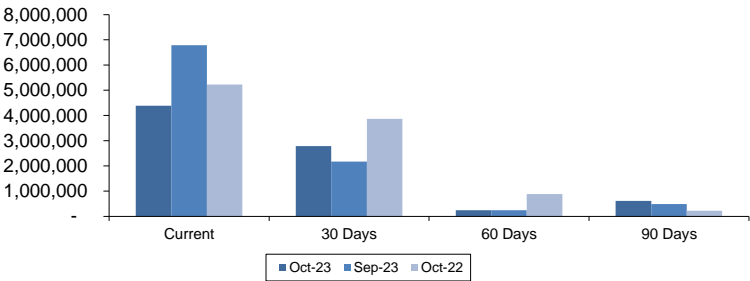
Aged Debtors Report (Major Debtors Report)

Period	Oct-23 \$	Sep-23 \$	Oct-22 \$
Current	4,394,094	6,791,010	5,224,150
30 Days	2,783,154	2,168,906	3,868,373
60 Days	247,355	241,463	884,675
90 Days	614,329	487,443	230,249
Total	8,038,932	9,688,822	10,207,447

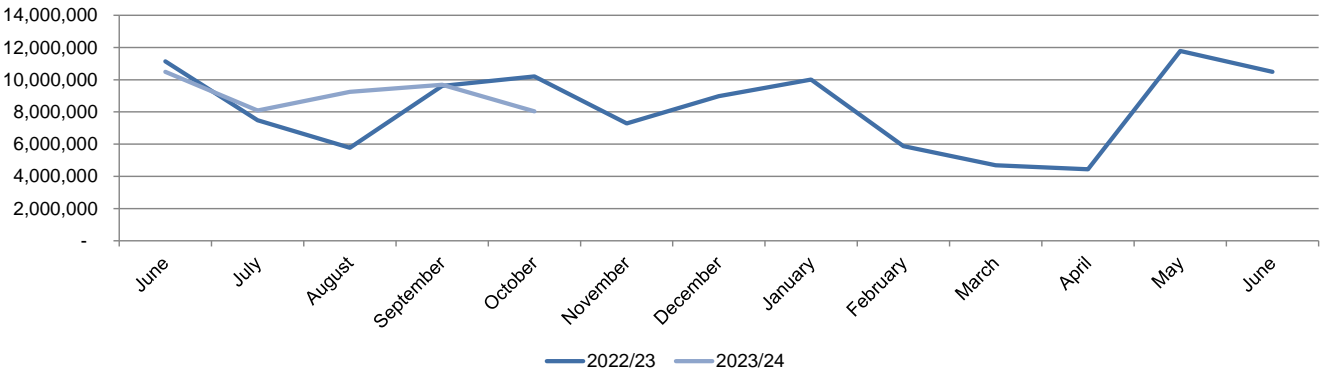
Outstanding Rates (\$)



Debtors balances



Trend of Debtors Balance (\$)



Works Program Summary

Result for the financial period ending 31 October 2023

Full Year Budget \$'000	Portfolio/Program	YTD Budget \$'000	YTD Actual Result \$'000	Variance to YTD budget (%) \$'000	% of FY Budget Spent \$'000
35,313	City Infrastructure - Assets & Facilities	9,554	6,129	-36%	17%
260	Buildings - Council Support Services	73	259	256%	100%
150	Public Toilets	42	-	-100%	0%
140	Retaining walls	34	185	436%	132%
7,780	Bridges	1,217	185	-85%	2%
1,160	Footpaths	324	622	92%	54%
1,983	Roadside Furniture	318	224	-30%	11%
7,240	Road Rehabilitation	3,507	711	-80%	10%
7,500	Road Resurfacing	2,294	2,550	11%	34%
300	Parking Infrastructure	84	-	-100%	0%
4,975	Stormwater System	1,123	1,308	16%	26%
3,825	Fleet Replacement	536	83	-84%	2%
5,000	Planning & Environment - Transport	1,754	1,321	-25%	26%
3,610	Cycleways	1,315	392	-70%	11%
250	Pedestrian Access and Mobility Plan (PAMP)	44	533	1120%	213%
1,140	Local Area Traffic Management (LATM)	395	396	0%	35%
13,458	Planning & Environment - Environment & Sustainability	3,848	1,500	-61%	11%
1,050	Blackbutt Reserve	294	58	-80%	6%
254	Flood Planning	71	67	-6%	26%
8,009	Coast, Estuary and Wetlands	2,495	519	-79%	6%
1,810	Bushland and Watercourses	472	174	-63%	10%
1,660	Street and Park Trees	403	454	13%	27%
675	Sustainability & Climate	114	228	101%	34%
6,400	Corporate Services	1,686	1,141	-32%	18%
800	Digital Enablement	193	68	-65%	9%
5,600	Core Systems Development & Maintenance	1,493	1,072	-28%	19%
41,160	City Shaping	12,345	7,436	-40%	18%
36,500	Citywide	10,877	6,890	-37%	19%
4,660	Summerhill	1,468	546	-63%	12%
22,334	Creative & Community Services	6,388	6,039	-5%	27%
7,450	Aquatics	3,581	3,041	-15%	41%
349	Community Buildings	98	112	15%	32%
920	Civic Venues / Civic Services	257	270	5%	29%
12,245	Recreation & Sport	2,072	2,328	12%	19%
475	Economic Development	130	30	-77%	6%
150	Art Gallery	42	2	-95%	1%
745	Museum / Libraries / Historic Fort Scratchley	208	256	23%	34%
4,375	Waste Services	2,277	4,844	113%	111%
4,375	Waste Management	2,277	4,844	113%	111%
9,216	City Infrastructure - Revitalisation	2,539	2,407	-5%	26%
5,620	City Centre	1,487	755	-49%	13%
2,300	Coastal	582	623	7%	27%
1,296	Urban Centres	470	1,029	119%	79%
137,255	Total Works Program	40,391	30,817	-24%	22%

Note: The Budget above is inclusive of operational and capital works

Councillors' Expense Register 2023/2024

	Annual Budget Allotments									Council Term Budget Allotments		
	EVENTS	ACCOMPANYING PERSON (Official Business)	OVERSEAS TRAVEL	PROFESSIONAL DEVELOPMENT	LGNSW / NGA ANNUAL CONFERENCE	COMMUNICATION EXPENSES	SPECIAL REQUIREMENTS & CARER EXPENSES	HOME OFFICE EXPENSES	TOTAL ANNUAL EXPENDITURE	AICD COURSE FEES	COMMUNICATION DEVICES	TOTAL TERM EXPENDITURE
LORD MAYOR												
Policy Provision	\$4,000	\$1,000	Paid in accordance with a specific resolution of Council	\$5,000	\$20,000 (shared among elected representatives inclusive of both events)	\$3,000	\$6,000	\$2,000		\$4,000 (may be combined with Professional Development expenses in the year undertaken)	\$4,000	
NELMES Nuatali	577.59	-	-	-	-	323.64	-	-	901.23	563.64	4,378.21	4,941.85
ALL COUNCILLORS												
Policy Provision	\$2,000	\$500	See Above	\$5,000	See Above	\$3,000	\$6,000	\$500		\$4,000	\$4,000	
CLAUSEN, Declan	363.27	-	-	-	-	263.64	-	-	626.91	-	2,537.85	2,537.85
CHURCH, John	-	-	-	-	-	83.64	-	-	83.64	-	2,428.59	2,428.59
MACKENZIE, John	-	-	-	-	-	294.56	-	-	294.56	-	3,219.67	3,219.67
DUNCAN, Carol	-	-	-	500.00	-	294.56	-	-	794.56	563.64	3,392.58	3,956.22
BARRIE, Jenny	122.73	-	-	210.00	-	294.56	-	-	627.29	-	3,219.67	3,219.67
McCABE, Charlotte	-	-	-	-	-	294.56	-	-	294.56	-	3,047.73	3,047.73
WINNEY-BAARTZ, Peta	-	-	-	420.00	-	294.56	-	-	714.56	-	3,219.67	3,219.67
WOOD, Margaret	-	-	-	-	-	294.56	-	-	294.56	-	3,219.67	3,219.67
WARK, Katrina	1,349.26	-	-	-	-	294.56	-	-	1,643.82	-	3,219.67	3,219.67
RICHARDSON, Deahnna	79.39	-	-	-	-	294.56	-	-	373.95	4,000.00	3,646.94	7,646.94
ADAMCZYK, Elizabeth	177.27	-	-	-	-	294.56	-	-	471.83	-	3,646.03	3,646.03
PULL, Callum	79.39	-	-	-	-	294.56	-	-	373.95	-	3,219.67	3,219.67
TOTAL (exc LM)	2,171.31	-	-	1,130.00	-	3,292.88	-	-	6,594.19	4,563.64	38,017.74	42,581.38
TOTAL (inc LM)	2,748.90	-	-	1,130.00	-	3,616.52	-	-	7,495.42	5,127.28	42,395.95	47,523.23

CEO and Lord Mayor Offices Expenses

	YTD Budget \$'000	YTD Actual \$'000
Employee costs	395	394
Materials & contracts	81	74
Other operating expenses	0	0
Total Operating Expenses	475	468

Ward 4

At 31 October 2023



City of
Newcastle

Wallsend Capital Works Update

Wallsend Local Centre Public Domain Plan

- Phase 1 survey engagement explored the community's views on urban design themes, safety and order of priority for future revitalisation stages. The final report is available on our website at: <https://haveyoursay.newcastle.nsw.gov.au/wallsend-engagement-hub>
- Phase 2 of this engagement will include ongoing targeted community consultation and Placemaking engagement activities for individual project stages as they progress.

Stage 3: Detail design and construction of the intersection of Cowper and Kokera Streets, including:

Stage 3A:

- Flood assessment and modelling completed, and report submitted for review by CN.
- Detailed design tender scheduled for release in November 2023, with the detailed design process to progress through 2023/24.

Stage 3A: Ironbark creek widening and realignment, Cowper Street culvert bypass

Stage 3B: Installation of traffic signals at the intersection of Cowper and Kokera Streets

Stage 3B:

- Detailed design has been approved by Transport for NSW (TfNSW).

Stage 4: Detailed design of Boscawen Street and Nelson Street Bridge replacement works

Boscawen Street Bridge:

- Construction commencement scheduled in 2024.

Nelson Street Bridge:

- 100% detail design submitted and reviewed by internal stakeholders.
- Construction tender scheduled in 2024/25.

Stage 5: Detailed design of: Traffic lights and shared path at Nelson Street and Cowper Street intersection

- Detailed design and construction of Stage 5 will follow the construction of Stages 3A and 3B.

Proposed roundabout at the intersection of Cowper Street and Newcastle Road

Wallsend Active Hub amenities

- Building installation is complete and sustained vandalism ahead of opening requiring repairs.
- Opening planned for mid-November 2023.

Investment Policy compliance report

October 2023

Executive summary:

1 **Socially Responsible Investment:**

Application of the investment function has remained consistent with requirements outlined within Part E of CN's Investment Policy, "*Environmentally and Socially Responsible Investments (SRI)*".

2 **Portfolio holdings:**

As at the end of October 2023 CN's overall investment portfolio holdings are \$380.6 million. These holdings are split between Income producing/defensive and Capital Growth asset classes.

3 **Performance commentary – Income producing/Defensive funds:**

As at the end of October 2023 CN's allocation to the income producing/defensive assets sat at 88%. The 1-month annualised yield of these assets was 4.04% as at 31 October 2023.

Our expectation remains unchanged that in the short term the steep upward movement in interest rates witnessed since early in calendar year 2022 will challenge CNs ability to exceed the performance of the benchmark.

This challenge will continue until such time as existing long-term fixed investments placed prior to calendar year end 2021 begin to mature and/or the RBA Cash Rate declines.

4 **Performance commentary – Capital growth funds:**

For the month of October 2023, the TCorp Fund posted a fair value decrement, being a negative return of (1.64%).

The October performance of the TCorp Fund was reflective of a tough month for risk assets both globally and in Australia, with financial markets reacting to expectations that central banks could keep interest rates higher for longer than previously envisaged.

Highlighting this expectation of higher interest rates for longer were comments made by the RBA Governor Bullock to the Federal Senate Economics Legislation Committee on 26 October "*While goods price inflation is easing quite a lot as supply issues unwind, consistently we're seeing that, although services inflation is declining, it's still higher than what we're comfortable with, and it's also reasonably persistent.*"

The message of probable higher interest rates for longer caused equities to fall during the month with the MSCI World Index (excluding Australia) falling (2.92%) and the Australian S&P ASX 200 Accumulation Index also falling (3.78%).

Since TCorp first created the Long-Term Growth Fund in 2006, the Fund has proven to be relatively resilient with the ability to recover fair value losses over time following broader global equity downturns.

Short term performance of the TCorp Growth fund should continue to be viewed cautiously given the high volatility associated with a diversified growth fund such as these which mandate allocations to domestic and international shares.

CN remains confident in the long-term strategic rationale that supports this investment, and therefore our strategy as a long-term holder remains unchanged.

Investment Policy compliance report

October 2023

5 **Risk management compliance:**

CN's temporary surplus funds are invested consistent with its adopted Investment and Borrowing Policy and The Local Government Act and Regulations.

Actual performance against CN's Policy limits is disclosed later in this report.

6 **New and matured investments:**

New investments placed during October 2023 continued to focus on meeting the objectives outlined in CN's Investment and Borrowing Policy.

Further disclosure of investment portfolio composition and details of any investment placements or maturities during the reporting period are detailed later in this report.

7 **Income producing/Defensive funds – Actual v Budget:**

Cumulative Net returns to October 2023 from the Income producing/Defensive funds totalled \$4.56m against a budget of \$3.01m (excluding Newcastle Airport and non-investment portfolio sources of interest). This resulted in Interest and Investment income outperforming the budget by \$1.55m for the financial year to date.

From the commencement of 2023, when the interest income budget was set, to now, the RBA Cash Rate has continued to reset higher than budget formulated assumptions. It is probable that this will result in an upward revision to the full year budget at a future quarterly review.

8 **Capital Growth Funds – Actual v Budget:**

Net returns to October 2023 from the Capital Growth Fund totalled a cumulative FYTD decrement of (\$1.21m) against a budgeted increment of \$0.93m. This resulted in net returns underperforming budget by (\$2.14m) for the financial year to date.

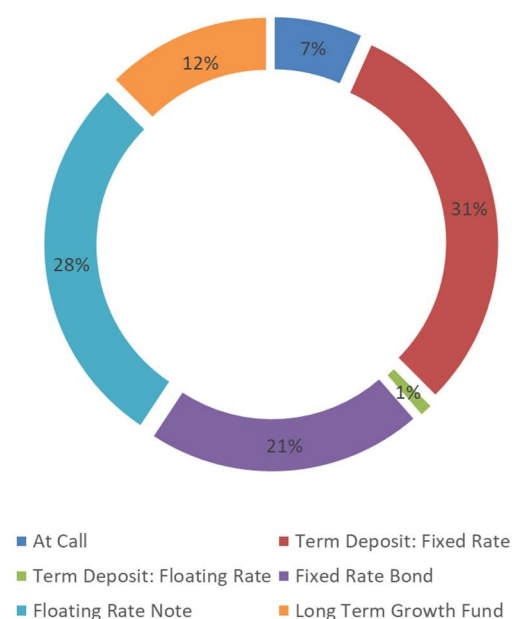
Ongoing performance will continue to be closely monitored and any possible adjustments made to the budget will be carefully assessed and recommended if deemed appropriate.

Investment Policy compliance report

October 2023

Portfolio holdings:

Asset Class allocation		
Investment Category	Investment type	CN exposure (\$'000)
Income producing / Defensive	Cash At Call	25,524
	Term Deposit: Fixed rate	117,279
	Term Deposit: Floating rate	4,000
	Floating Rate Note	107,600
	Fixed Rate Bond	78,842
Capital Growth	Long Term Growth Fund	47,305
Total		380,550



Performance:

Income producing/Defensive category*:

	3 year (% p.a.)	1 year %	3 months %	FYTD %	1 month %	1 month annualised (% p.a.)
CN's return#	2.06%	3.53%	0.99%	1.31%	0.33%	4.04%
Performance objective^	1.47%	3.66%	1.04%	1.41%	0.33%	4.09%
Excess return	0.59%	(0.13%)	(0.05%)	(0.10%)	0.00%	(0.05%)

* Exclusive of Capital Growth (disclosed separately below).

^ CN's Performance objective is set at the Ausbond Bank Bill Index.

Cash at Call funds have been included in the calculation of CN's reported investment portfolio performance from January 2021. This change has not been applied retrospectively to historical months.

Capital Growth category*:

	3 year (% p.a.)	1 year %	3 months %	FYTD %	1 month %
CN's return	4.67%	4.27%	(4.18%)	(2.49%)	(1.64%)
Performance objective^	7.91%	8.83%	1.75%	1.75%	0.58%
Excess return	(3.24%)	(4.56%)	(5.93%)	(4.24%)	(2.22%)

	Return since Inception#
CN's return	10.01%

* Capital Growth category consists solely of CN's exposure to TCorp Individually Managed Growth Funds.

^ CN's Performance objective is set at CPI + 3.5% p.a. (over a rolling 10yrs). Prior months performance objective used in place of current month (not available at time of preparation).

Return since inception considers the month end dollar value of the investment against CN's capital contributions since inception. Initial investment into the Capital Growth category occurred in February 2019 with incremental contributions thereafter.

Investment Policy compliance report

October 2023

Risk Management compliance:

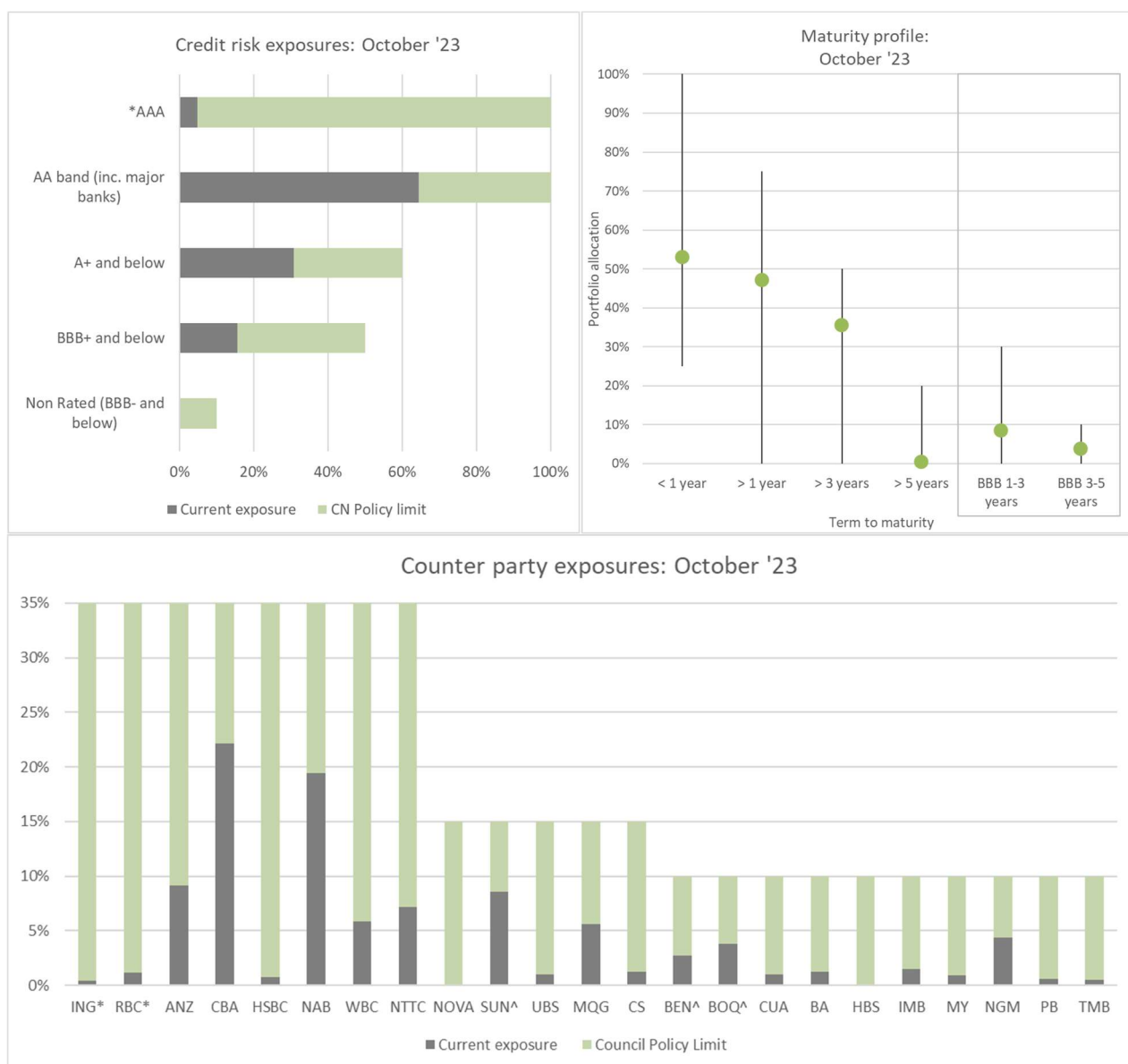
Portfolio exposure:

Investment category	Minimum exposure	Maximum exposure	CN exposure
Income producing / Defensive	80%	100%	88%
Capital Growth [^]	0%	30%	12%

[^] Capital Growth category consists solely of CN's exposure to TCorp Individually Managed Growth Funds.

Income producing/Defensive risk limits:

The below risk limits apply only to the Income producing/Defensive category of CN's investment portfolio.



*Where there is an Asterisk marked in the above graphs all of CN's investments in this category are assigned a AAA rating due to additional credit support of the investment class.

[^] = Total exposures to Suncorp Bank, Bank of Queensland, and Bendigo Adelaide Bank include a component of AAA rated individual investments.

Investment Policy compliance report

October 2023

New and matured Investments:

New Investments:

Contract date	Settlement date	Institution	Long Term Credit rating (S&P)	Asset Class	Face value	Rate of Return	Term	Maturity date
5 Oct '23	5 Oct '23	NAB	AA-	Term Deposit: Fixed rate	\$3,500,000	5.15%	187days	9 Apr '24
18 Oct '23	18 Oct '23	NAB	AA-	Term Deposit: Fixed rate	\$30,000,000	4.93%	90days	16 Jan '24
30 Oct '23	30 Oct '23	Suncorp Bank	A+	Term Deposit: Fixed rate	\$10,000,000	5.23%	163days	10 Apr '24

Matured Investments:

Date matured	Institution	Asset Class	Face value	Rate of Return	Original Term	Original date invested
3 Oct '23	CBA	Term Deposit: Fixed rate	\$7,000,000	5.12%	97days	28 Jun '23
5 Oct '23	NAB	Term Deposit: Fixed rate	\$3,500,000	5.18%	97days	30 Jun '23
9 Oct '23	NAB	Term Deposit: Fixed rate	\$3,000,000	0.70%	2.6years	8 Mar '21
11 Oct '23	IMB	Term Deposit: Fixed rate	\$4,000,000	5.24%	113days	20 Jun '23
18 Oct '23	NGM Group	Term Deposit: Fixed rate	\$10,000,000	5.30%	91days	19 Jul '23
18 Oct '23	Heritage and People's Choice	Term Deposit: Fixed rate	\$10,000,000	5.20%	91days	19 Jul '23
18 Oct '23	Suncorp Bank	Term Deposit: Fixed rate	\$10,000,000	5.10%	91days	19 Jul '23
26 Oct '23	NAB	Term Deposit: Fixed rate	\$7,000,000	5.21%	120days	28 Jun '23

I certify that the new investments detailed above have been made in accordance with the Local Government Act 1993, the Local Government (General) Regulation 2005, and Council's adopted Investment Policy.



David Clarke
Responsible Accounting Officer